

INDEX OF SHEETS

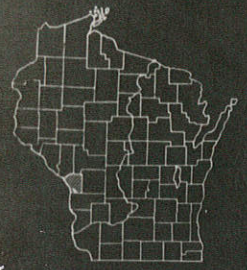
SHEET NO. 1	TITLE
SHEET NO. 2	TYPICAL CROSS SECTIONS
SHEET NO. 3	ESTIMATE OF QUANTITIES
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SHEET NO. 4-7	PLAN AND PROFILE STA. 3+20.6 TO STA. 110+50
SHEET NO. 8-15	DRAINAGE STRUCTURES AND STANDARD DETAILS
SHEET NO. NONE	CROSS SECTIONS

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	412-G(1)	1	15

DIVISION JOB NO. 5706

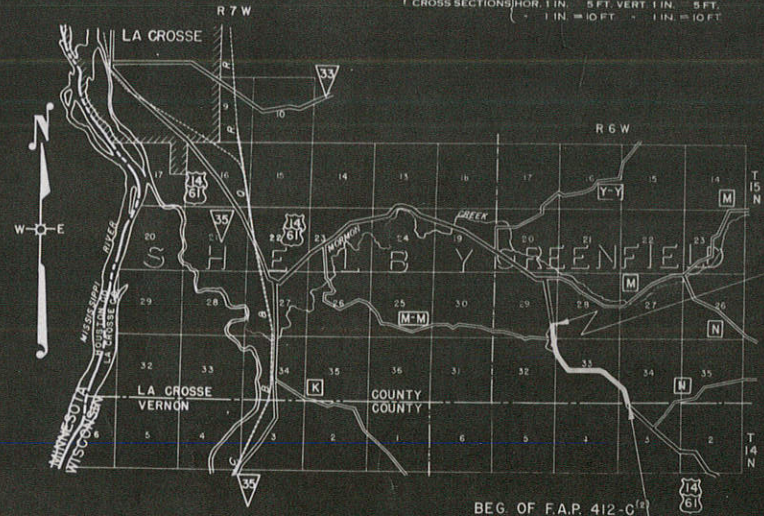
STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

PLAN AND PROFILE OF PROPOSED
WESTBY — LA CROSSE
LA CROSSE COUNTY
FEDERAL AID PROJECT NO. 412-C⁽²⁾



BEGINNING AT A POINT NEAR THE S.W. CORNER OF SEC. 34
T 15 N, R 6 W, AND EXTENDING NORTHWESTERLY TO A POINT
NEAR THE S.E. CORNER OF SECTION 29, T 15 N, R 6 W

SCALES
PLAN 1 IN. = 100 FT.
PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.
- 1 IN. = 10 FT. - 1 IN. = 10 FT.

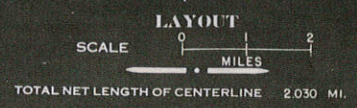


END OF F.A.P. 412-C⁽²⁾
STA. 110+50

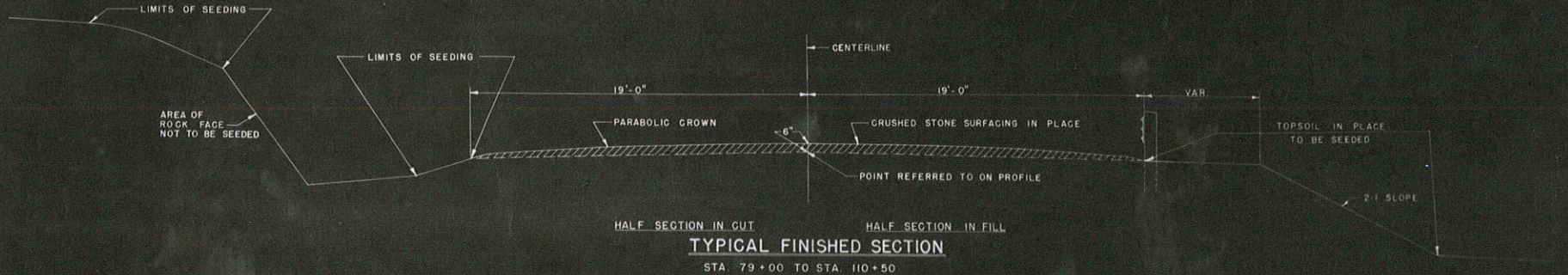
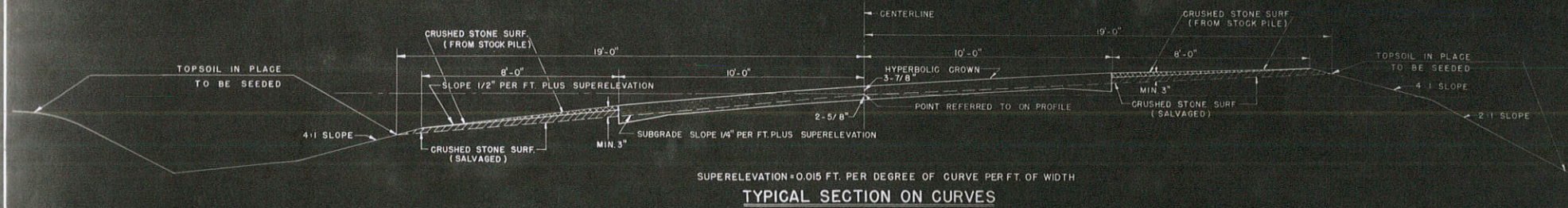
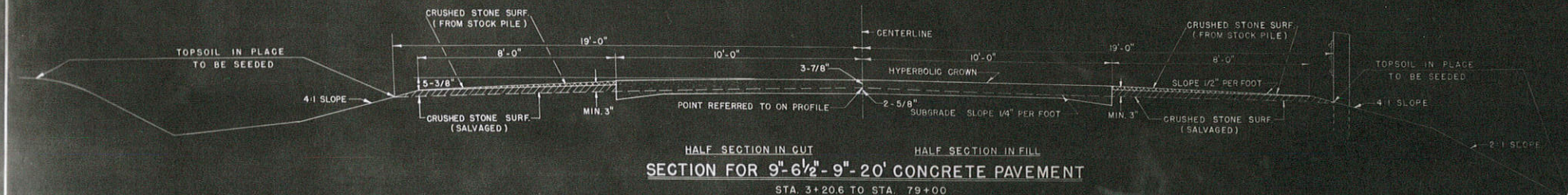
BEG. OF F.A.P. 412-C⁽²⁾
STA. 3+20.6
STA. 2+68.1 F.A.P. 412B

CONVENTIONAL SIGNS

STATE LINE	---	CULVERTS IN PLACE	---
COUNTY LINE	---	CULVERTS REQUIRED	---
TOWNSHIP OR RANGE LINE	---	DROP INLET	---
SECTION LINE	---	POWER POLE	---
NEW RIGHT OF WAY LINE	---	TELEPHONE OR TELEGRAPH POLE	---
PRESENT RIGHT OF WAY LINE	---	RIGHT OF WAY MARKERS	---
WIRE FENCE	---	REFERENCE STAKE FOR HUBS ONLY	---
WOVEN	---	MARSH	---
BARBED	---	HEDGE	---
LOT LINE	---	TREES	---
CORPORATE OR CITY LIMITS	---	GROUND ELEVATION	DATUM LINE
PROPERTY LINE	---	GRADE ELEVATION	DATUM LINE
TRAVELED WAY OR P.E.	---		
RAILROADS	---		
BASE OR SURVEY LINE	---		



STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WIS.		FEDERAL WORKS AGENCY PUBLIC ROADS ADMINISTRATION	
Supervisor J.R.A.	Dist. Computer H.A.B./J.D.	Dist. Engineer M.O. Checker	Div. Checker R.C.J.
CORRECT:		RECOMMENDED FOR APPROVAL	
DATE 7-19-40	DATE 10/10/41	DISTRICT ENGINEER PUBLIC ROADS ADMINISTRATION FEDERAL WORKS AGENCY	
RECOMMENDED FOR APPROVAL		APPROVED:	
DESIGN ENGINEER	CONSTRUCTION ENGINEER	COMMISSIONER PUBLIC ROADS ADMINISTRATION FEDERAL WORKS AGENCY	
APPROVED: <i>E.C. Ratten</i>	DATE 11/7/41	CHECKED IN THE FIELD BY: <i>M.C. Clark</i> ASST. DIV. ENGR.	

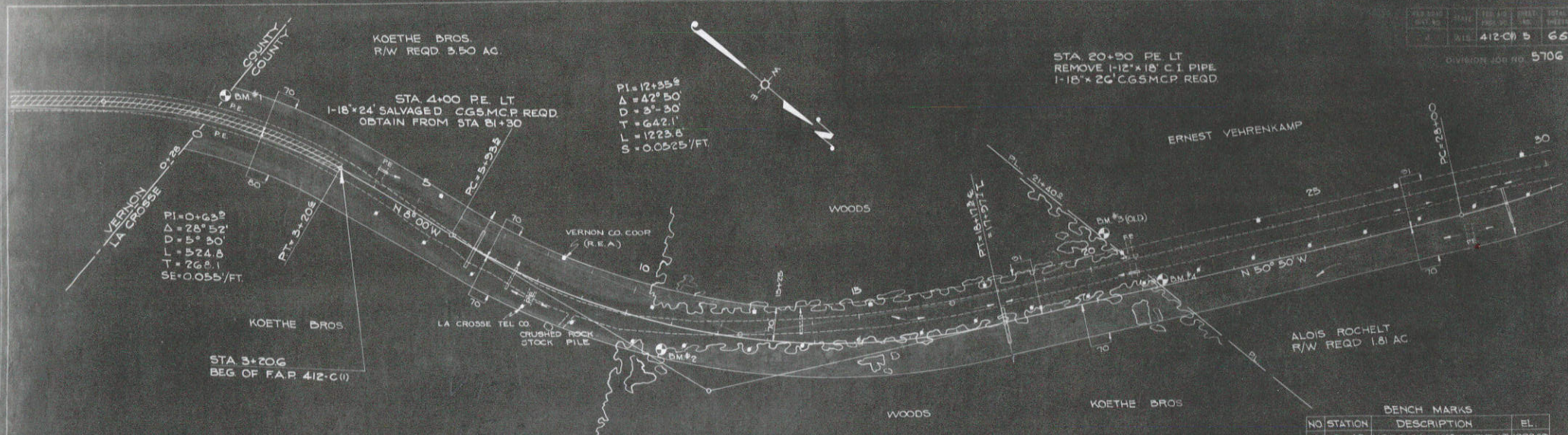


- APPROVED APPLICABLE DRAWINGS
- TRANSVERSE JOINTS SD 2.6 & 27.1
 - LONGITUDINAL JOINTS SD 4.2
 - TRANSVERSE JOINTS SD 5.2
 - PAVEMENT REINFORCEMENT SD 8.4
 - CONCRETE HEADERS SD 9.6
 - MARKER POSTS SD 12.4
 - GABLE GUARD FENCE SD 13.1
 - CONSTRUCTION BARRICADE SD 16.1

TYPICAL CROSS SECTION
 FOR
WESTBY - LA CROSSE ROAD

PLAN 38

NO.	DATE	BY	FIELD	TOTAL
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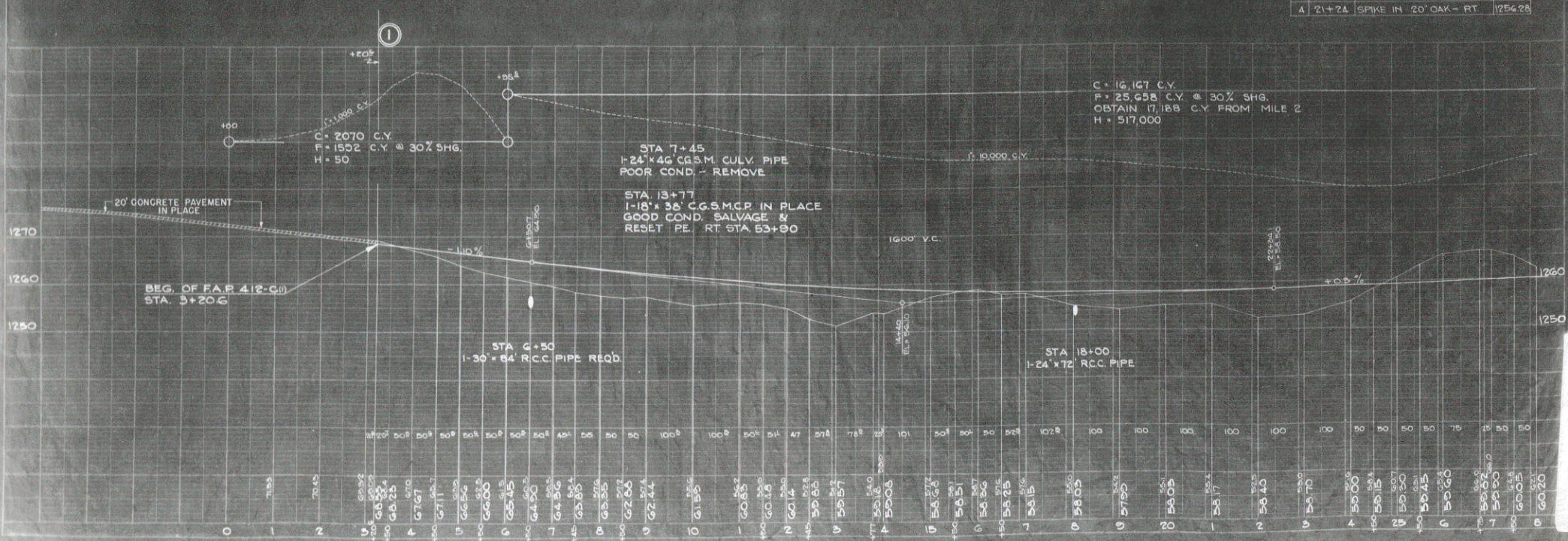


$PI = 12+35.8$
 $\Delta = 42^{\circ} 50'$
 $D = 31-30'$
 $T = 642.1'$
 $L = 1223.8'$
 $S = 0.0525'/FT.$

$PI = 0+63.2$
 $\Delta = 28^{\circ} 57'$
 $D = 5-30'$
 $L = 574.8'$
 $T = 268.1'$
 $SE = 0.055'/FT.$

BENCH MARKS

NO	STATION	DESCRIPTION	EL.
1	0+42	U.S.G.S. CONC. MONUMENT LT	1272.60
2	11+45	SPIKE IN 24" OAK - RT	1260.38
3	20+41	SPIKE IN 18" OAK - LT	1258.12
4	21+24	SPIKE IN 20" OAK - RT	1256.28



$C = 16,167$ C.Y.
 $F = 25,658$ C.Y. @ 30% SHG.
 OBTAIN 17,188 C.Y. FROM MILE 2
 $H = 517,000$

STA 7+45
 1-24" x 46" CGS.M. CULV. PIPE
 POOR COND. - REMOVE

STA 13+77
 1-18" x 36" CGS.M.C.P. IN PLACE
 GOOD COND. SALVAGE &
 RESET PE. RT STA 53+90

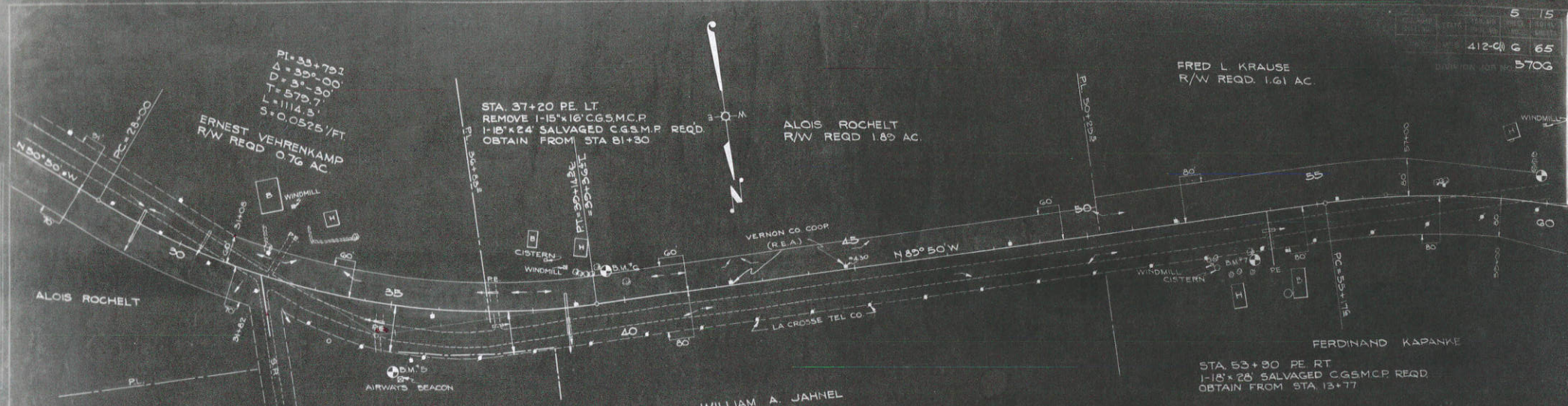
STA 4+50
 1-30" x 64" R.C.C. PIPE REQD.

STA 18+00
 1-24" x 72" R.C.C. PIPE

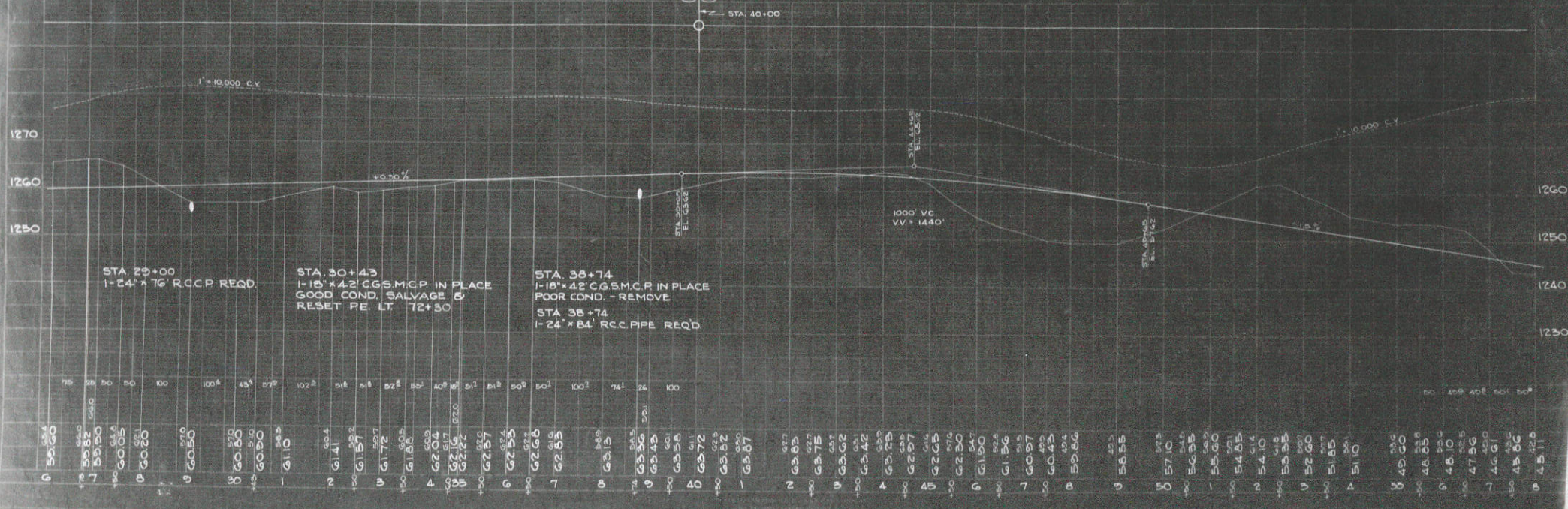
1:2500
 1" = 100'
 1:5000
 1" = 200'

1:5000
 1" = 100'
 1:10000
 1" = 200'

5 15
 412-91 G 65
 570G



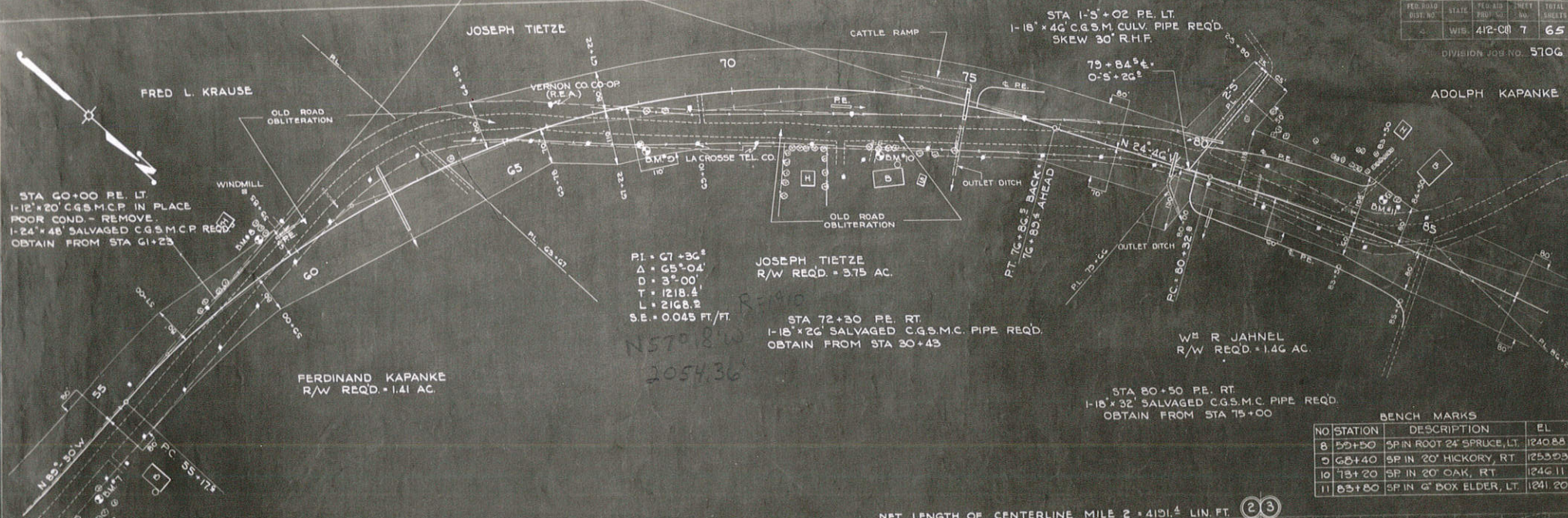
NET LENGTH OF CENTERLINE-MILE NO. 1 = 36769 LIN. FT.



BENCH MARKS

NO	STATION	DESCRIPTION	EL.
5	34+50	DISCRETE BENCH MARK	1245.85
6	30+50	SP. N. CORNER PINE, 75 FT.	1244.27
7	53+42	SP. IN IO MULBERRY, RT.	1258.42

ADOLPH KAPANKE



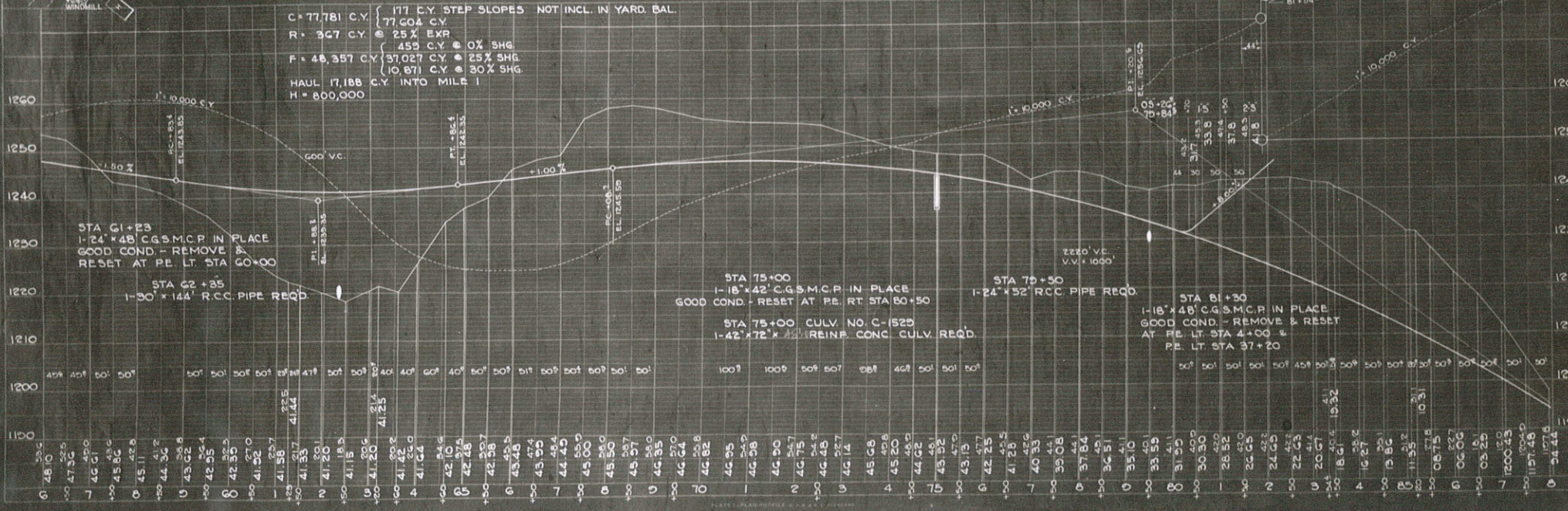
PI - 67+36.2
 Δ - 65°04'
 D - 3°00'
 T - 1218.4
 L - 2168.2
 S.E. = 0.045 FT./FT.

1570810
 205436

BENCH MARKS

NO	STATION	DESCRIPTION	EL.
8	59+50	SP IN ROOT 24' SPRUCE, LT.	1240.85
9	68+40	SP IN 20' HICKORY, RT.	1253.05
10	73+20	SP IN 20' OAK, RT.	1246.11
11	83+80	SP IN 6' BOX ELDER, LT.	1241.20

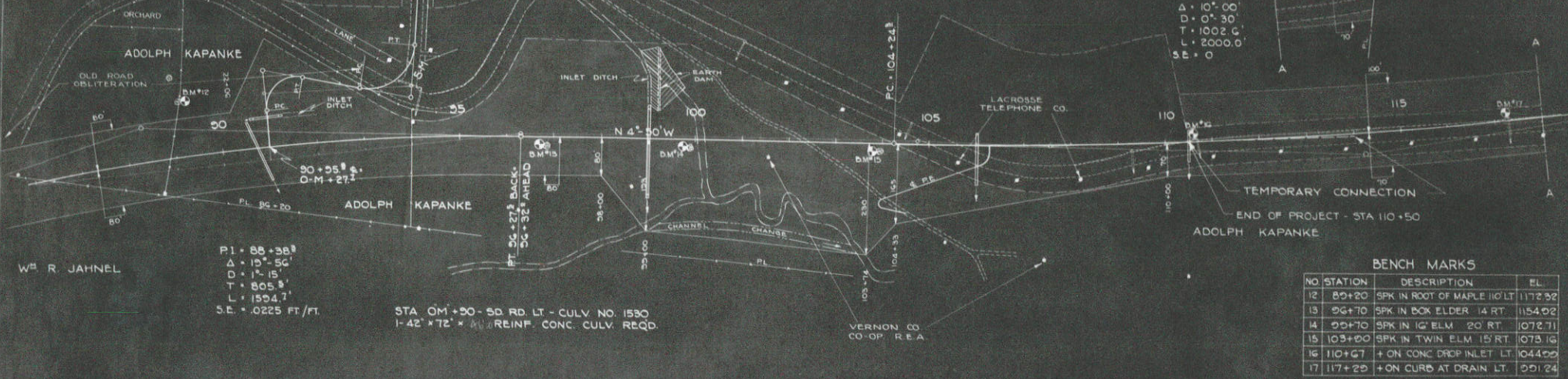
NET LENGTH OF CENTERLINE MILE 2.41514 LIN. FT. (23)



- 138 1/4" = 100'
- 138 3/4" = 200'
- 139 1/2" = 300'
- 139 3/4" = 400'
- 140 1/2" = 500'
- 140 3/4" = 600'
- 141 1/2" = 700'
- 141 3/4" = 800'
- 142 1/2" = 900'
- 142 3/4" = 1000'

PI = 1M+85.1
 Δ = 106° 00'
 D = 90° 50'
 T = 85 G'
 L = 116 G'
 S.E. = 0.035 FT/FT
 PC = 1M+05.2
 PT = 2M+22.1 BACK
 2M+15.4 AHEAD

PI = 4-M+51.4
 Δ = 55° 45'
 D = 57° 17'
 T = 110 G'
 L = 107.1'
 S.E. = 0.035 FT/FT
 PC = 3-M+40.9
 PT = 5M+07.2 BACK
 5M+35.3 AHEAD



W² R. JAHNEL

PI = 88+38.8
 Δ = 10° 50'
 D = 1° 15'
 T = 805.8'
 L = 1594.7'
 S.E. = .0225 FT/FT

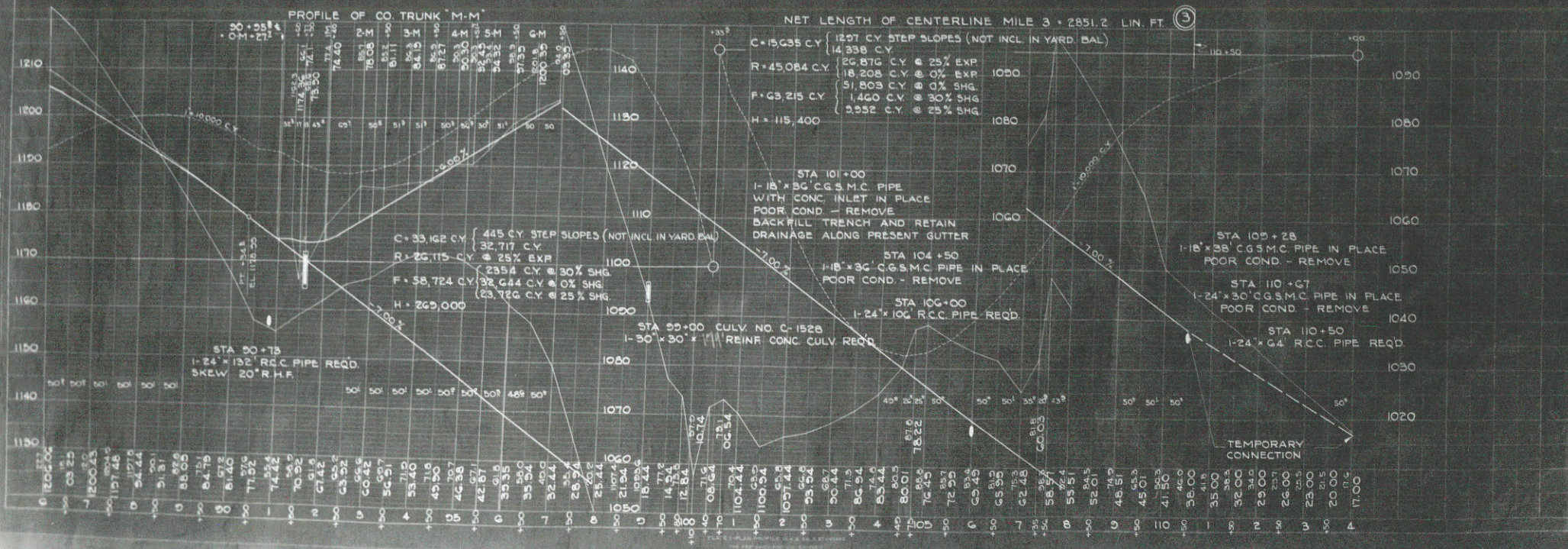
STA. 0M+30-50 RD. LT. - CULV. NO. 1530
 1-42" x 72" x 4' REINF. CONC. CULV. REQD.

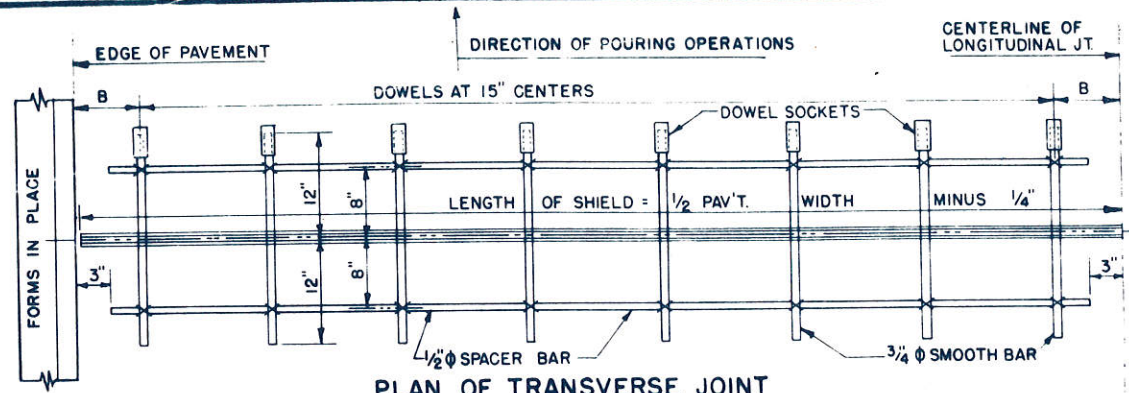
ADOLPH KAPANKE
 R/W REQD. = 13.07 AC.

PI = 114+26.2
 Δ = 10° 00'
 D = 0° 30'
 T = 1002 G'
 L = 2000.0'
 S.E. = 0

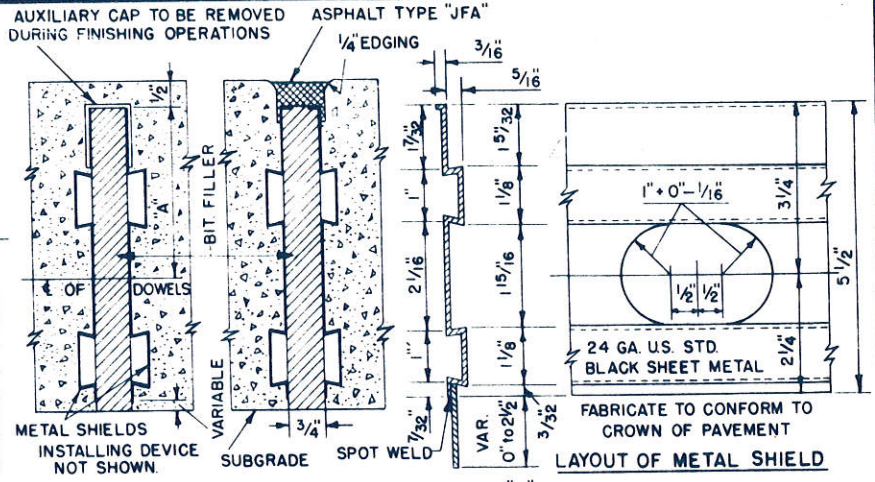
BENCH MARKS

NO	STATION	DESCRIPTION	EL.
12	80+20	SPK IN ROOT OF MAPLE 110' LT.	1172.32
13	96+70	SPK IN BOX ELDER 14 RT.	1154.02
14	99+70	SPK IN 12' ELM 20 RT.	1072.71
15	103+00	SPK IN TWIN ELM 15 RT.	1073.16
16	110+67	+ ON CONC DROP INLET LT.	1044.25
17	117+25	+ ON CURB AT DRAIN LT.	1051.24

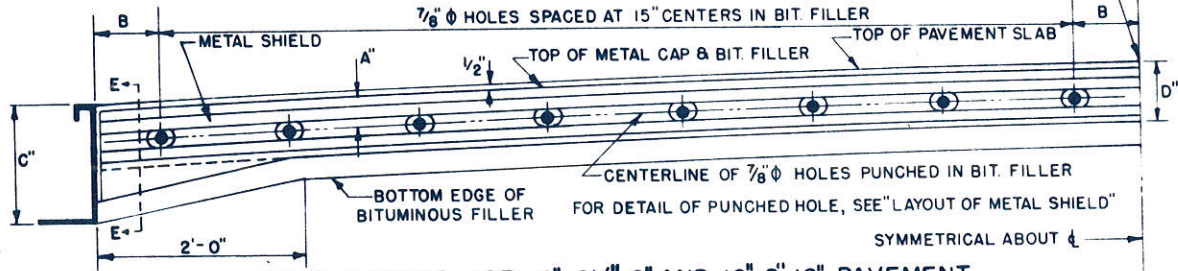




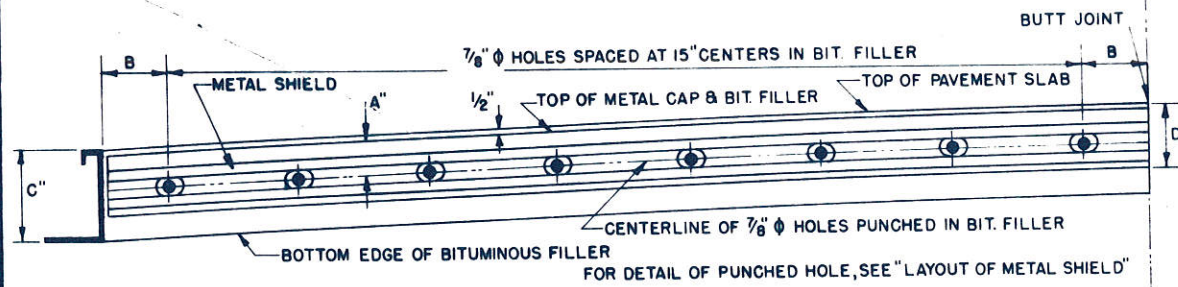
NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED.



INSTALLATION PRACTICE

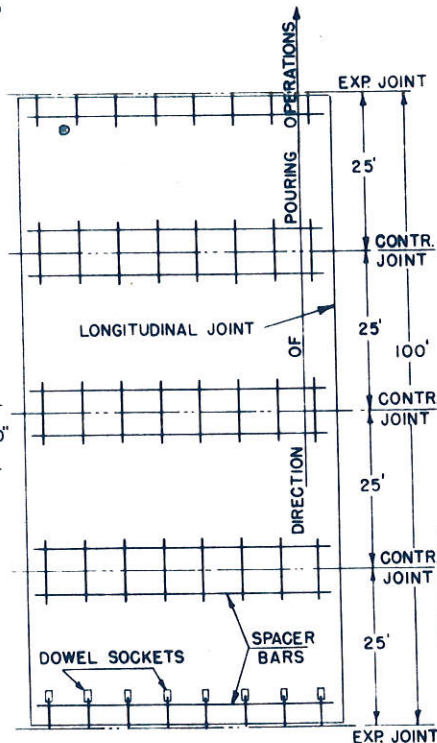


JOINT LAYOUT FOR 9'-6 1/2" AND 10'-8"-10" PAVEMENT 18'-20' OR 22' WIDE

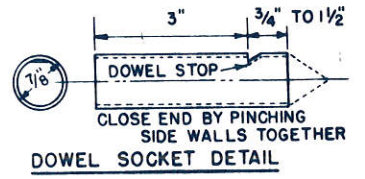


JOINT LAYOUT FOR UNIFORM 7" AND 8 1/2" PAVEMENT.

WIDTH OF PAVEMENT	9'-6 1/2" - 9"				UNIFORM 7"				UNIFORM 8 1/2"				10'-8"-10"			
	A	B	C	D	A	B	C	D	A	B	C	D	A	B	C	D
18'	2 3/4	9	9	6 1/2	3	9	7	7	3 3/4	9	8 1/2	8 1/2				
20'	2 3/4	7 1/2	9	6 1/2	3	7 1/2	7	7	3 3/4	7 1/2	8 1/2	8 1/2	3	7 1/2	10	8
22'													3 1/2	8	10	8



PLAN SHOWING JOINT LAYOUT



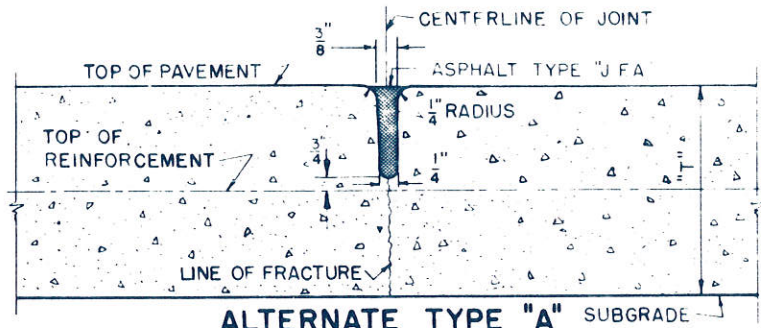
GENERAL NOTES

- MARGINAL OR SPACER BARS SHALL BE 1/2" Ø & DOUBLE CROSSWIRED TO THE DOWELS, I.E. WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION USE NO. 16 GA. LOOP TIE WIRE.
- THE TERM "BITUMINOUS FILLER" USED HERE REFERS TO AN APPROVED PREMOULDED BIT. EXP. JT. FILLER, 3/4" THICK, SHAPED TO CONFORM TO PERTINENT CROSS SECTION OF PAVEMENT SLAB AND CUT 1/2" BELOW CROWN OF PAVEMENT.
- ENDS OF SHIELDS TO BE CLOSED BY CRIMPING IN A MANNER SATISFACTORY TO THE ENGINEER.
- METHOD OF CONNECTING BUTT JOINTS SHALL MEET THE APPROVAL OF THE ENGINEER.
- MODIFICATIONS OF DESIGN WILL BE ACCEPTABLE SUBJECT TO THE APPROVAL OF THE ENGINEER.
- METAL SHIELDS ARE NOT TO BE HELD TOGETHER BY MEANS OF NAILS OR OTHER DEVICES WHICH WOULD TEND TO HOLD SHIELDS TOGETHER PERMANENTLY AFTER CONCRETE HAS SET.

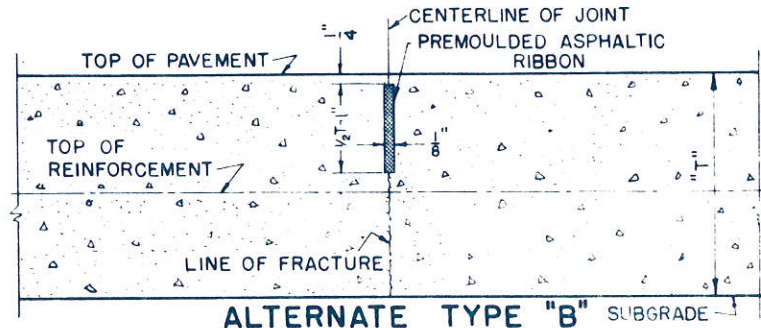
TRANSVERSE EXPANSION JOINT FOR CONCRETE PAVEMENT PREMOULDED BITUMINOUS TYPE

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:
Frank Green DESIGN ENGINEER
M. Blum CONSTRUCTION ENGINEER
 DATE: 3-20-40
 DRAWN G.A.L. CHECKED D.F.C. DATE: *E.L. Berntzen* STATE HIGHWAY ENGINEER

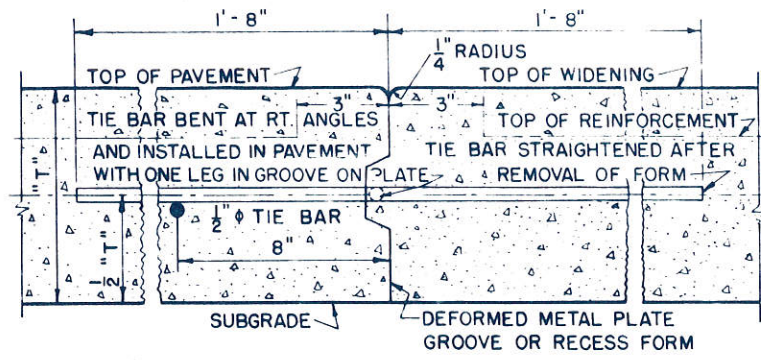


**ALTERNATE TYPE "A"
POURED JOINT**



**ALTERNATE TYPE "B"
PRE MOULDED JOINT**

LONGITUDINAL WEAKENED PLANE JOINT



LONGITUDINAL CONSTRUCTION JOINT

SPACING OF TIE BARS IN LONGITUDINAL CONSTRUCTION JOINT:
 4'-0" CTR. TO CTR. THRU ABUTTING THICKENED EDGE PAVEMENT SLABS.
 3'-0" CTR. TO CTR. THRU ABUTTING UNTHICKENED EDGE PAVEMENT SLABS.

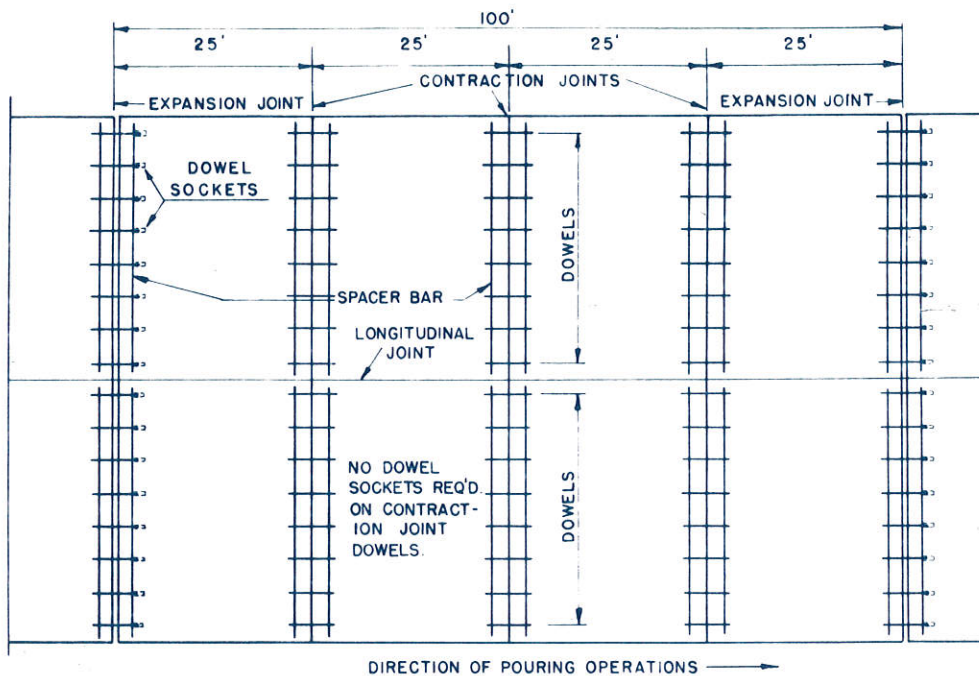
CONSTRUCTION NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DIMENSION "T" INDICATES PAVEMENT DEPTH ALONG LINE OF LONGITUDINAL JOINTS.

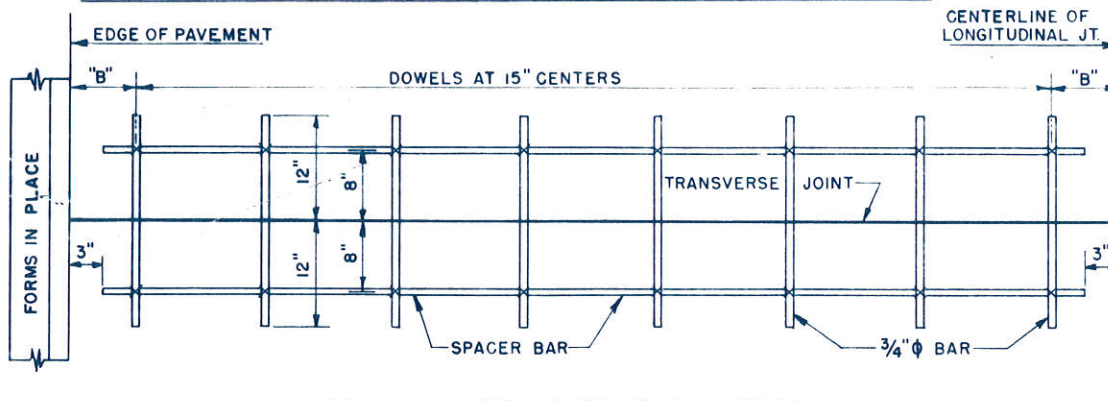
FOLLOWING THE INSTALLATION OF THE JOINT OR TEMPORARY FILLER STRIPS FOR CREATING A WEAKENED PLANE, THE FINISHING MACHINE SHALL SCREED OVER THE SECTION AT LEAST ONE TIME.

LONGITUDINAL JOINT DETAILS FOR REINFORCED CONCRETE PAVEMENT	
STATE HIGHWAY COMMISSION OF WISC.	
RECOMMENDED FOR APPROVAL:	
<i>Frank Chase</i> DESIGN ENGINEER	<i>W. Bluh</i> CONSTRUCTION ENGINEER
3-20-40	DATE
APPROVED	
DRAWN C.A.L.	3-20-40 DATE
CHECKED n.f.c.	<i>E.G. Rottler</i> STATE HIGHWAY ENGINEER



PLAN SHOWING JOINT LAYOUT

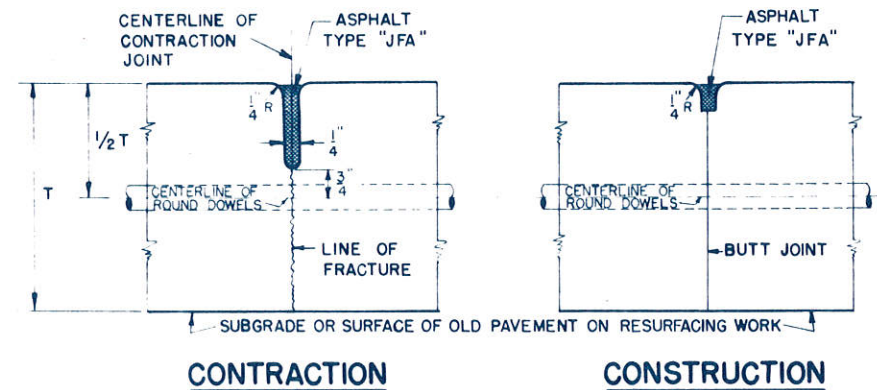
FOR JOINT SPACING ON CONCRETE RESURFACE AND WIDENING SEE STD. SPECIFICATIONS.



PLAN OF TRANSVERSE JOINT

NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED.

WIDTH OF PAVEMENT	"B" DIMENSION
18'	9"
20'	7 1/2"
22'	6"

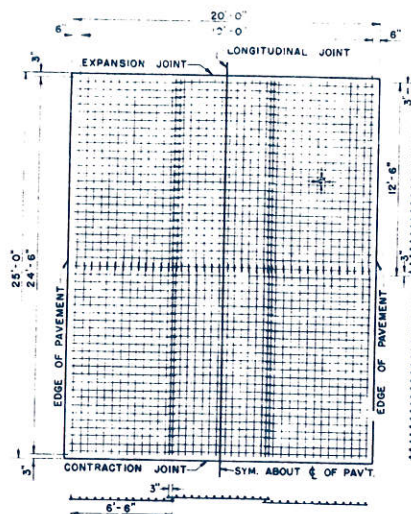


GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

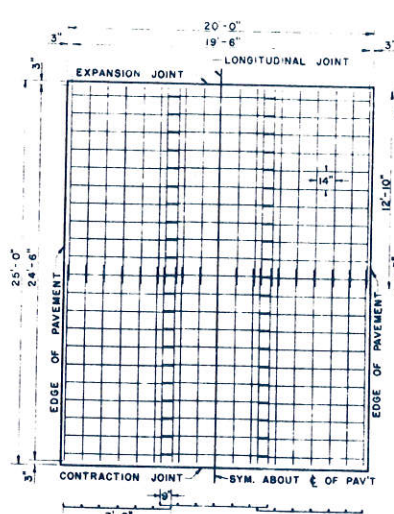
DOWELS FOR CONTRACTION JOINTS IN CONCRETE RESURFACING SHALL BE PLACED 2" BELOW TOP OF NEW SLAB AND DIRECTLY BENEATH PAVEMENT REINFORCEMENT.

TRANSVERSE JOINTS CONTRACTION - CONSTRUCTION	
STATE HIGHWAY COMMISSION OF WISC.	
RECOMMENDED FOR APPROVAL:	
<i>Frank Chase</i> DESIGN ENGINEER	<i>M. Bluk</i> CONSTRUCTION ENGINEER
3-20-40	
APPROVED:	
<i>E. G. Rostky</i> STATE HIGHWAY ENGINEER	
DRAWN W.W.-C.L.	3-20-40
CHECKED <i>NFC</i>	DATE



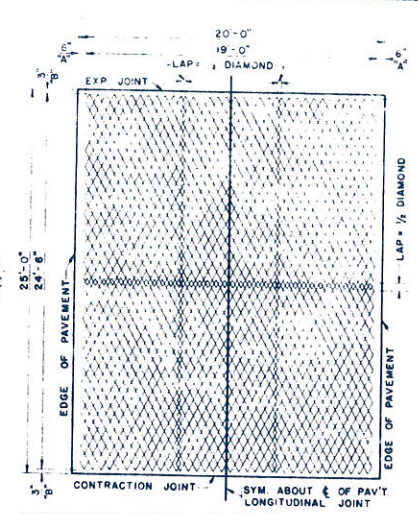
STEEL FABRIC REINFORCEMENT

NOTE:
THE DIAMETER OF ALL MEMBERS SHALL BE NOT LESS THAN 0.2253 INCHES (NO. 4 GAUGE).



WELDED BAR MAT REINFORCEMENT

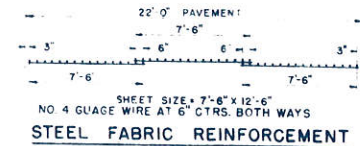
NOTE:
MEMBERS SHALL BE NOT LESS THAN THREE-EIGHTHS INCH ROUND SMOOTH STEEL RODS.



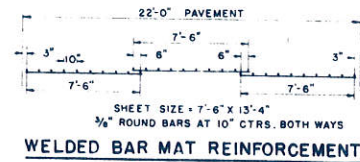
EXPANDED METAL MESH REINFORCEMENT

PAVT WIDTH	SHEET SIZE	"A"	"B"
20'	6'-6" X 12'-6"	6"	3"
22'	7'-3" X 12'-6"	4 1/2"	3"
30'	7'-6" X 12'-6"	4 1/2"	3"

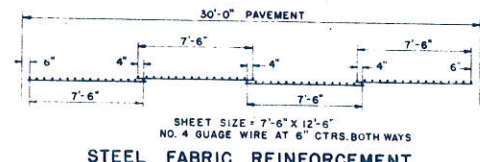
NOTE
SIDE LAP AND END LAP = 1/2 DIAMOND
SIZE OF DIAMOND SHALL BE NOT LESS THAN 5" X 12" NOR MORE THAN 6 1/2" X 12" UNLESS OTHERWISE SPECIFIED.
DOWEL BARS NOT SHOWN.
MESH SHALL BE EXPANDED FROM NO. 7 GAGE SHEET STEEL 0.1785" THICK
THE MESH SHALL WEIGH NOT LESS THAN 56LB/100 SQ FT. AND THE SECTIONAL AREA SHALL BE NOT LESS THAN 0.1647 SQ. IN./FT. OF WIDTH



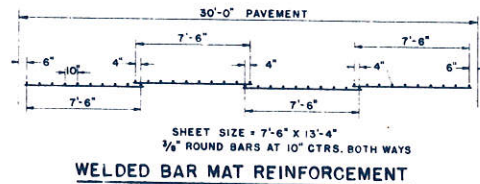
STEEL FABRIC REINFORCEMENT



WELDED BAR MAT REINFORCEMENT



STEEL FABRIC REINFORCEMENT



WELDED BAR MAT REINFORCEMENT

GENERAL NOTES

ALL REINFORCEMENT MUST BE SHIPPED FROM THE FACTORY AND DELIVERED ON THE SITE OF THE WORK IN FLAT SHEETS.

DETAILS OF CONSTRUCTION NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

PAVEMENT REINFORCEMENT

STATE HIGHWAY COMMISSION OF WISCONSIN
RECOMMENDED FOR APPROVAL:

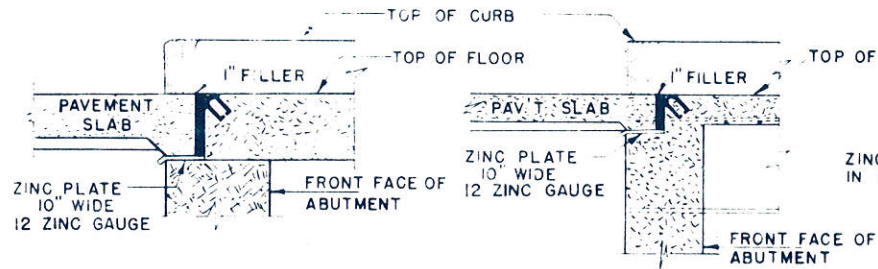
Frank Brown
DESIGN ENGINEER

M. B. ...
CONSTRUCTION ENGINEER

APPROVED:

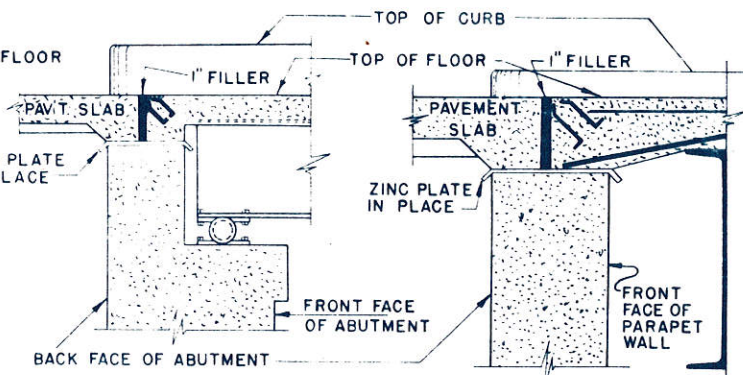
Drawn by: WJA DATE: 3-20-40
Checked by: WJA

E. L. Roan
STATE HIGHWAY ENGINEER



SLAB SPANS
6 TO 24 FT. INCL.

I-BEAM SPANS
8 TO 45 FT. INCL.
DECK GIRDER SPANS
20 TO 45 FT. INCL.
ARE SIMILAR

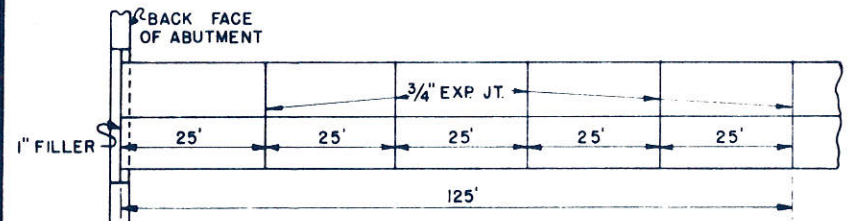


I-BEAM SPANS
50 TO 60 FT. INCL.
DECK GIRDER SPANS
50, 55 & 60 FT. INCL.
ARE SIMILAR

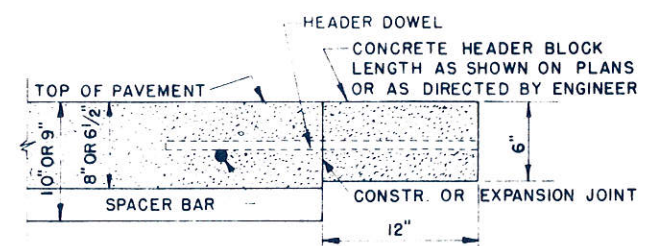
STEEL TRUSS AND
PLATE GIRDERS

METHODS OF CONNECTING CONCRETE PAVEMENT WITH BRIDGE STRUCTURES

NOTE: 1" FILLER TO BE PREMOLDED BITUMINOUS JOINT OR EQUIVALENT.
EXPANSION JOINT FILLER ALSO TO BE PLACED BETWEEN CURB ENDS OR OTHER PROJECTING APPURTENANCES IN THE EVENT PAVEMENT IS PLACED IN CONTACT THEREWITH



DETAIL OF 3/4 INCH EXPANSION JOINTS IN STRUCTURE
APPROACHES



CONSTRUCTION JOINT WITH 1/2" ϕ OR \square TIE BARS WHEN PLACED AT EDGE OF PAVEMENT OR STANDARD EXPANSION JOINT WITH STANDARD DOWELS WHEN PLACED AT END OF PAVEMENT.

CONCRETE HEADER BLOCK

**BRIDGE APPROACHES
& HEADERS**

STATE HIGHWAY COMMISSION OF WISC.

RECOMMENDED FOR APPROVAL:

Frank Crave
DESIGN ENGINEER

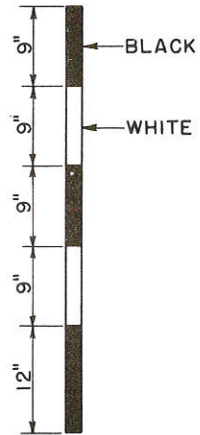
3-20-40 *M. Shub*
CONSTRUCTION ENGINEER

APPROVED

DRAWN: C.A.L. 3-20-40 *E.G. Rottgen*
CHECKED: n.f.c. DATE STATE HIGHWAY ENGINEER

SECTION OF SNOW FENCE TO BE PAINTED ALTERNATELY BLACK AND WHITE AS SHOWN BELOW.
 LENGTH TO BE SUFFICIENT TO REACH FROM TOE OF THE BACKSLOPE TO TOE OF THE BACKSLOPE.
 PROVIDE SUFFICIENT NUMBER OF INTERMEDIATE POSTS TO ADEQUATELY SUPPORT THE FENCE.

STANDARD "ROAD CLOSED" SIGN (W.H.C. - NO. R-15) TO BE ERECTED ON FENCE AT THE TOP ON
 THE CENTERLINE OF ROAD.

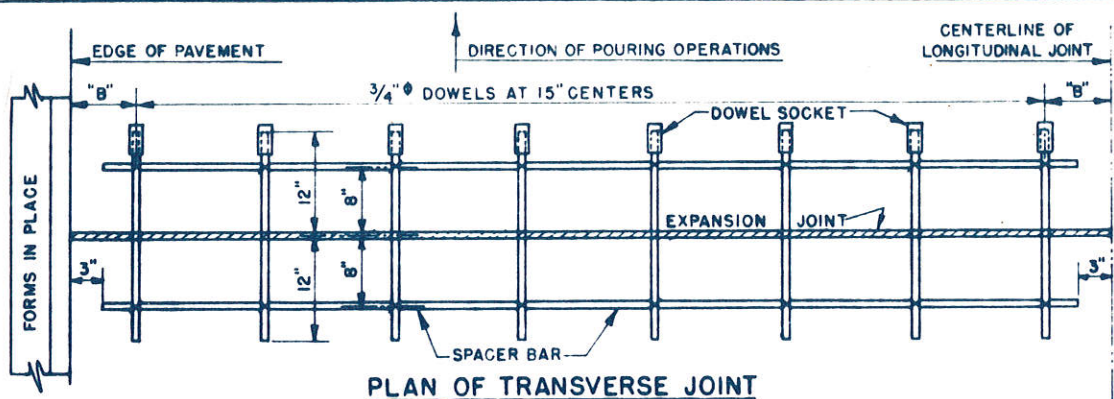


DETAIL SHOWING METHOD OF STRIPING
 FOR 4' SNOW FENCE.



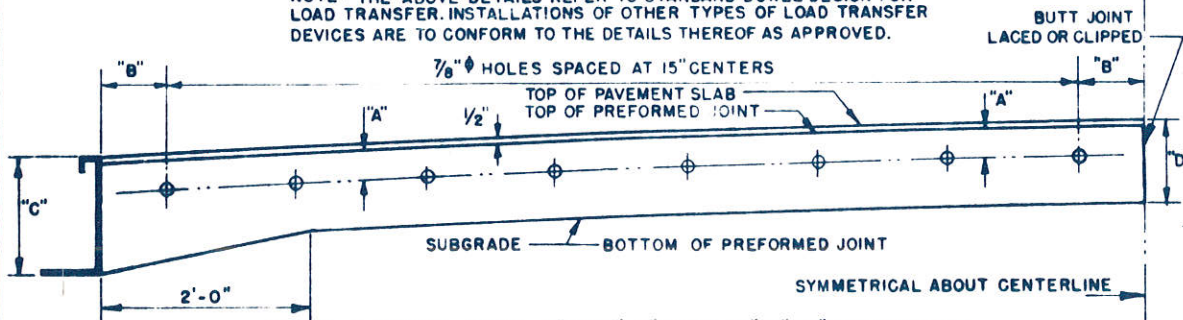
THIS SIGN TO BE PLACED
 AT BEGINNING OF DETOUR

STATE HIGHWAY COMMISSION OF WISCONSIN	
CONSTRUCTION BARRICADE	
RECOMMENDED FOR APPROVAL:	
<i>Frank Crase</i> DESIGN ENGINEER	
<i>M. J. [Signature]</i> CONSTRUCTION ENGINEER	
APPROVED:-	
DRAWN HM CHECKED WHG	3/28/38 DATE
<i>E. G. [Signature]</i> STATE HIGHWAY ENGINEER	

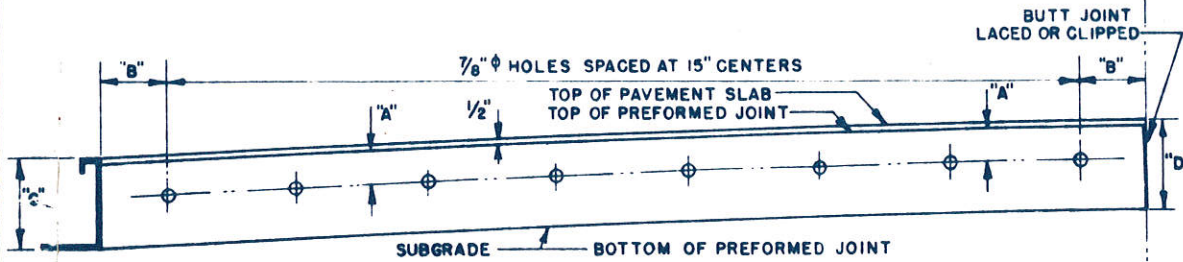


PLAN OF TRANSVERSE JOINT

NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED.

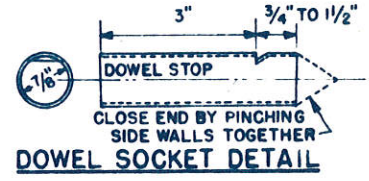
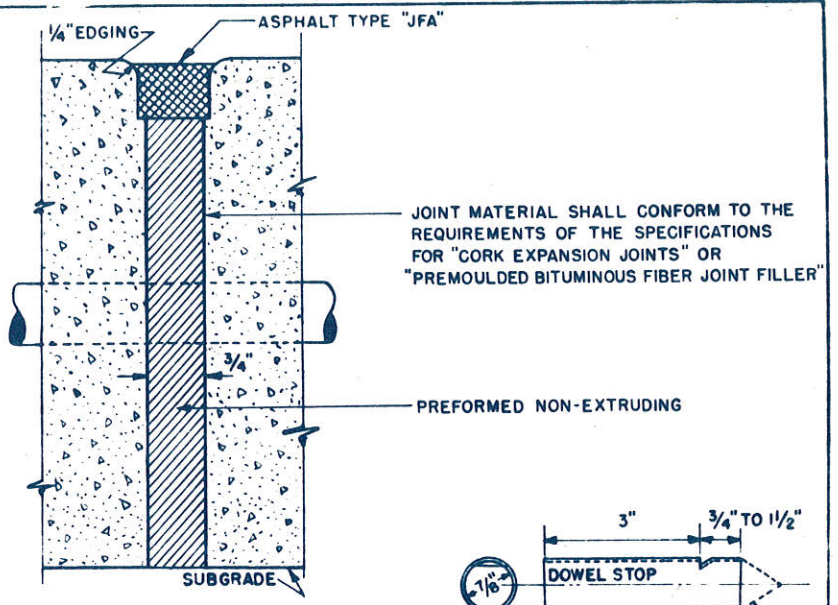


**JOINT LAYOUT FOR 9'-6 1/2"-9" AND 10'-8"-10" PAVEMENT
18', 20' OR 22' WIDE**



JOINT LAYOUT FOR UNIFORM 7" AND 8 1/2" PAVEMENT

WIDTH OF PAVEMENT	9'-6 1/2"-9"				UNIFORM 7"				UNIFORM 8 1/2"				10'-8"-10"			
	DIMENSION				DIMENSION				DIMENSION				DIMENSION			
	A	B	C	D	A	B	C	D	A	B	C	D	A	B	C	D
18'	2 3/4	9	9	6 1/2	3	9	7	7	3 3/4	9	8 1/2	8 1/2				
20'	2 3/4	7 1/2	9	6 1/2	3	7 1/2	7	7	3 3/4	7 1/2	8 1/2	8 1/2	3 1/2	7 1/2	10	8
22'													3 1/2	6	10	8



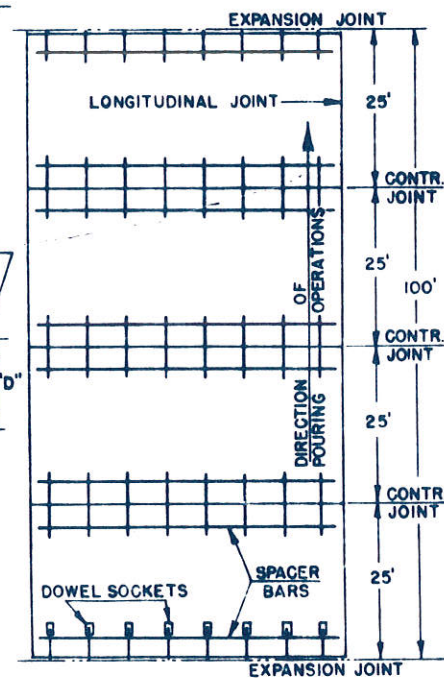
DOWEL SOCKET DETAIL

GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SPACER BARS SHALL BE 1/2" ROUND, SMOOTH OR DEFORMED BARS, WIRED IN BOTH DIRECTIONS. USE NO. 16 GA. LOOP TIE WIRE.

DOWELS FOR CONTRACTION JOINTS IN CONCRETE RESURFACING SHALL BE PLACED 2" BELOW TOP OF NEW SLAB AND DIRECTLY BENEATH PAVEMENT REINFORCEMENT.



PLAN SHOWING JOINT LAYOUT

FOR JOINT SPACING ON CONCRETE RESURFACE AND WIDENING SEE STD. SPECIFICATIONS.

TRANSVERSE EXPANSION JOINT

PREFORMED RESILIENT TYPE

STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

Franklin DESIGN ENGINEER *McClure* CONSTRUCTION ENGINEER

APPROVED:

DRAWN: C.A.L. 3-20-40 DATE *E. L. Bailey* STATE HIGHWAY ENGINEER