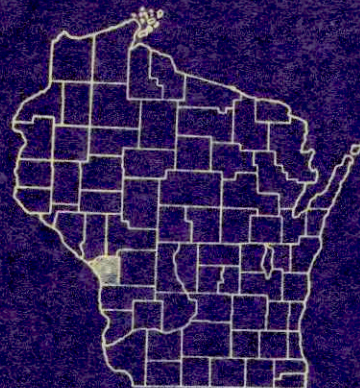


INDEX OF SHEETS

- SHEET NO. 1 TITLE
- SHEET NO. 2 TYPICAL CROSS SECTIONS
- SHEET NO. 2 ESTIMATE OF QUANTITIES
- SHEET NO. 2 MISCELLANEOUS QUANTITIES
- SHEET NO. — RIGHT OF WAY PLAT
- SHEET NO. 3-4 PLAN AND PROFILE STA. 0+10 TO STA. 56+00
- SHEET NO. 5-11 STANDARD DETAILS
- SHEET NO. 12-19 DRAINAGE STRUCTURES
- SHEET NO. 20-33 CROSS SECTIONS



STATE OF WISCONSIN  
STATE HIGHWAY COMMISSION OF WISCONSIN

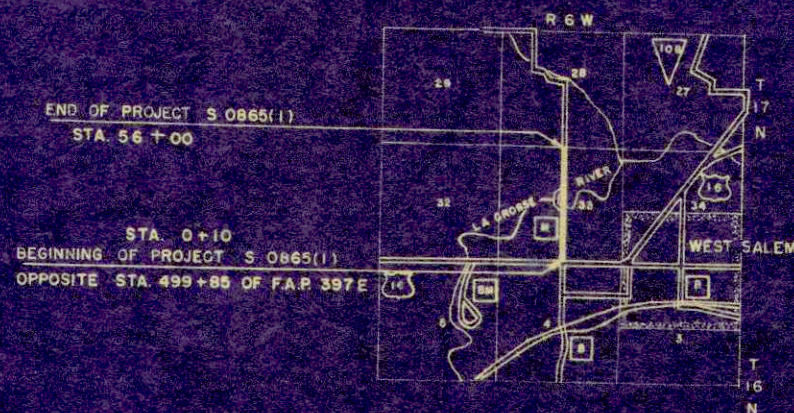
PLAN AND PROFILE OF PROPOSED

**WEST SALEM - HOLMEN**  
LA CROSSE COUNTY  
PROJECT S 0865(1)

BEGINNING AT A POINT NEAR THE SOUTH ONE QUARTER CORNER OF SECTION 33, TOWNSHIP 17 NORTH, RANGE 6 WEST AND EXTENDING NORTHERLY TO A POINT NEAR THE NORTH ONE QUARTER CORNER OF SECTION 33, TOWNSHIP 17 NORTH, RANGE 6 WEST.

COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
32.6	665.0		11.1	WIS. 5 N	1	33

SCALES  
 PLAN 1 IN. = 100 FT.  
 PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.  
 CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.  
 HOR. 1 IN. = 10 FT. VERT. 1 IN. = 10 FT.



By # 72

CONVENTIONAL SIGNS

- |                           |           |                               |           |
|---------------------------|-----------|-------------------------------|-----------|
| STATE LINE                | — — — — — | CULVERTS IN PLACE             | — — — — — |
| COUNTY LINE               | — — — — — | CULVERTS REQUIRED             | — — — — — |
| TOWNSHIP OR RANGE LINE    | — — — — — | DROP INLET                    | □         |
| SECTION LINE              | — — — — — | POWER POLE                    | — — — — — |
| NEW RIGHT OF WAY LINE     | — — — — — | TELEPHONE OR TELEGRAPH POLE   | — — — — — |
| PRESENT RIGHT OF WAY LINE | — — — — — | RIGHT OF WAY MARKERS          | — — — — — |
| WIRE FENCE { WOVEN        | — — — — — | REFERENCE STAKE FOR HUBS ONLY | — — — — — |
| { BARBED                  | — — — — — | MARSH                         | — — — — — |
| LOT LINE                  | — — — — — | HEDGE                         | — — — — — |
| CORPORATE OR CITY LIMITS  | — — — — — | TREES                         | — — — — — |
| PROPERTY LINE             | — — — — — |                               |           |
| TRAVELED WAY OR P.E.      | — — — — — |                               |           |
| RAILROADS                 | — — — — — |                               |           |
| BASE OR SURVEY LINE       | — — — — — |                               |           |

LAYOUT

SCALE 1 MILE  
 TOTAL NET LENGTH OF CENTERLINE = 1.039 MI.

STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WIS.		DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
SURVEYOR E.C.G.	NOTE BOOK 777		
DISTRICT COMPUTER D.A.J.	H. O. CHECKER		
DISTRICT CHECKER R.G.J.	CORRECT		
CORRECT			
DATE 8/9/54	T.M. Kowalchuk DISTRICT ENGINEER	APPROVED:	DATE
RECOMMENDED FOR APPROVAL			
DATE 8/11/54	J.L.Pitt ENGINEER OF DESIGN		
APPROVED:			
DATE 8/11/54	E.C. Rostetter STATE HIGHWAY ENGINEER		
			DISTRICT ENGINEER

# ESTIMATE OF QUANTITIES

CONTRACT NO. 1 B 2

THIS PROJECT IS TO BE EXECUTED UNDER THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION OF THE STATE HIGHWAY COMMISSION OF WISCONSIN - EDITION OF 1951. APPROVED JANUARY 23, 1952, FEDERAL AID REQUIRED  
 CONTRACT PROVISIONS APPROVED NOVEMBER 8, 1948, AND SPECIAL PROVISIONS AS ATTACHED TO PROPOSALS.

RI - 6/23/54

PROJECT	SHEET NO.	TOTAL SHEETS
S 0865 (1)	2	33

STATION TO STATION	NET LENGTH OF CENTER LINE	CLEAR GRUBBING		UNCL. EXC.	PLACING SHOULDER OR AGGREGATE	MANIPULATION	SAND GRAVEL FILL	FINISHING ROADWAY	GROUTED RIPRAP	GRAVEL OR CRUSHED STONE						CONC. MASTY INLET STA.	CONC. MASTY DROP PIPE STA.	MARKER POSTS FOR R/W	CONC. MSRY. BRIDGE STA.	REMOVE OLD MAT'L FOR COURSE	BIT. PLATE FOR BEAM STRUCT.	EXCAV. FOR BRDGS.	CONC. MASTY BRDGS.	BAR STEEL REINF. (BRDGS)	STRUCT. CARBON STEEL	SHEET LEAD	UNTR. TIMBER TEST PILING (1 AT 40)	TRTD. TIMBER DELIV'D	TREATED TIMBER DRIVEN	PILE SHOES	FLOOR DRAINS	RIP RAP	CONC. CURB & GUTTER 30" TYPE "A"	TOPSOIL		CONC. SURF. DRAIN BRIDGE	SEEDING	INLET TYPE "I-N" STA.					
		2101-1	2101-4							2106-5	S.P.	S.P.	2110-1	2114-1	2512-3																			2203-1	2313-2				2411-11	2411-12	2411-13	2411-14	2411-42
UNIT	LIN. FT.	STA.	STA.	C.Y.	C.Y.	L.S.	C.Y.	STA.	C.Y.	C.Y.	C.Y.	C.Y.	C.Y.	C.Y.	C.Y.	L.S.	L.S.	EACH	EACH	L.S.	L.S.	GAL.	LIN. FT.	C.Y.	C.Y.	LB.	LB.	LB.	L.S.	LIN. FT.	LIN. FT.	EACH	EACH	C.Y.	UN. FT.	SQ. YD.	SQ. YD.	EACH	SQ. YD.	EACH			
0+10 - 56+00	5590.0	13	11	25,180	196	1	8730	54	11	3294	1165	140	284	92	68	102	124	1	1	10	23	1	1	19,780	315	540	526.4	61,640	187,020	345	1	480	480	16	8	147	60	5300	2	7900	1		
APPROACH 56+00 - 57+00										56	20																																
		13	11	25,180	196	1	8730	54	11	3350	1165	140	284	92	68	102	124	1	1	10	23	1	1	19,780	315	540	526.4	61,640	187,020	345	1	480	480	16	8	147	60	5300	2	7900	1		

CONTRACT 2

### CLEARING

SEC. NO.	STATION TO STATION	STA.
1	0+00 - 2+00	2
1	23+00 - 26+00	3
1	28+00 - 31+00	3
1	34+00 - 35+00	1
1	39+00 - 40+00	1
1	41+00 - 42+00	1
1	43+00 - 45+00	2

### GRUBBING

SEC. NO.	STATION TO STATION	STA.
1	0+00 - 2+00	2
1	23+00 - 26+00	3
1	29+00 - 31+00	2
1	34+00 - 35+00	1
1	39+00 - 40+00	1
1	41+00 - 42+00	1
1	43+00 - 44+00	1

### RIPRAP

SEC. NO.	STATION	LOCATION	C.Y.
1	NORTH ABUTE. B-32-4		145
1	28+27	DOWN DRAIN RT.	1
1	28+53	DOWN DRAIN LT.	1

### GROUTED RIPRAP

STA 27+73	12' RT.	=	5.5 C.Y.
STA 27+98	12' LT.	=	5.5 C.Y.

### STEEL PLATE BEAM GUARD

SEC. NO.	STATION TO STATION	LOCATION	LENGTH	REMARKS
1	24+36 - 25+51	RT.	54'-9"	SEE DETAIL LAYOUT
1	25+72 - 25+82	RT.	29'-9"	SEE DETAIL LAYOUT
1	28+20 - 29+48	RT.	128'	
1	28+46 - 29+49	RT.	103'	

### CONCRETE SURFACE DRAINS

SEC. NO.	STATION	LOC.	CONC.	BAR STEEL
1	28+27	RT.	12	
1	28+53	RT.	12	

APPLICABLE DETAIL DRAWINGS

- CONCRETE CURB & GUTTER 5-1.1.3
- CONCRETE SURFACE DRAIN AT BRIDGES 5-2.2.1
- INLETS 5-3.5.1
- INLET COVERS 5-3.6.1
- MARKER POSTS 7-1.3.1
- CLASS "B" STEEL PLATE BEAM GUARD 7-3.4.1
- CONSTRUCTION BARRICADE 7-4.1.1

### PIPE CULVERTS

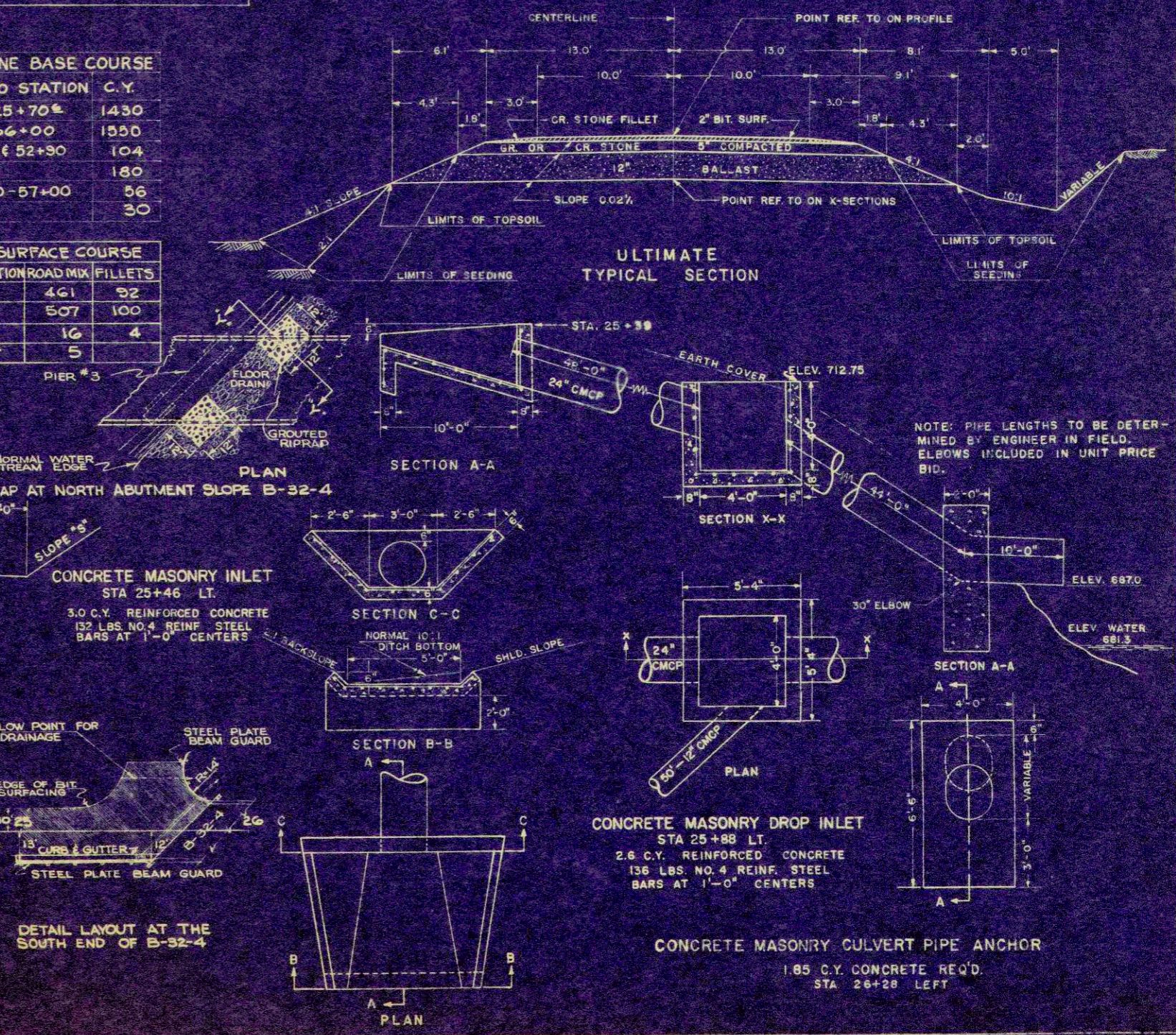
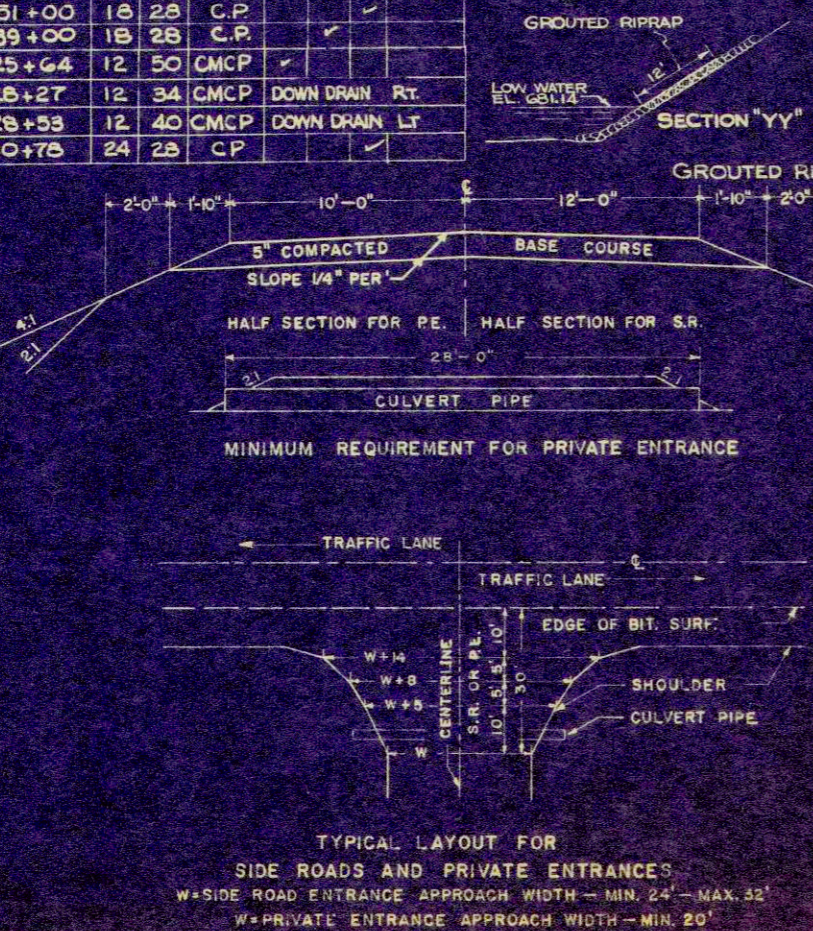
SEC. NO.	STATION	DIAM. INCHES	LENGTH LIN. FT.	TYPE	PE & SR	MARKERS
1	8+30	18	28	C.P.		
1	12+00	24	56	C.P.		2
1	15+65	24	52	C.P.		2
1	22+50	18	28	C.P.		
1	25+00	24	48	C.P.		2
1	25+38 - 25+86	24	48	CMCP		DITCH DRAIN LT.
1	25+90 - 26+38	24	54	CMCP		DOWN DRAIN LT. INCLUDING 1-90° ELBOW
1	32+20	24	36	C.P.		
1	39+50	18	28	C.P.		
1	44+65	30	92	C.P.		2
1	49+30	36	68	C.P.		2
1	52+90	24	36	C.P.		
1	30+78	24	28	C.P.		
1	51+00	18	28	C.P.		
1	39+00	18	28	C.P.		
1	25+64	12	50	CMCP		
1	28+27	12	34	CMCP		DOWN DRAIN RT.
1	28+53	12	40	CMCP		DOWN DRAIN LT.
1	30+78	24	28	C.P.		

### GR. OR CR. STONE BASE COURSE

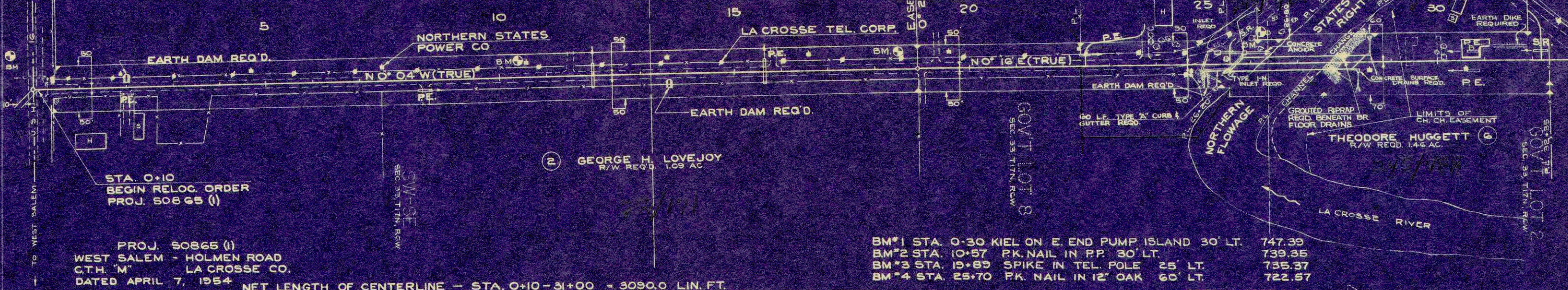
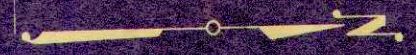
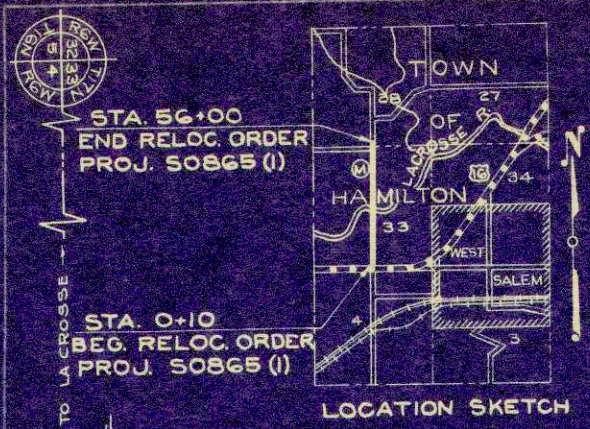
SEC. NO.	STATION TO STATION	C.Y.
1	0+10 - 25+70	1430
1	28+31 - 56+00	1550
1	PE'S	180
1	APP. 56+00 - 57+00	56
1	25+56	30

### AGGREGATES FOR BIT. SURFACE COURSE

STATION	LOCATION	ROAD MIX	FILLETS
0+10 - 25+70	E	461	92
28+31 - 56+00	E	507	100
APP. 56+00 - 57+00	E	16	4
25+56	LT	5	



S.P.R. DIVISION	PROJECT	SHEET NUMBER	TOTAL SHEETS
34	50865 (I)	3	33



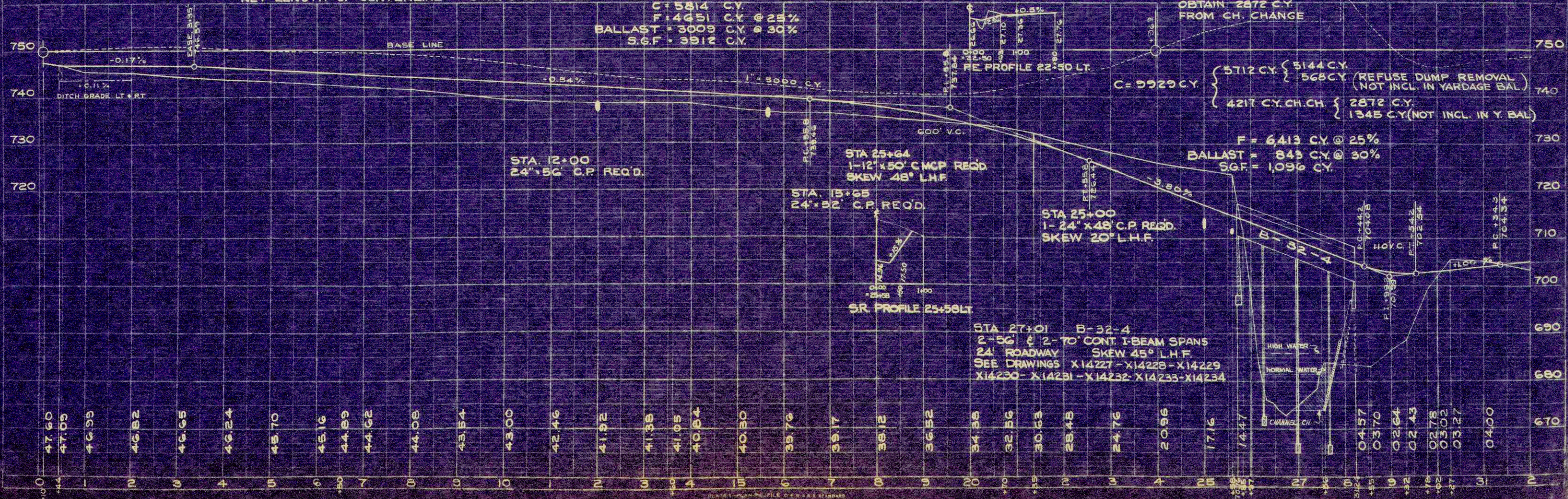
PROJ. 50865 (I)  
 WEST SALEM - HOLMEN ROAD  
 C.T.H. "M" LA CROSSE CO.  
 DATED APRIL 7, 1954

BM\*1 STA. 0+30 KIEL ON E. END PUMP ISLAND 30' LT. 747.39  
 BM\*2 STA. 10+57 P.K. NAIL IN P.P. 30' LT. 739.35  
 BM\*3 STA. 19+89 SPIKE IN TEL. POLE 25' LT. 735.37  
 BM\*4 STA. 25+70 P.K. NAIL IN 12" OAK 60' LT. 722.57

NET LENGTH OF CENTERLINE - STA. 0+10 - 31+00 = 3090.0 LIN. FT.

C = 5814 C.Y.  
 F = 4451 C.Y. @ 25%  
 BALLAST = 3009 C.Y. @ 30%  
 S.G.F. = 3912 C.Y.

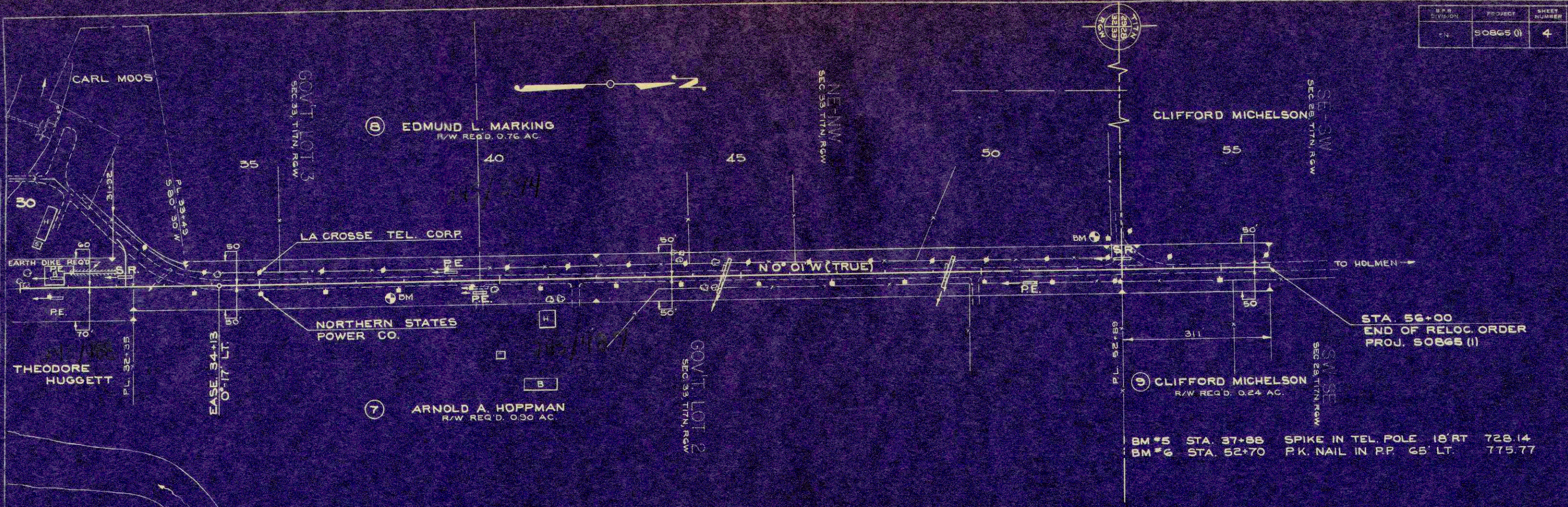
C = 9929 C.Y.  
 F = 6413 C.Y. @ 25%  
 BALLAST = 849 C.Y. @ 30%  
 S.G.F. = 1096 C.Y.



PLAN	DATE	BY	CHECKED

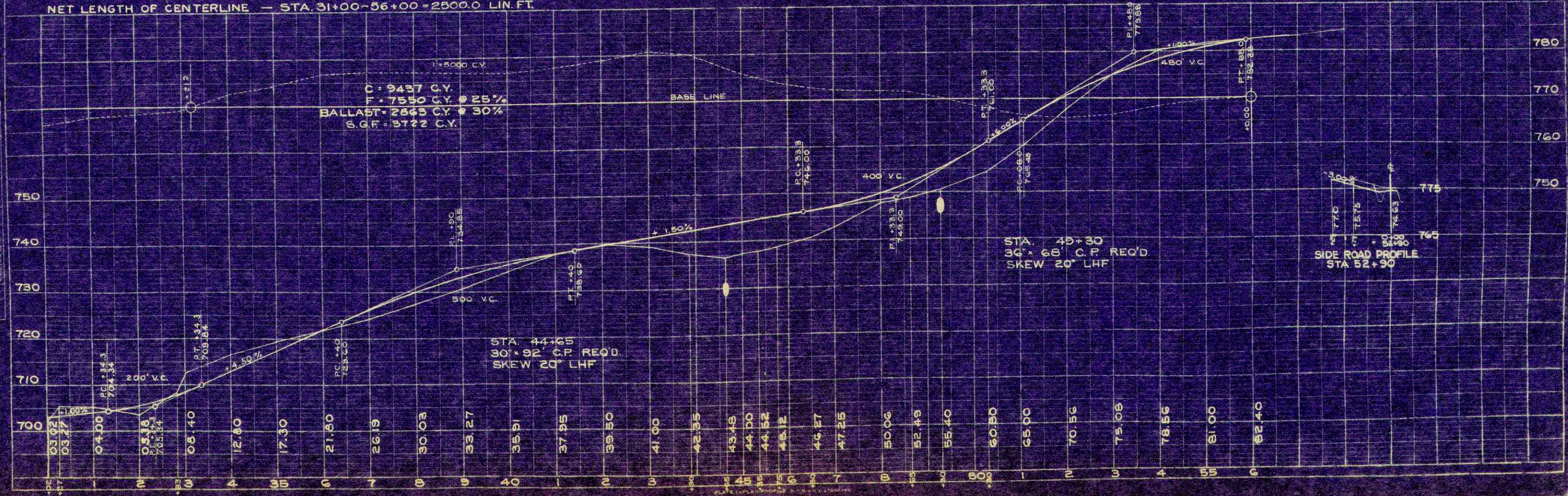
F.C.G. 4-55

TTU



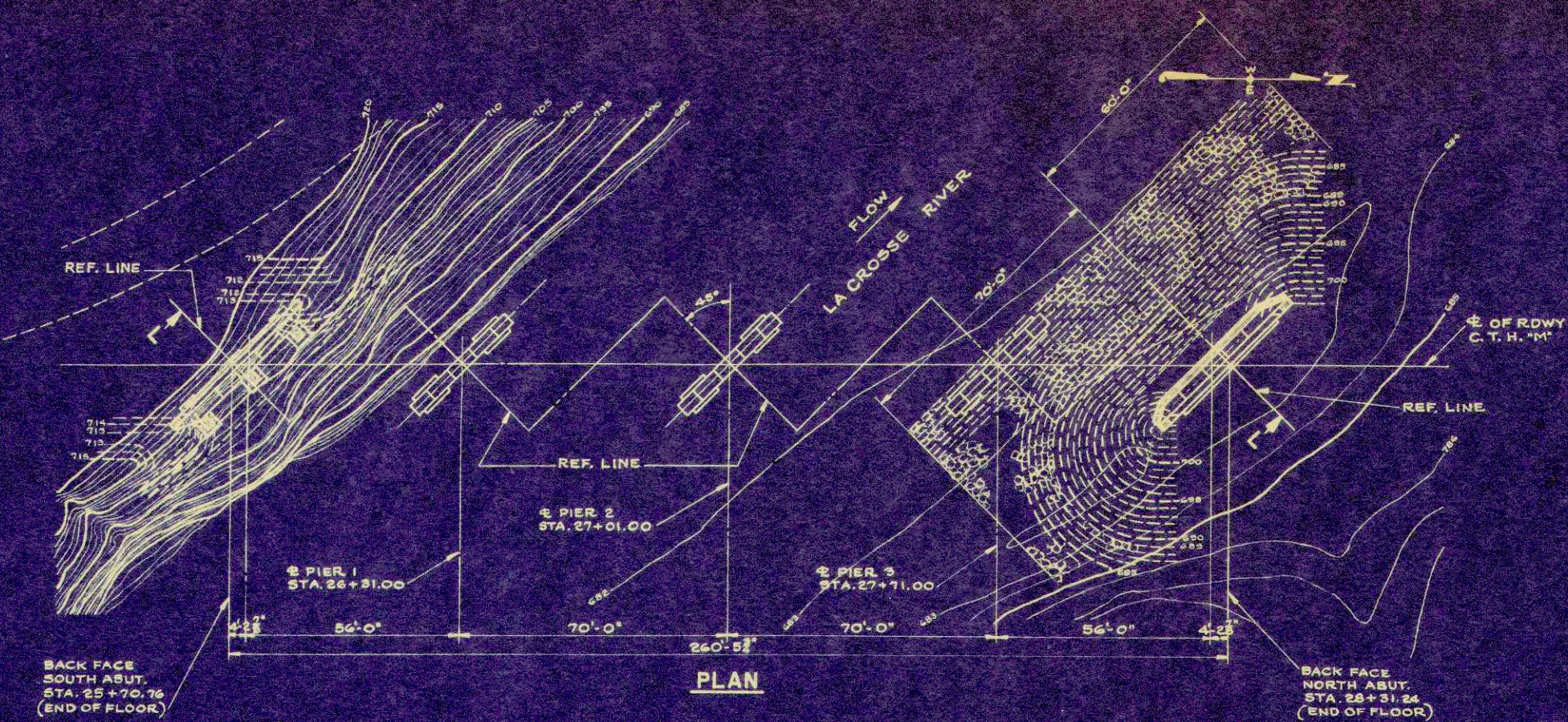
BM #5 STA. 37+88 SPIKE IN TEL. POLE 18' RT 728.14  
 BM #6 STA. 52+70 P.K. NAIL IN P.P. 65' LT. 775.77

NET LENGTH OF CENTERLINE — STA. 31+00-56+00 = 2500.0 LIN. FT.



DATE	BY

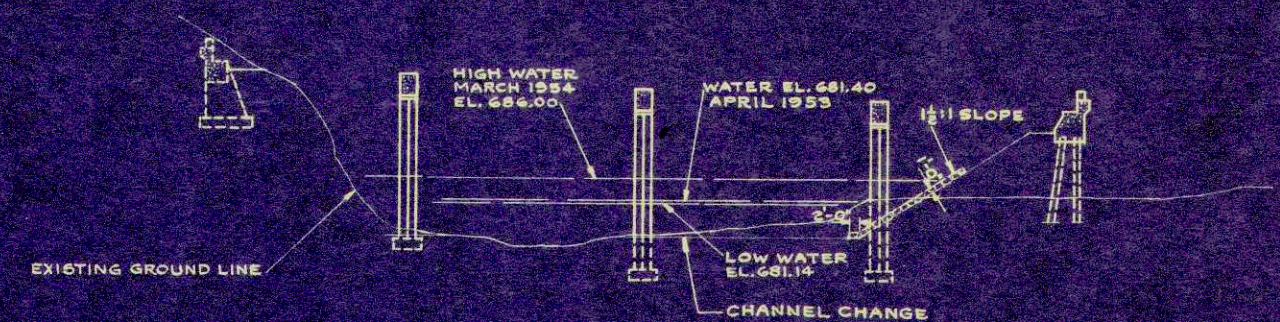
PROFILE	MARKED	DATE
NOTED		



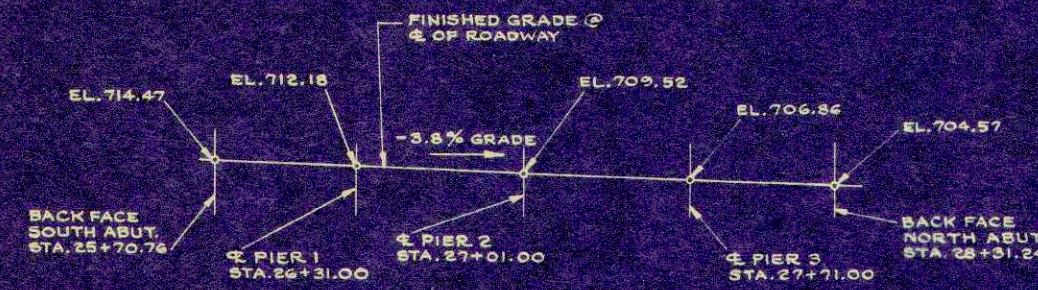
**PLAN**

**GENERAL NOTES**

DRAWINGS SHALL NOT BE SCALED.  
 ALL CONCRETE MASONRY SHALL BE GRADE "AA".  
 BEVEL EXPOSED EDGES OF CONCRETE 1" UNLESS OTHERWISE SPECIFIED.  
 BAR STEEL REINFORCEMENT SHALL BE IMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.  
 INTERFERING FLOOR STEEL SHALL BE CUT OFF AT FLOOR DRAINS.  
 ALL FIELD CONNECTIONS SHALL BE MADE WITH 3/4" RIVETS, UNLESS OTHERWISE SPECIFIED.  
 PILES FOR NORTH ABUTMENT SHALL BE TREATED TIMBER PILES 30'-0" LONG AND SHALL BE DRIVEN TO A MINIMUM BEARING VALUE OF 16 TONS PER PILE.  
 THE SLOPE OF THE BERM AT THE FRONT FACE OF NORTH ABUTMENT SHALL BE RIPRAPPED AS SHOWN IN "SECTION B1" ON SHEET X14233 AND TO THE EXTENT SHOWN IN "PLAN" AND "SECTION L" ON THIS SHEET, TO A MINIMUM THICKNESS OF 1'-0".  
 THE FOOTINGS OF SOUTH ABUTMENT AND PIERS 1, 2 AND 3 TO BE KEYED A MINIMUM OF 1'-0" INTO ROCK.



**SECTION L**



**GRADE DIAGRAM**

**LIST OF DRAWINGS**

1- GENERAL PLAN	X14227
2- SUPERSTRUCTURE	X14228
3- SUPERSTRUCTURE	X14229
4- SUPERSTRUCTURE	X14230
5- SOUTH ABUTMENT	X14231
6- PIERS	X14232
7- NORTH ABUTMENT	X14233
8- BILL OF BARS	X14234

**TOTAL ESTIMATED QUANTITIES**

BID ITEMS	UNIT	SUPER-STRUCTURE	SOUTH ABUTMENT	PIER 1	PIER 2	PIER 3	NORTH ABUTMENT	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.	—	275	45	65	125	30	540
CONCRETE MASONRY	C.Y.	194.2	68.2	66.7	70.4	67.4	59.5	526.4
BAR STEEL REINFORCEMENT	LB.	47,270	2,540	3,380	3,510	3,380	1,560	61,640
STRUCTURAL CARBON STEEL	LB.	185,550	—	490	490	490	—	187,020
SHEET LEAD	LB.	345	—	—	—	—	—	345
UTREATED TIMBER TEST PILING (1 @ 43)	LUMP SUM	—	—	—	—	—	—	1
TREATED TIMBER PILING - DELIVERED	L.F.	—	—	—	—	—	480	480
TREATED TIMBER PILING - DRIVEN	L.F.	—	—	—	—	—	480	480
FLOOR DRAINS	EACH	8	—	—	—	—	—	8
RIPRAP	C.Y.	—	—	—	—	—	145	145
PILE SHOES	EACH	—	—	—	—	—	16	16

STATE HIGHWAY COMMISSION OF WISCONSIN

**GENERAL PLAN**

LA CROSSE HAMILTON STA. 27+01.00

SECTION 33 TURN 17. N. RANGE 6 W.

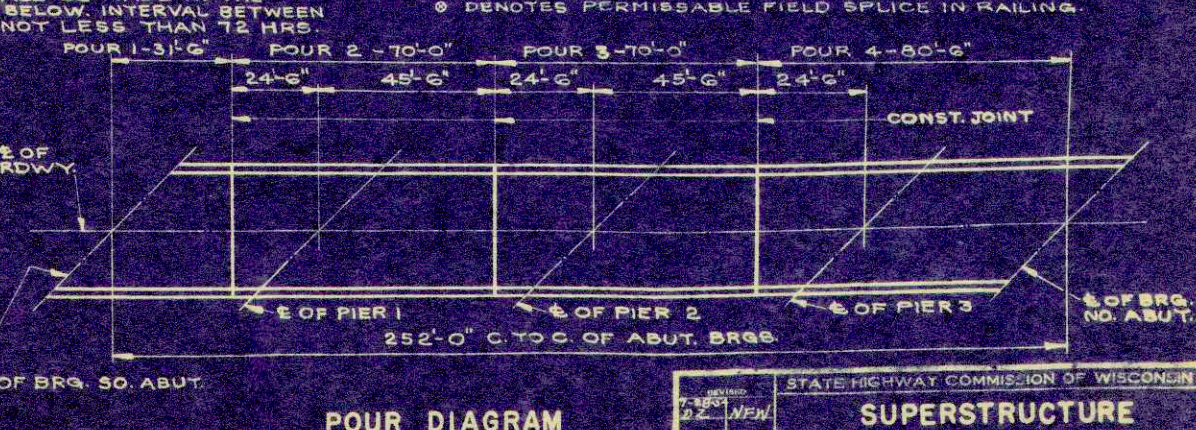
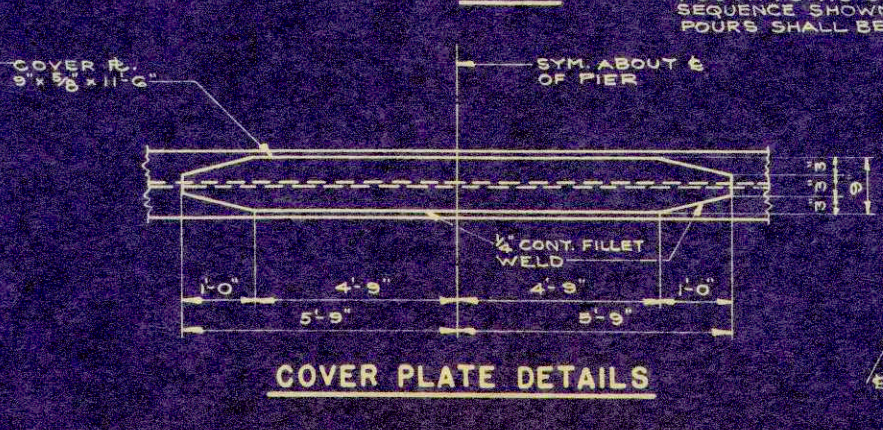
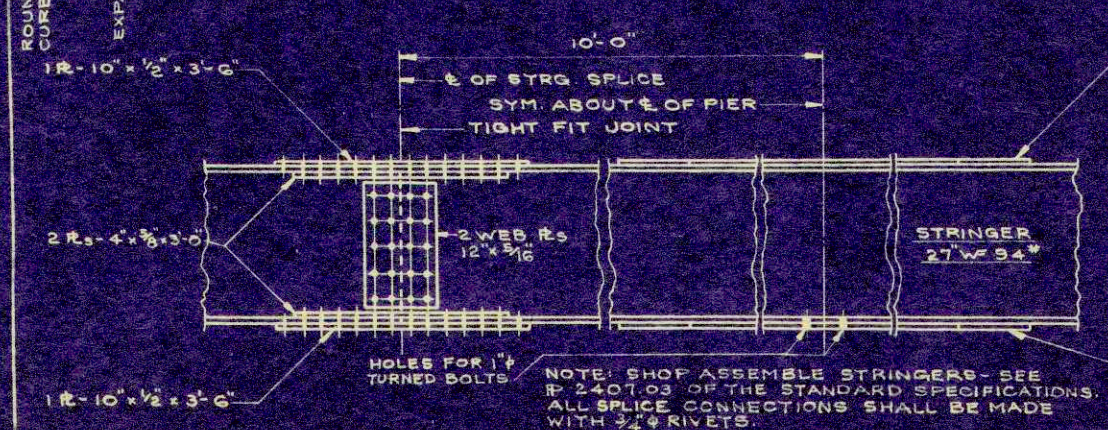
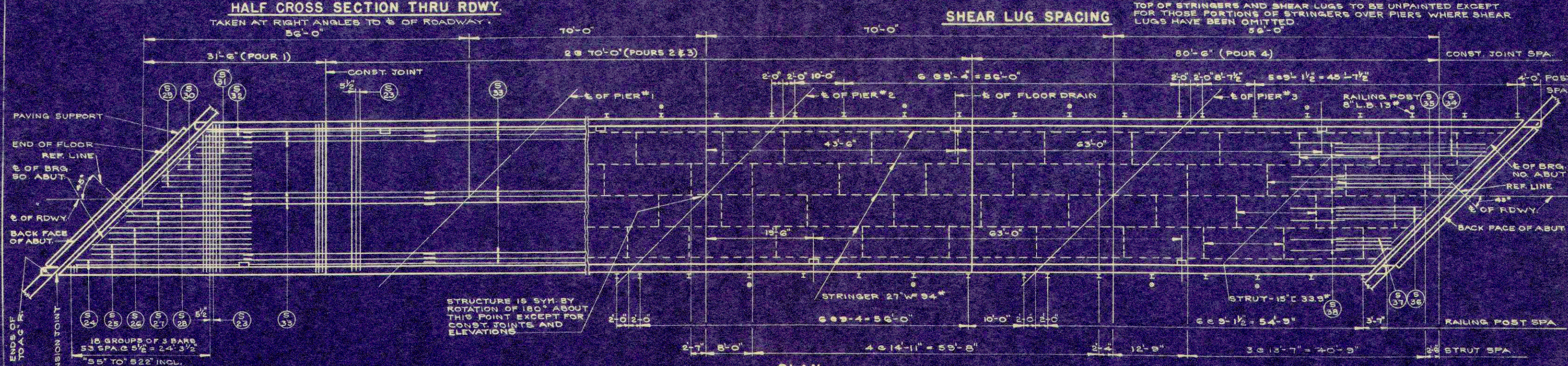
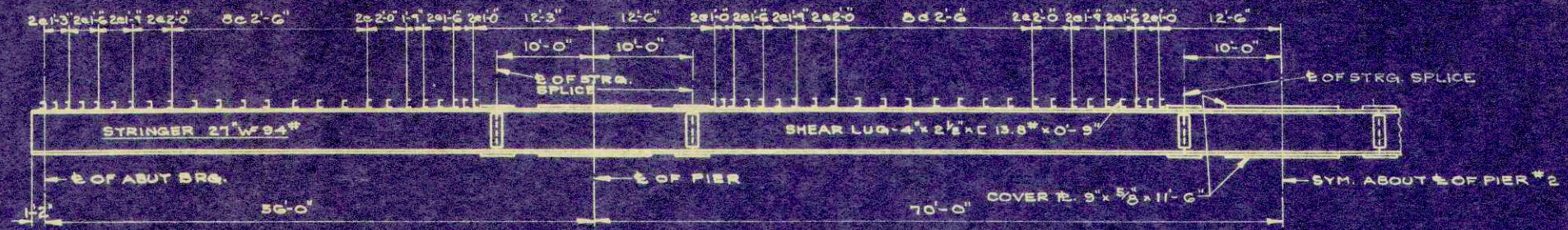
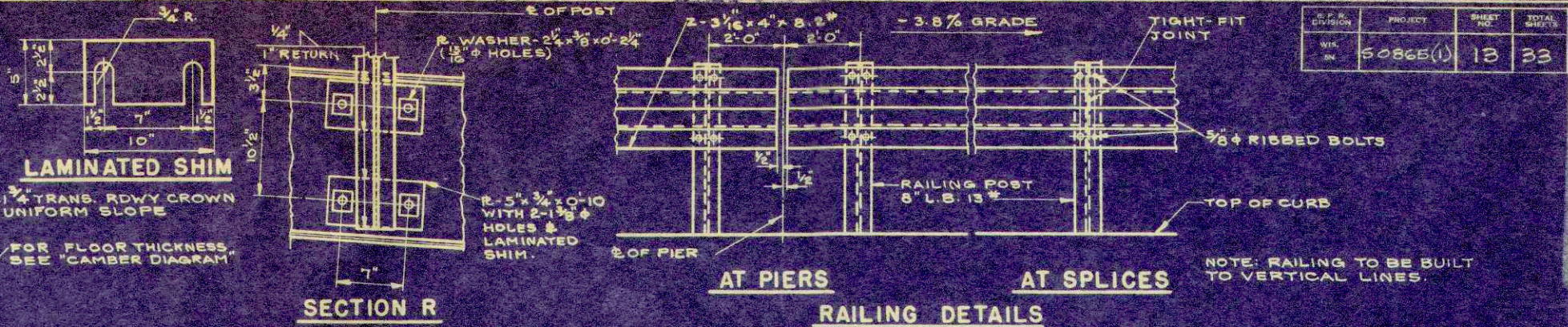
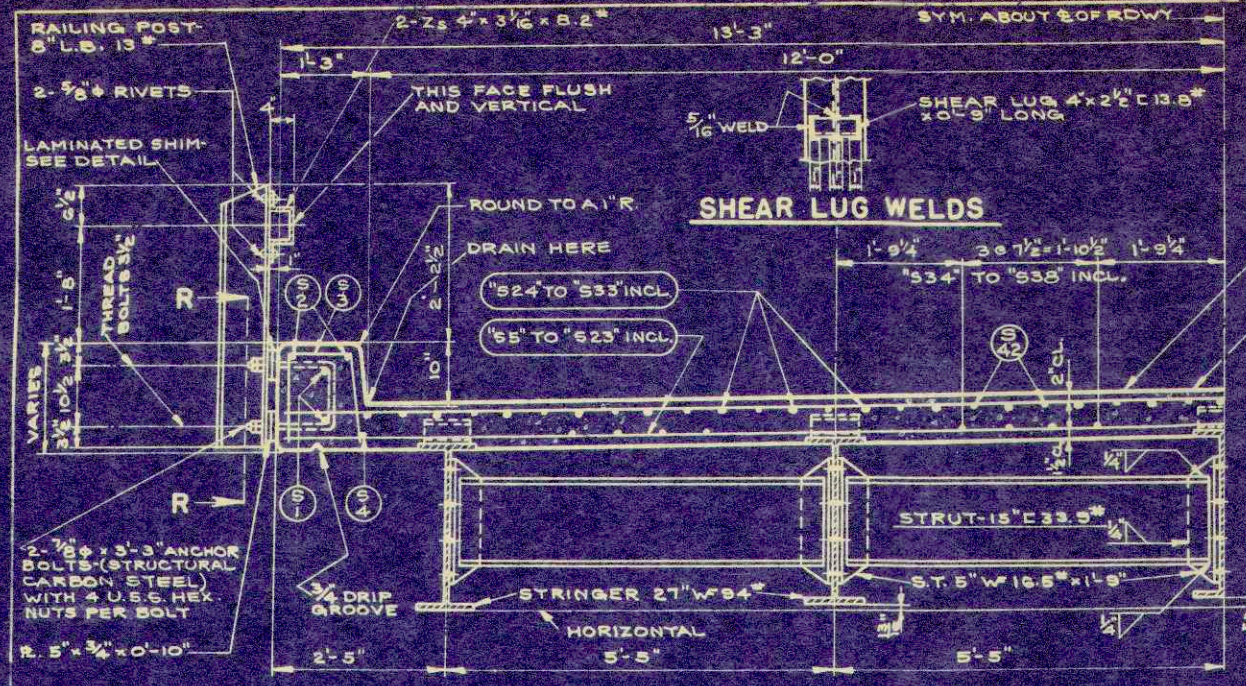
DATED 1954 BY P.A. Madsen, N.F.W. 1951 Lada H 15

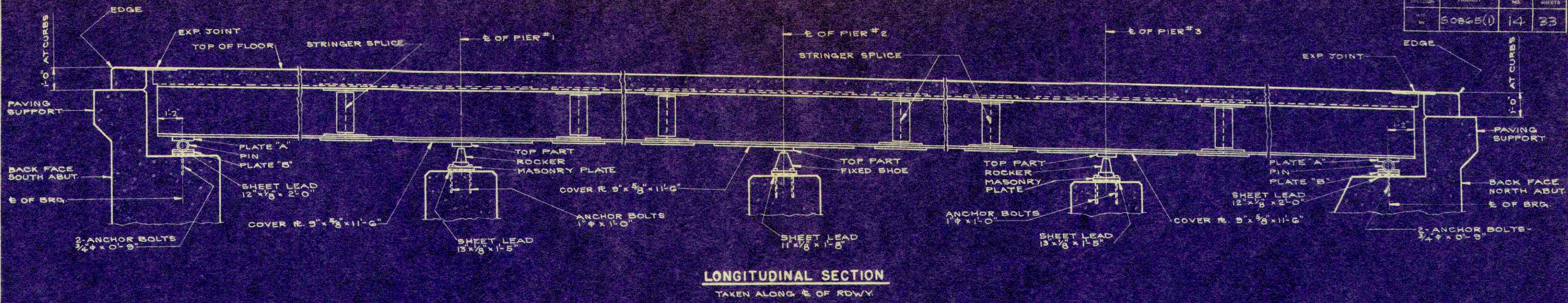
BRIDGE ENGINEER: *C.H. Kuhl*

STATE HIGHWAY ENGINEER: *P. J. ...*

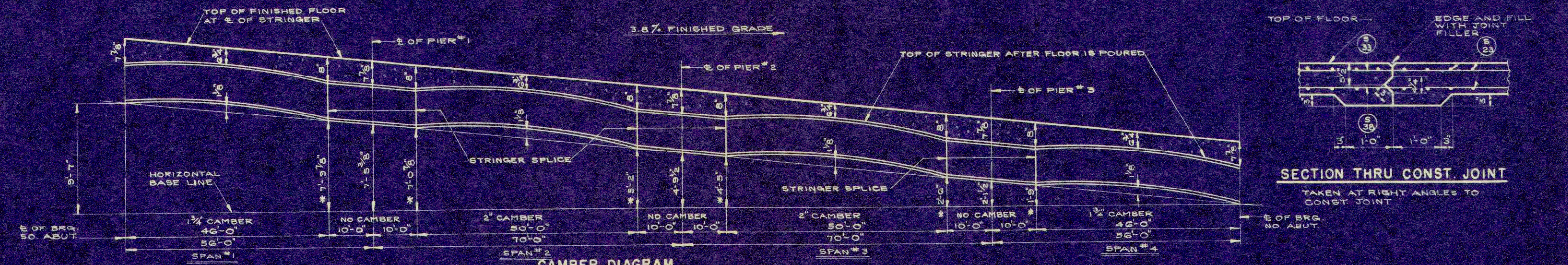
STRUCTURE: B-32-4 SHEET 1 OF 8

X14227





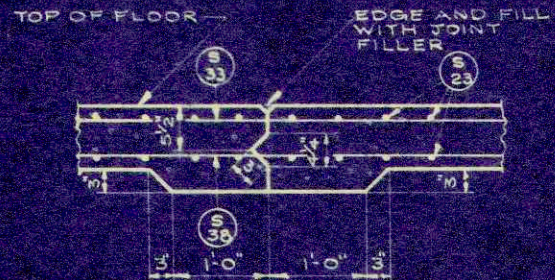
**LONGITUDINAL SECTION**  
TAKEN ALONG E OF RDWY.



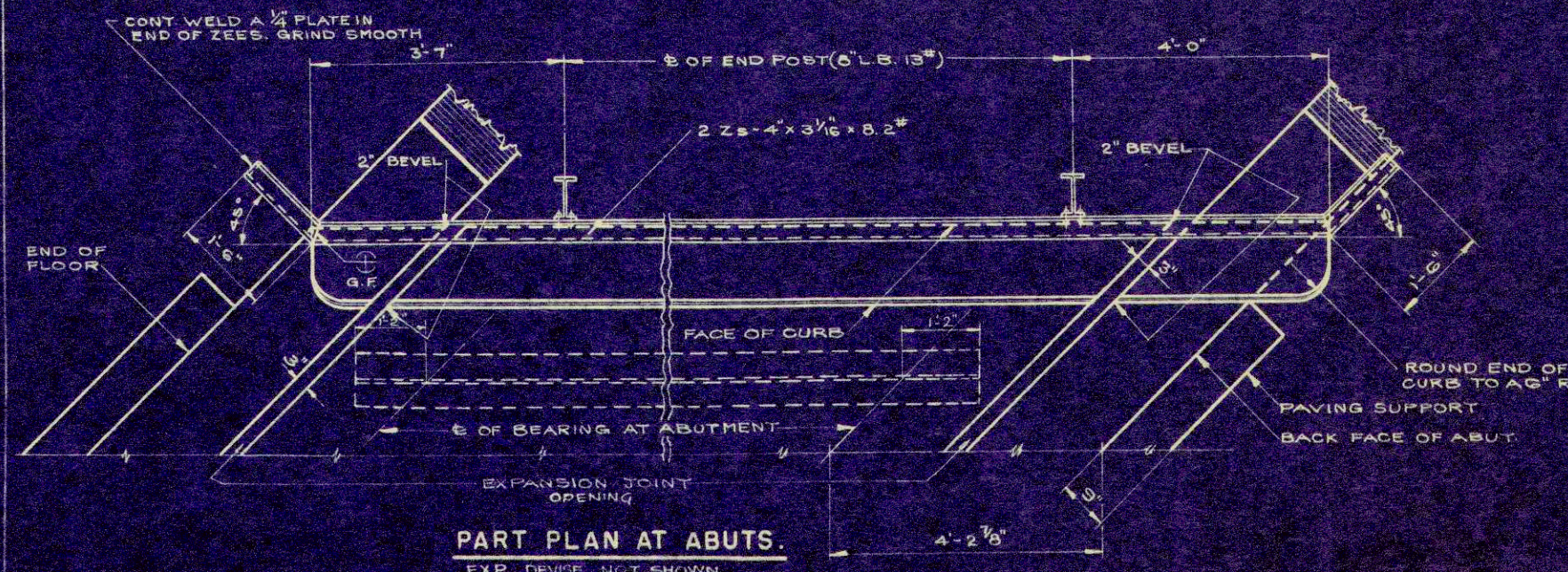
**CAMBER DIAGRAM**

CAMBER AS CALLED FOR ABOVE IS BEFORE ERECTION AND SHALL CONFORM TO AN ARC OF A CIRCLE.

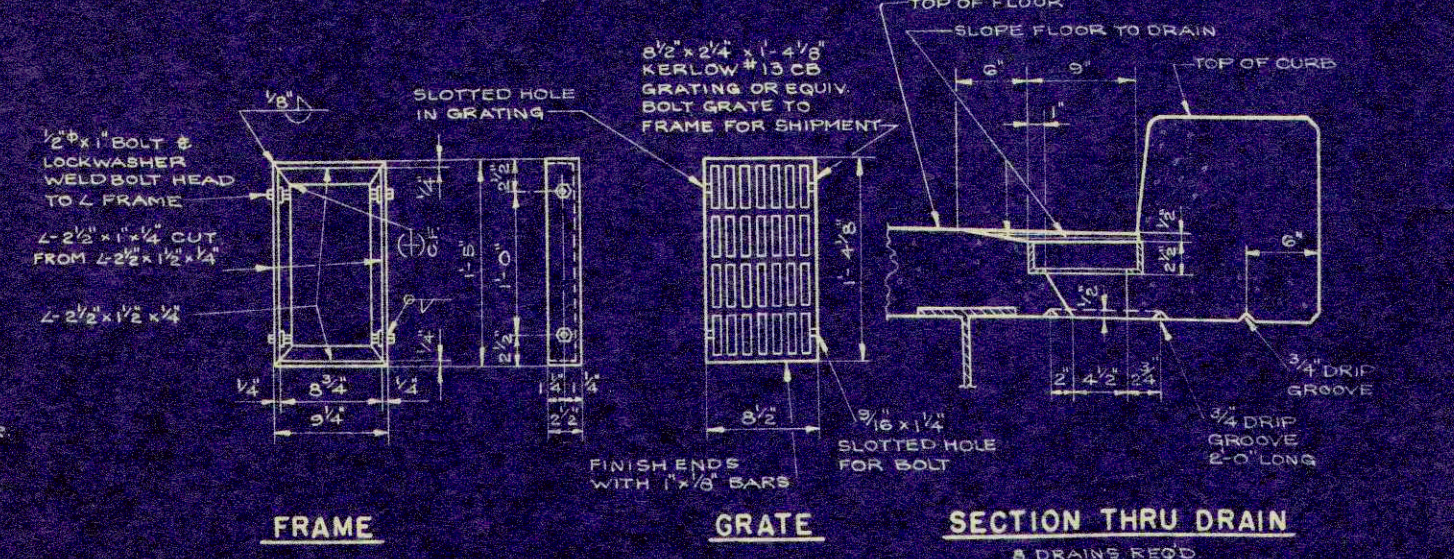
\* BLOCKING DIMENSIONS WITH OUT DEAD LOAD.



**SECTION THRU CONST. JOINT**  
TAKEN AT RIGHT ANGLES TO CONST JOINT



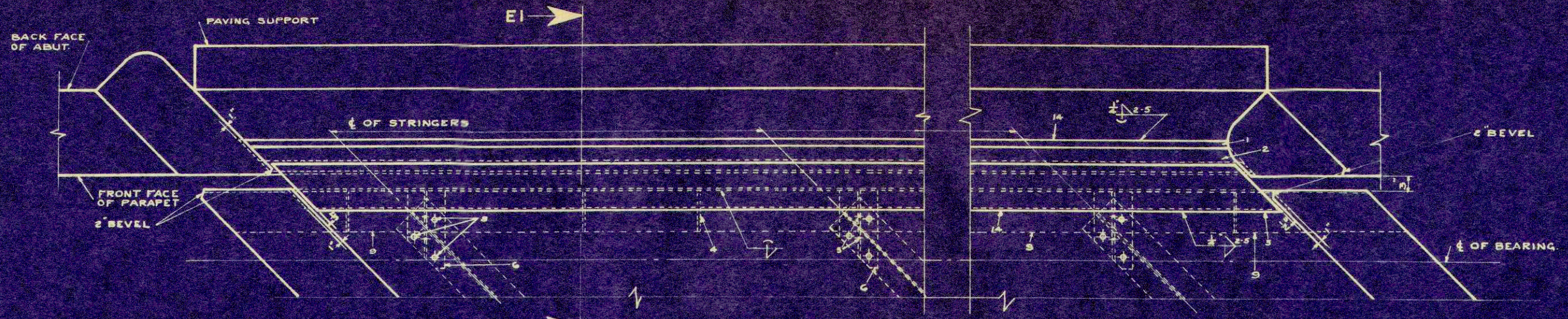
**PART PLAN AT ABUTS.**  
EXP. DEVICE NOT SHOWN



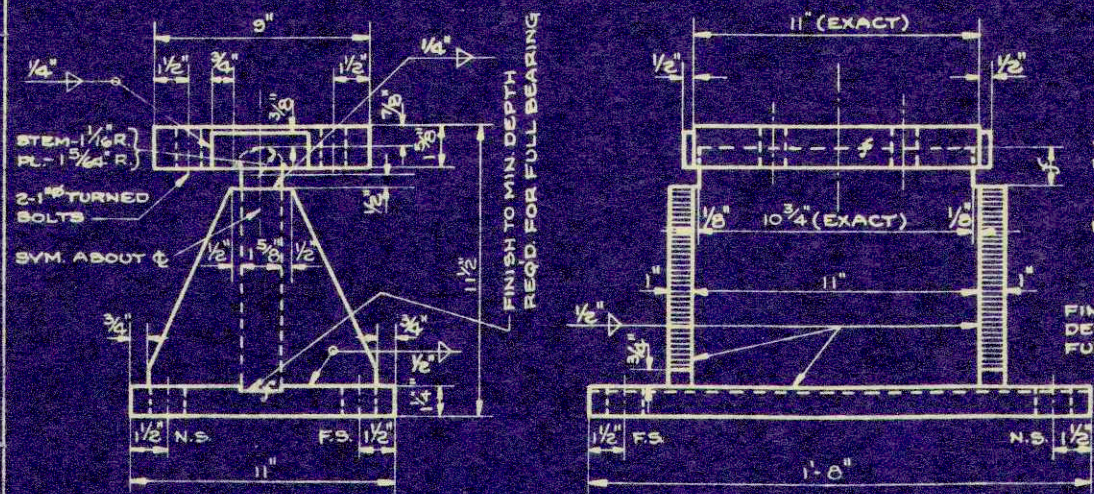
**FRAME**

**GRATE**

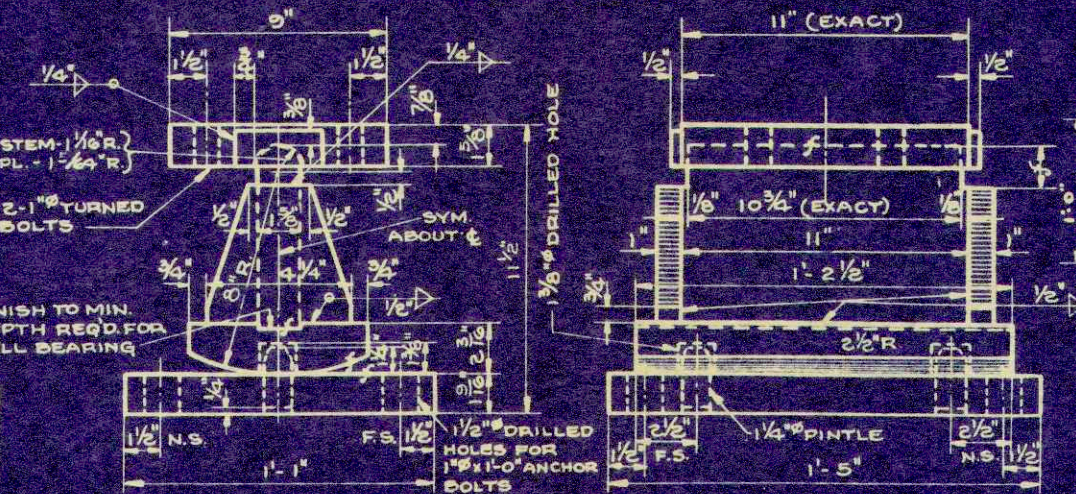
**SECTION THRU DRAIN**  
& DRAINS REQ'D



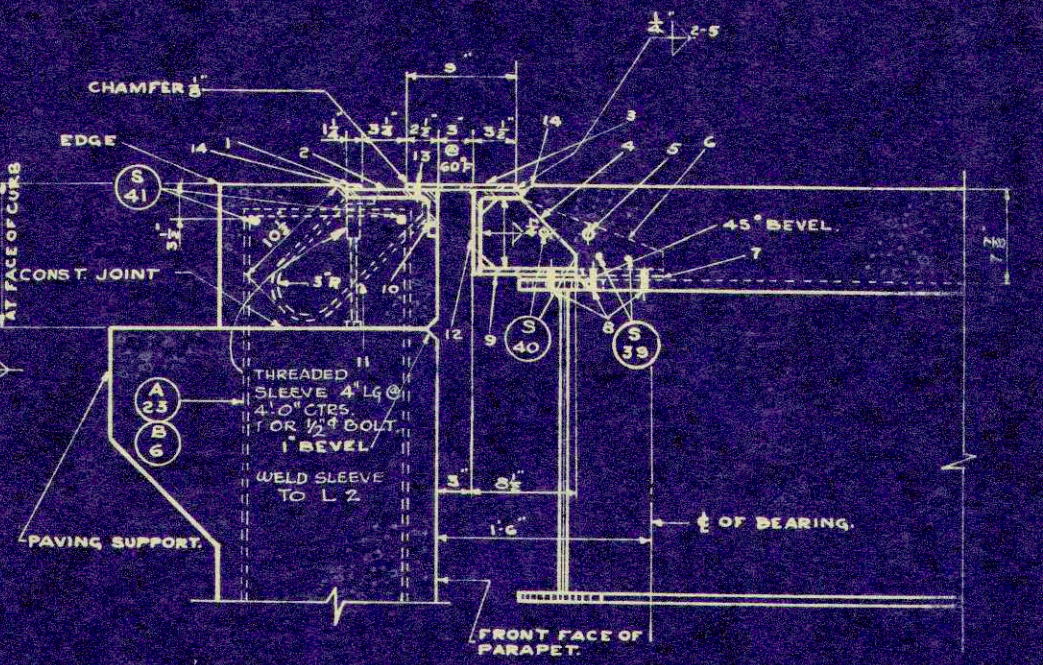
**PART PLAN AT ABUTMENTS**  
SHOWING EXPANSION JOINT.



**FIXED SHOE BEARING**  
WT. 218 EACH - 5 REQ'D.



**ROCKER BEARING**  
WT. 258 EACH - 10 REQ'D.



**SECTION E1**

**BEARING NOTES**

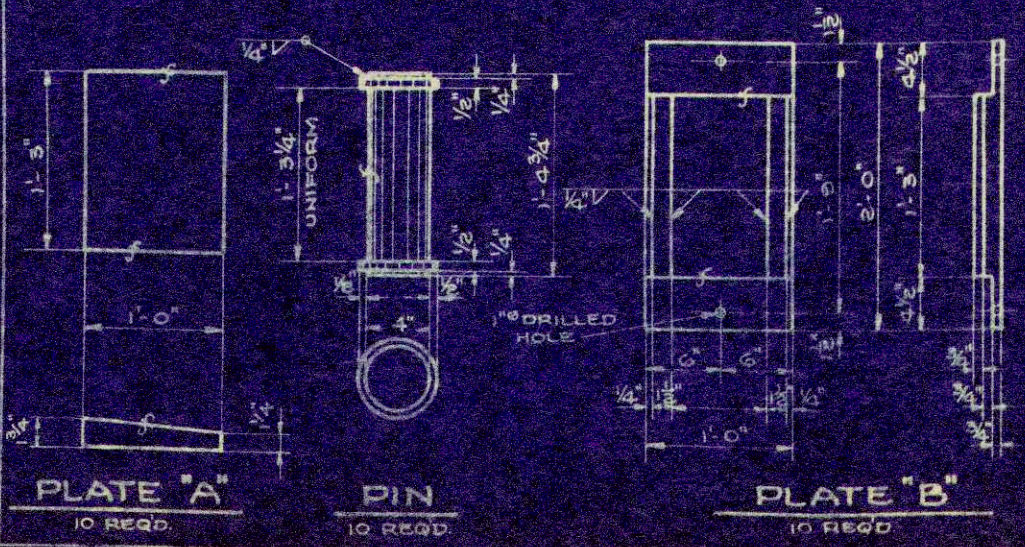
ALL STRUCTURAL CARBON STEEL BEARING PLATES SHALL BE FLAT ROLLED PLATES WITH ALL SURFACES SMOOTH & FREE FROM WARP & ALL EDGES SMOOTH, STRAIGHT & VERT. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE-FLAME CUTS. ALL SURFACES MARKED 'F' SHALL BE MACHINE FINISHED. PAY QUANTITY FOR SIDE PLATES ON BEARINGS WILL BE BASED ON ACTUAL SIZE. PINTLES SHALL BE MACHINED TO A DRIVING FIT. BOLT TOP PLATE TO I-BEAM FOR SHIPMENT. THREAD ANCHOR BOLT 3". PROVIDE ONE 1/4" WASHER & ONE HEX NUT PER ANCHOR BOLT. WT. OF BEARINGS AS GIVEN ABOVE SHALL INCLUDE TURNED BOLTS, ANCHOR BOLTS & PINTLES.

**EXPANSION JOINT NOTES**

ROADWAY EXPANSION JOINTS SHALL BE BUILT TO ROADWAY CROWN AND GRADE. ALL PLATE CUTS SHALL BE MACHINE-FLAME CUTS AND ALL ROUGH EDGES AND SURFACES SHALL BE GROUND SMOOTH.

**MAKE-UP**

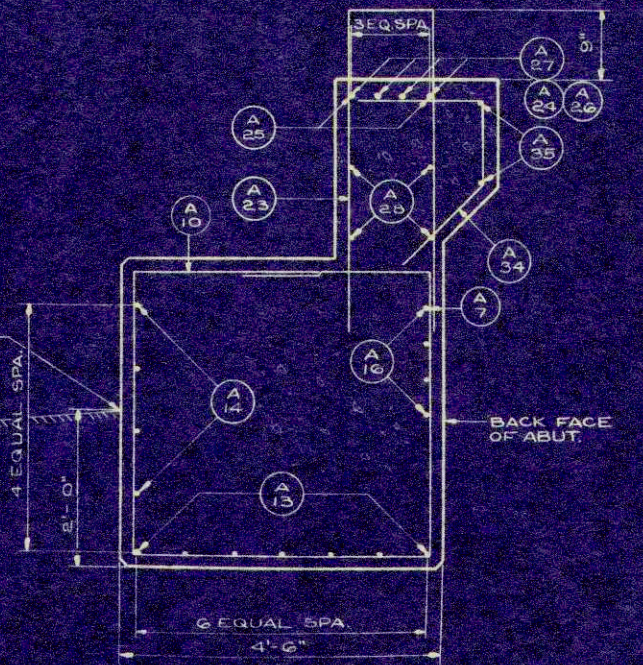
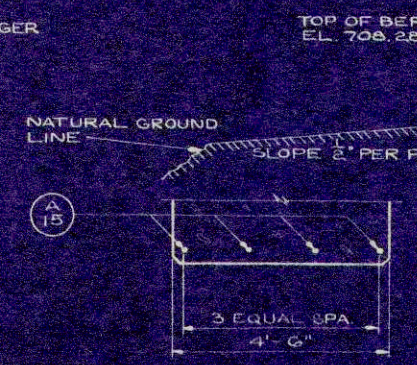
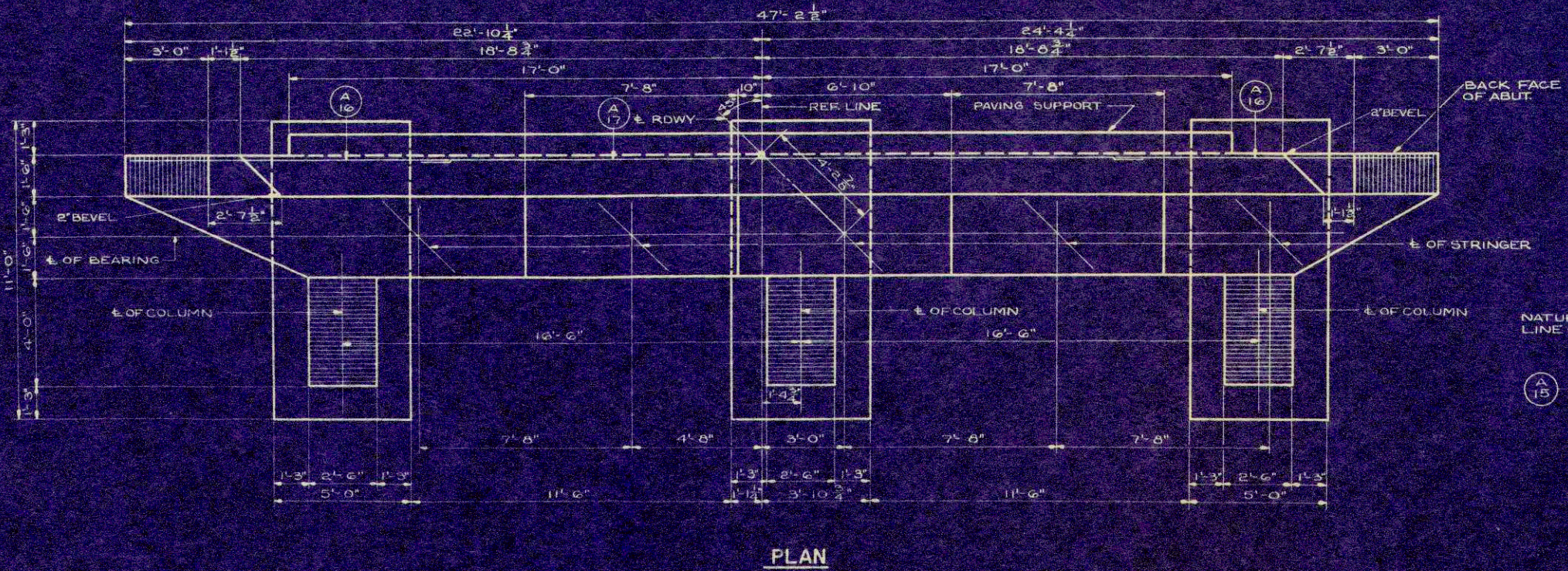
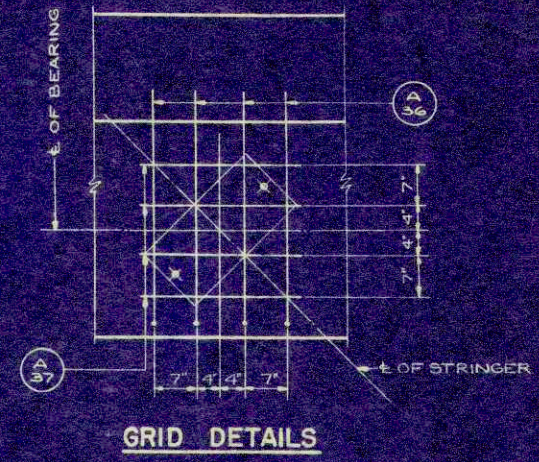
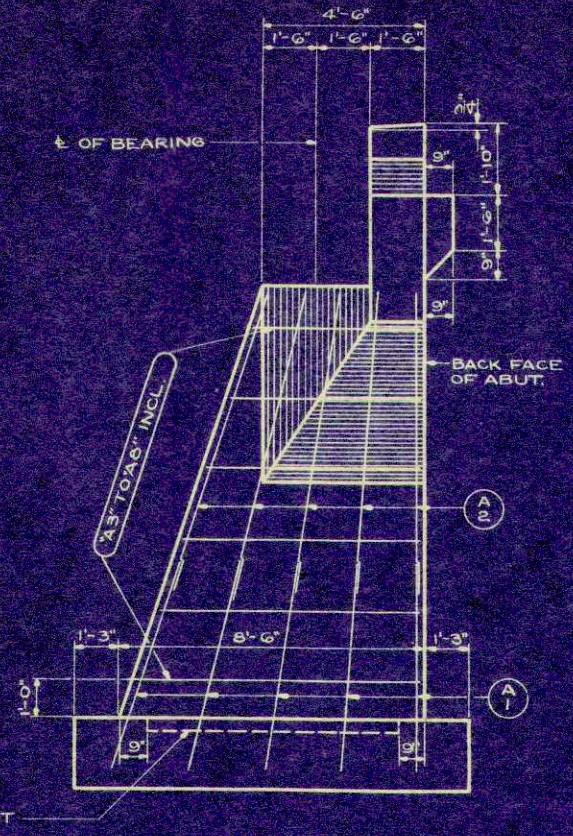
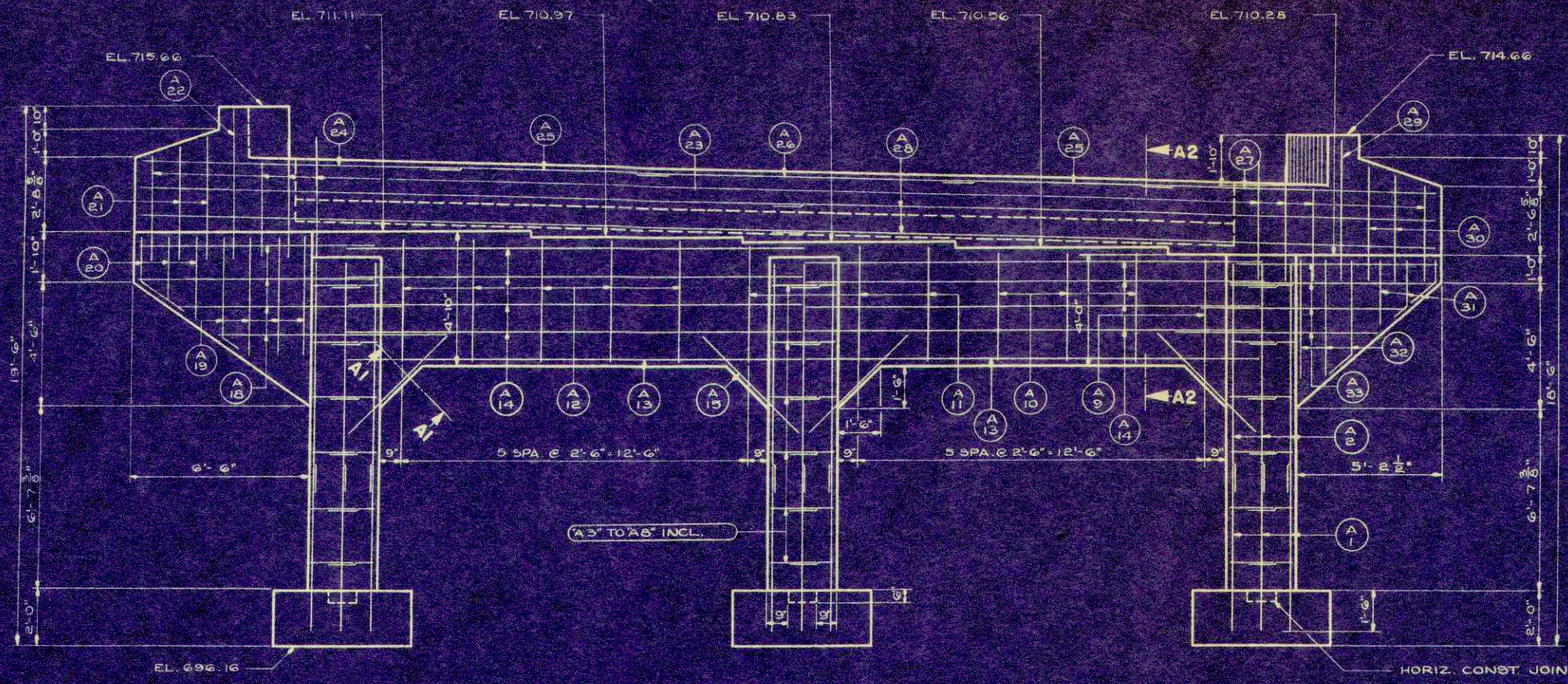
- 1 - BAR 12 x 1/2
- 2 - L 7 x 4 x 1/2
- 3 - PL 9 x 1/2
- 4 - 3/16" @ 2'-0" CENTERS BETWEEN STRINGERS.
- 5 - 1/2" HOLE, WELDS 40 BAR TO STRUCTURAL TEE. (FIELD WELD.)
- 6 - STRUCTURAL TEE 7" WF 24" AT STRINGERS
- 7 - LAMINATED BEVELED SHIM, 1/2" MINIMUM THICKNESS.
- 8 - 3/8" RIBBED BOLTS, BEVELED WASHERS IF STRINGER FLANGE IS BEVELED. DRILL STRINGERS HOLES IN FIELD.
- 9 - PL 8 x 1/2
- 10 - 1/2" @ OPEN HOLES @ 3'-0" CENTERS.
- 11 - ANCHORS 3" @ 3'-0" @ 1'-6" CENTERS. 1/2" CONTINUOUS WELD TO 1.
- 12 - L 7 x 4 x 1/2
- 13 - 3/8" @ BOLTS @ 4'-0" CENTERS. HEADS UP. REMOVE IMMEDIATELY AFTER L IS FASTENED TO FORMS.
- 14 - ROUND WITH EDGING TOOL AND FILL WITH JOINT FILLER.





NOTE: ALL ELEVATIONS AND DIMENSIONS TAKEN AT B.F. OF ABUT.

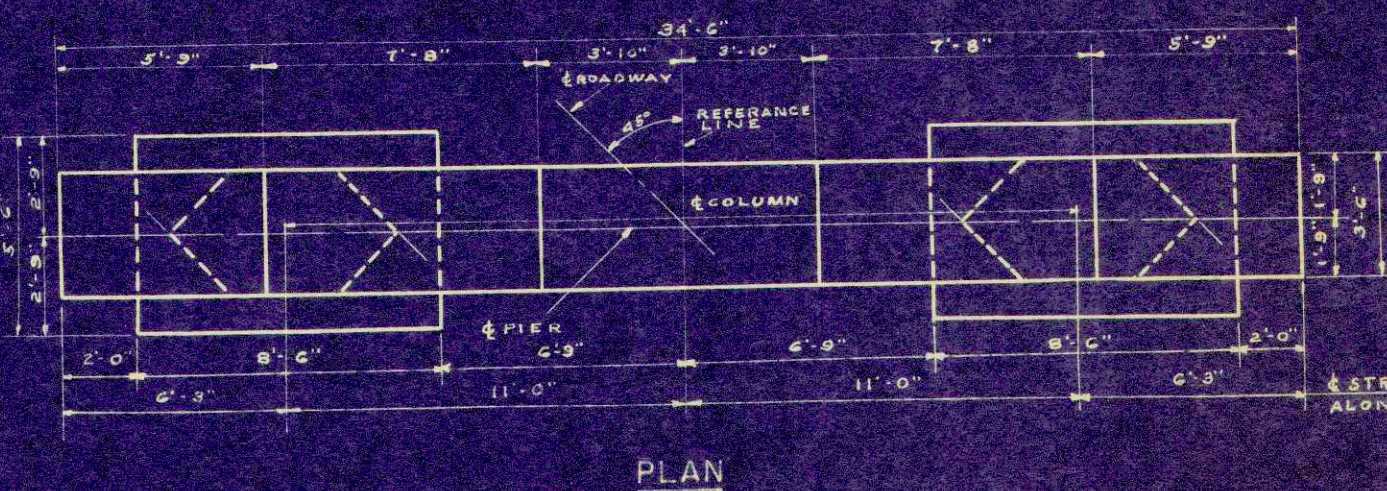
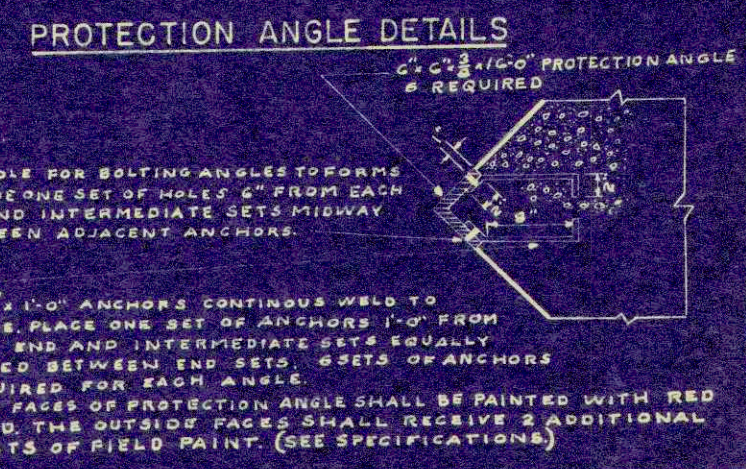
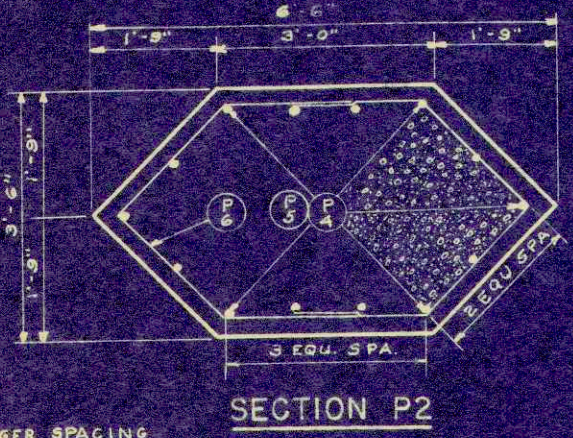
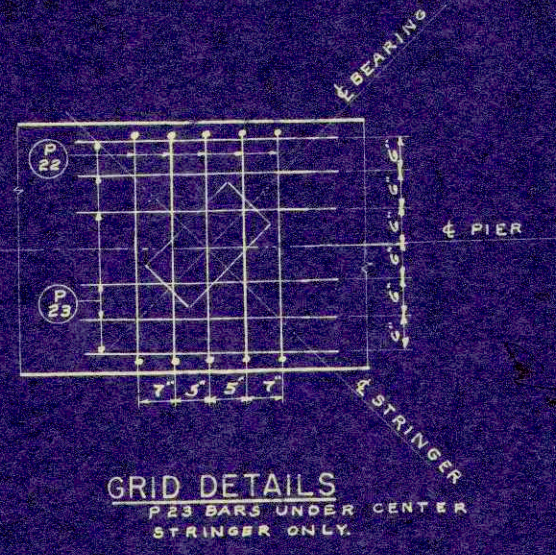
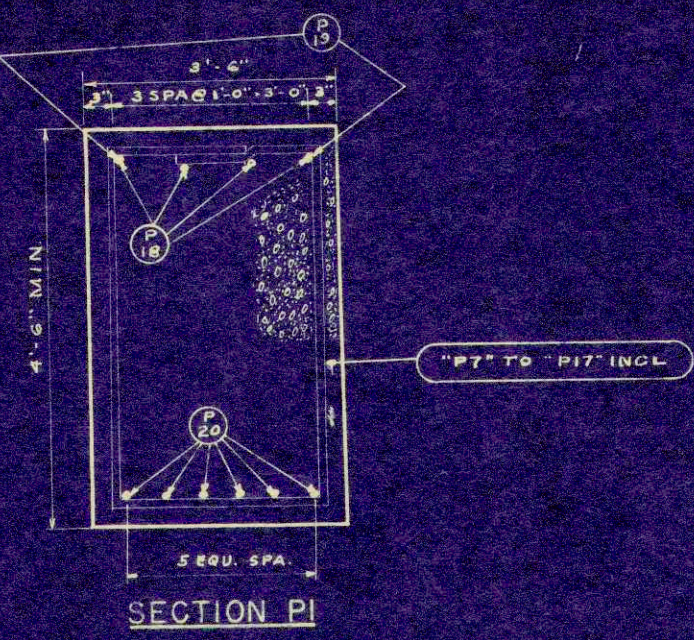
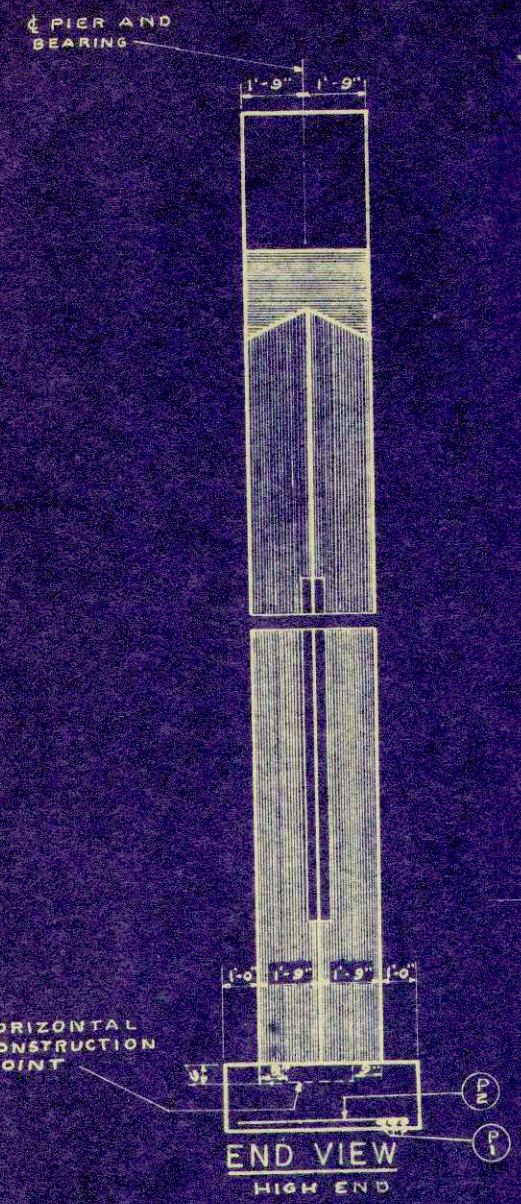
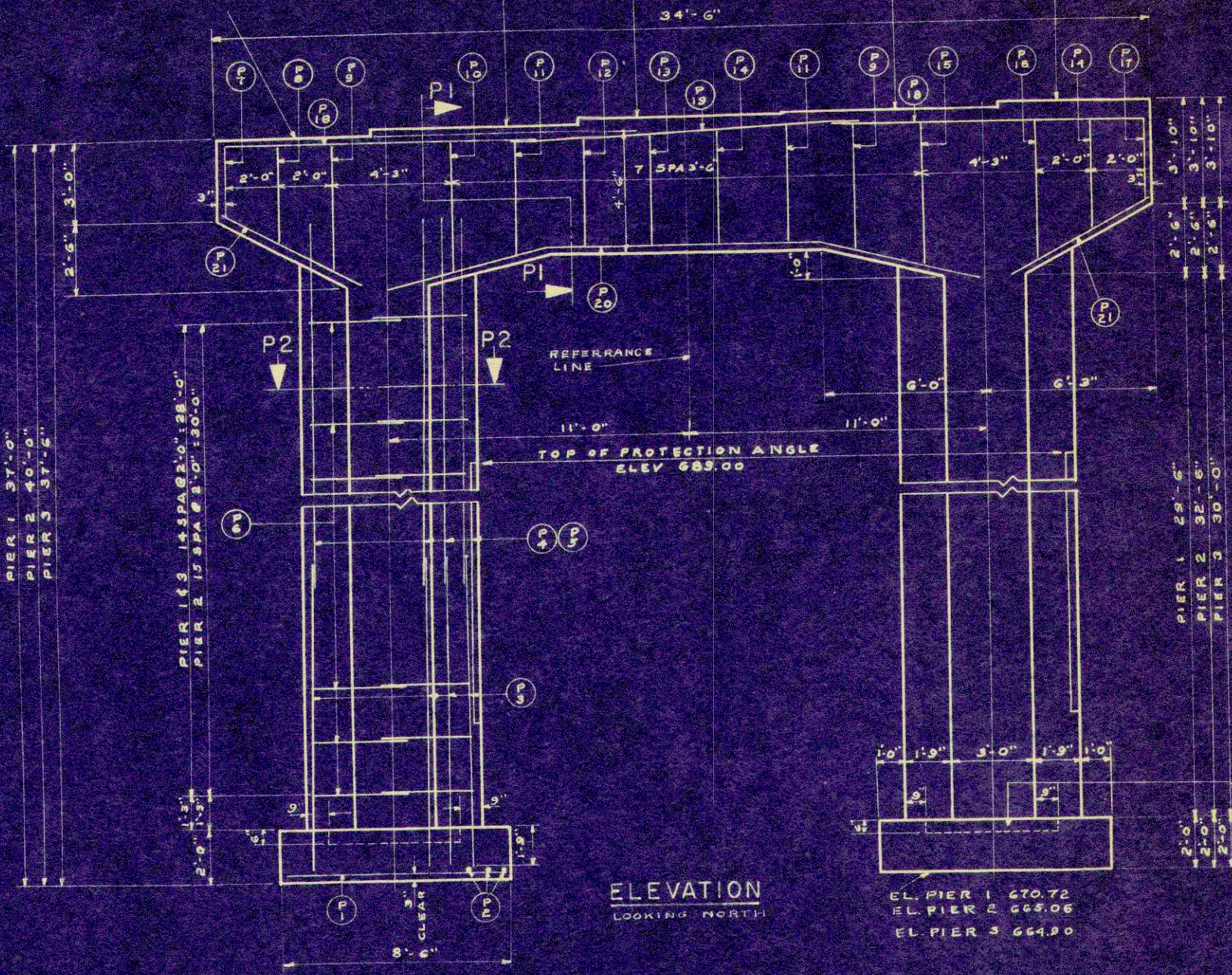
D. P. R. DIVISION	PROJECT	SHEET NO.	TOTAL SHEETS
50865(1)	1G	33	

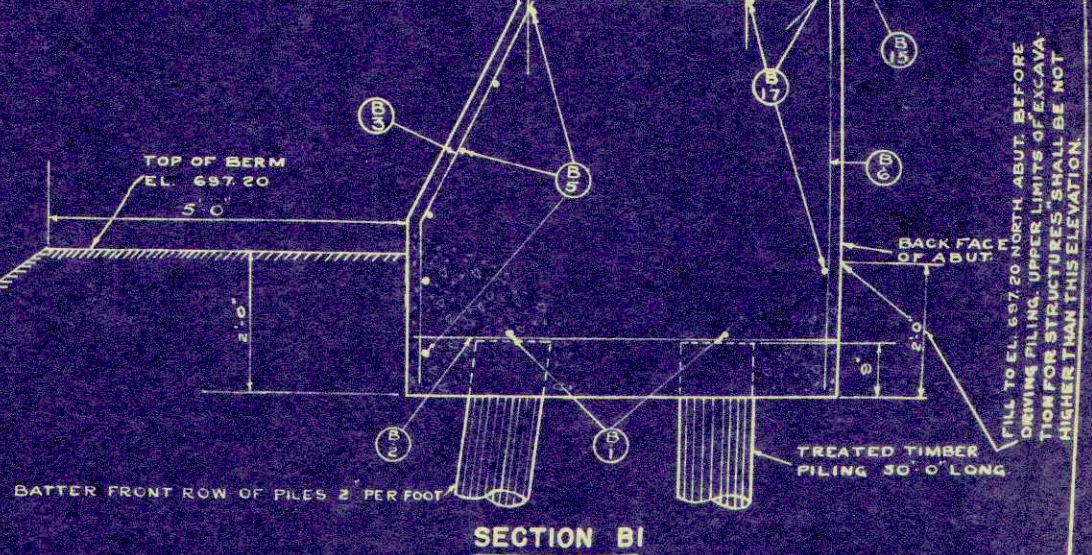
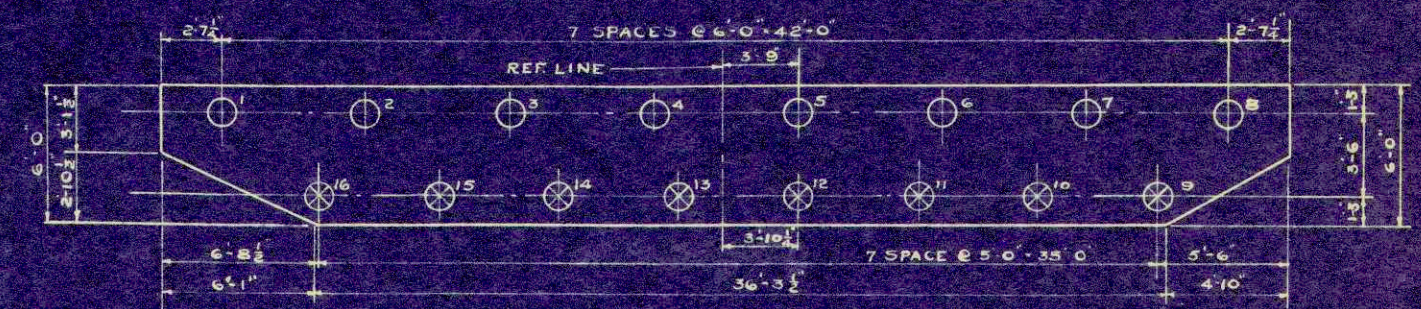
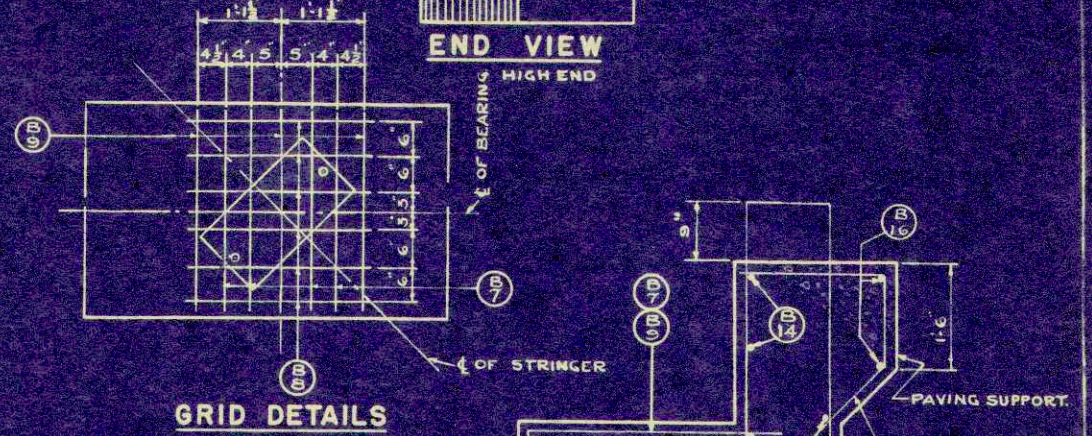
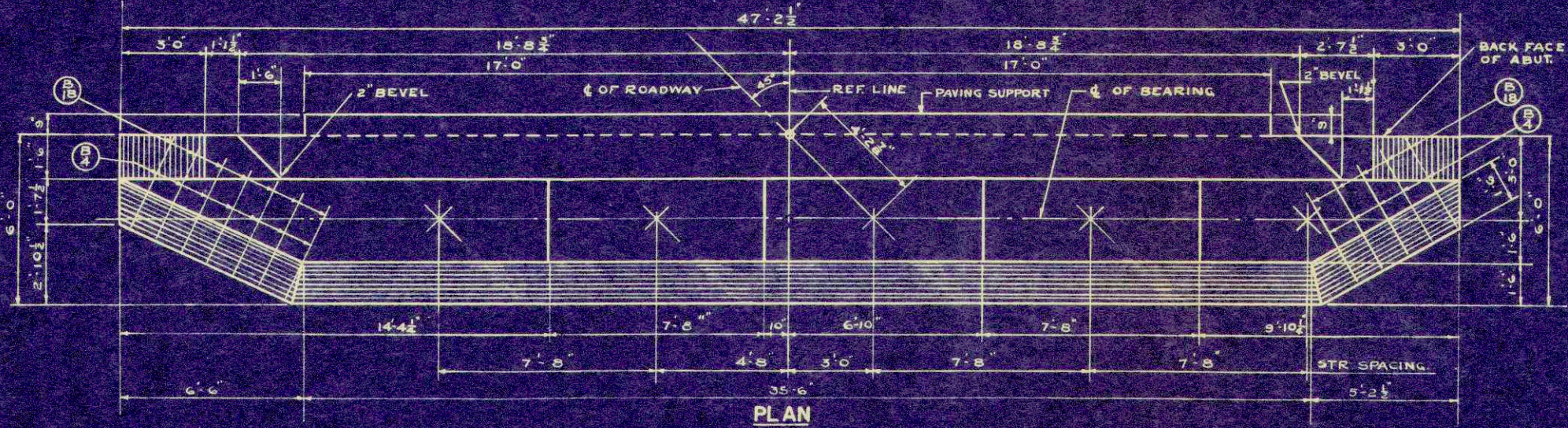
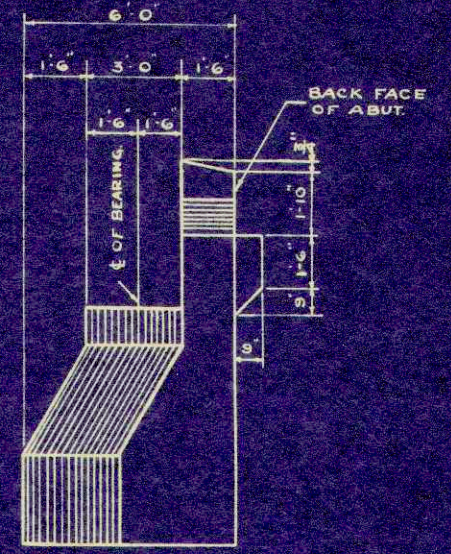
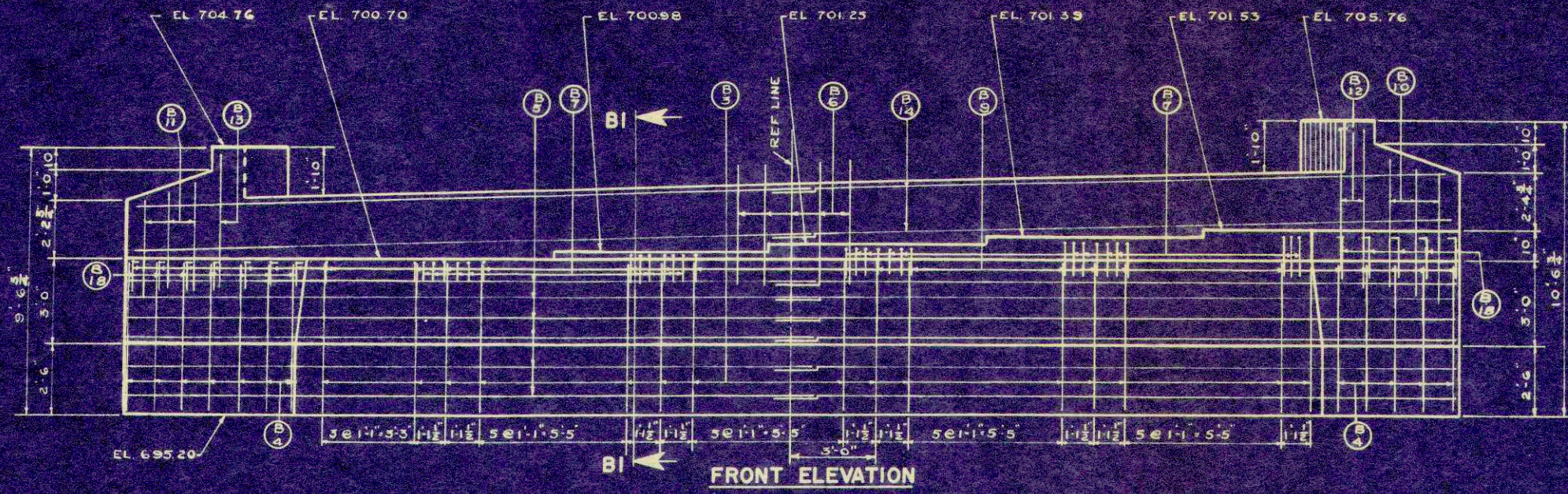


STATE HIGHWAY COMMISSION OF WISCONSIN			
<b>SOUTH ABUTMENT</b>			
DATE: 9-28-54	DRWN: B.F.	CD: NFW	SPEC: 1951
STRUCTURE: B-32-4	SHEET: 5 OF 8		LAND: M15

X 14231

PIER 1 EL 707.72	PIER 1 EL 707.99	PIER 1 EL 708.27	PIER 1 EL 708.41	PIER 1 EL 708.55
PIER 2 EL 705.06	PIER 2 EL 705.33	PIER 2 EL 705.61	PIER 2 EL 705.75	PIER 2 EL 705.89
PIER 3 EL 702.40	PIER 3 EL 702.67	PIER 3 EL 702.95	PIER 3 EL 703.09	PIER 3 EL 703.23

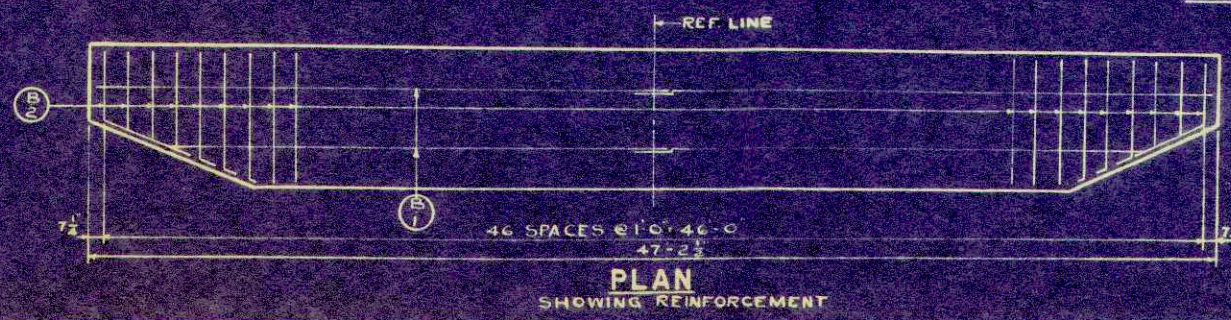




FILL TO EL. 697.20 NORTH ABUT. BEFORE DRIVING PILING. UPPER LIMITS OF EXCAVATION FOR STRUCTURES SHALL BE NOT HIGHER THAN THIS ELEVATION.

⊗ DENOTES BATTERED PILES. SHOWING PILE NUMBERING & SPACING

NOTE: ALL ELEVATIONS AND DIMENSIONS TAKEN AT BACK FACE OF ABUT.



STATE HIGHWAY COMMISSION OF WISCONSIN			
<b>NORTH ABUTMENT</b>			
DATE: 3-29-34	BY: WE	CHK: NFW	APP: 1931
STRUCTURE B 32 4	SHEET 7 OF 8		

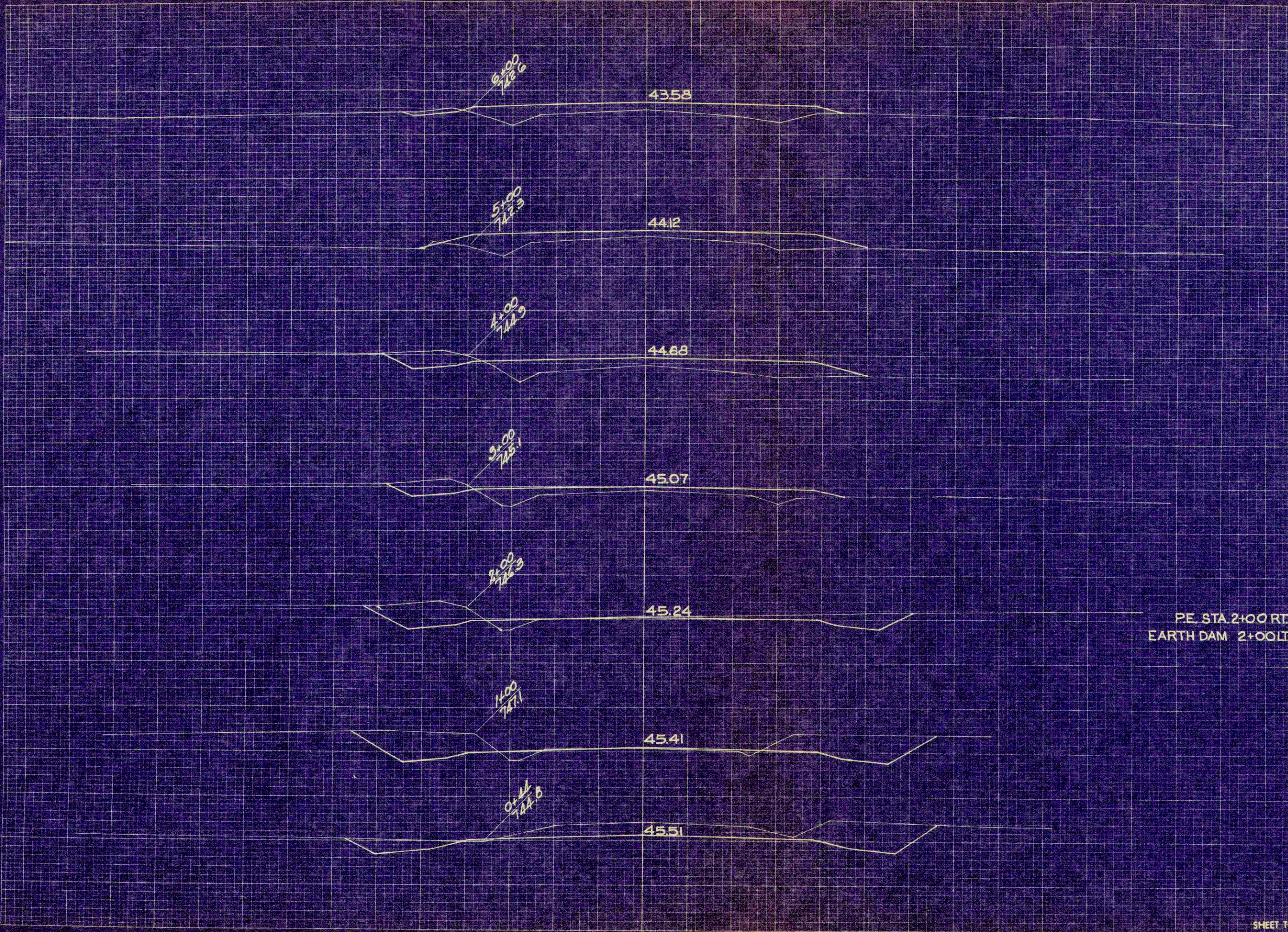
X14233



P.R.A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. SN	S 0865 (1)	20	33

FINAL SURVEY  
 SURVEYED  
 PLOTTED  
 NOTE BOOK  
 NO. 7111

ORIGINAL SURVEY  
 SURVEYED  
 PLOTTED  
 NOTE BOOK  
 NO. 7111

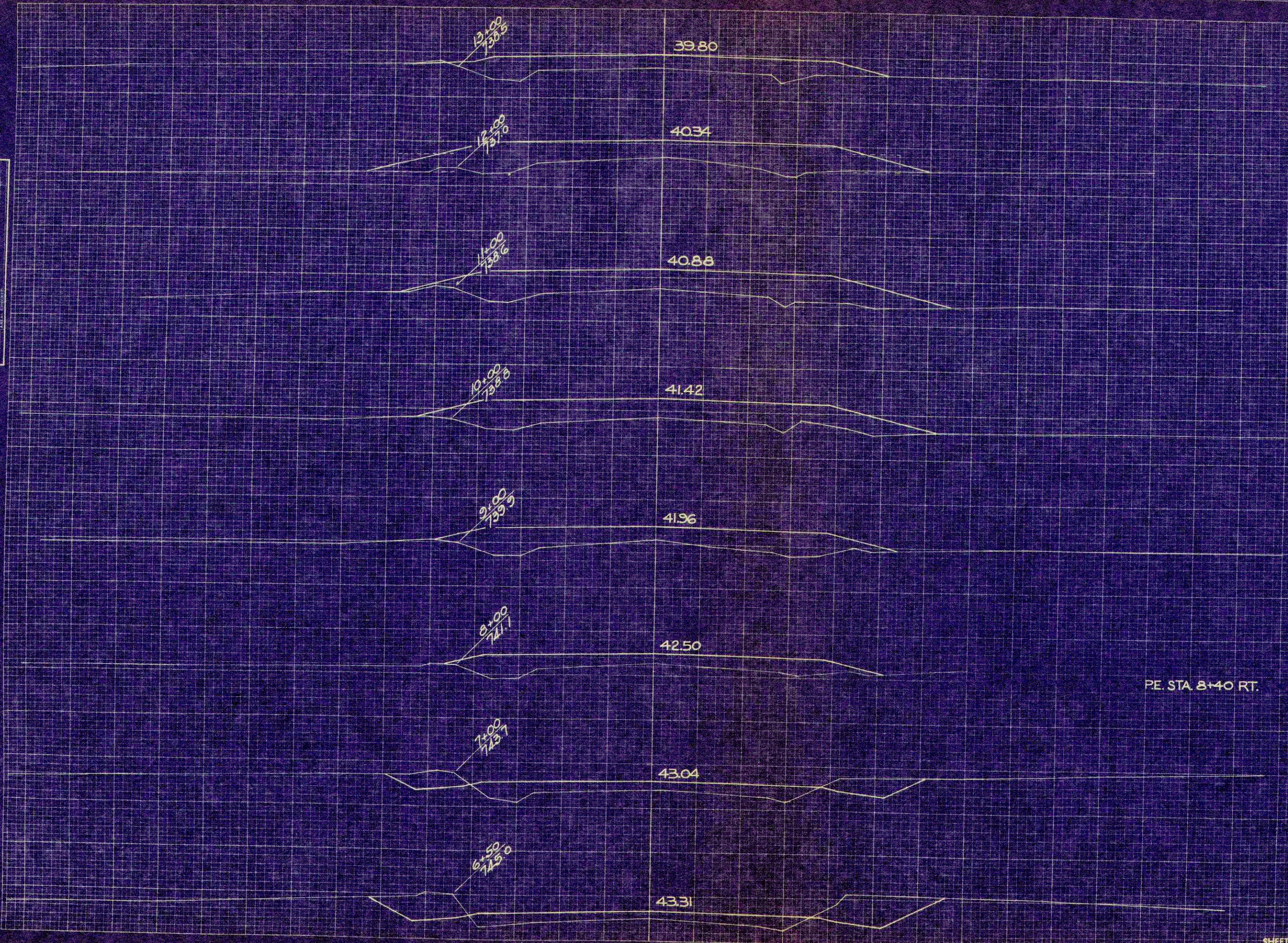


STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
0+20	0		0
+44	41		2
1	170		6
2	211		15
3	96		72
4	48		167
5	28		204
6	15		194
			10
			15
SHEET TOTAL		609	685

PE. STA. 2+00 RT.  
 EARTH DAM 2+00 LT.

FINAL SURVEY PLOTTED BY DATE  
 NOTE: BENCH MARK NO. 7777  
 AREA CHECKED

ORIGINAL SURVEY PLOTTED BY DATE  
 NOTE: BENCH MARK NO. 7777  
 AREA CHECKED



PE. STA. 8+40 RT.

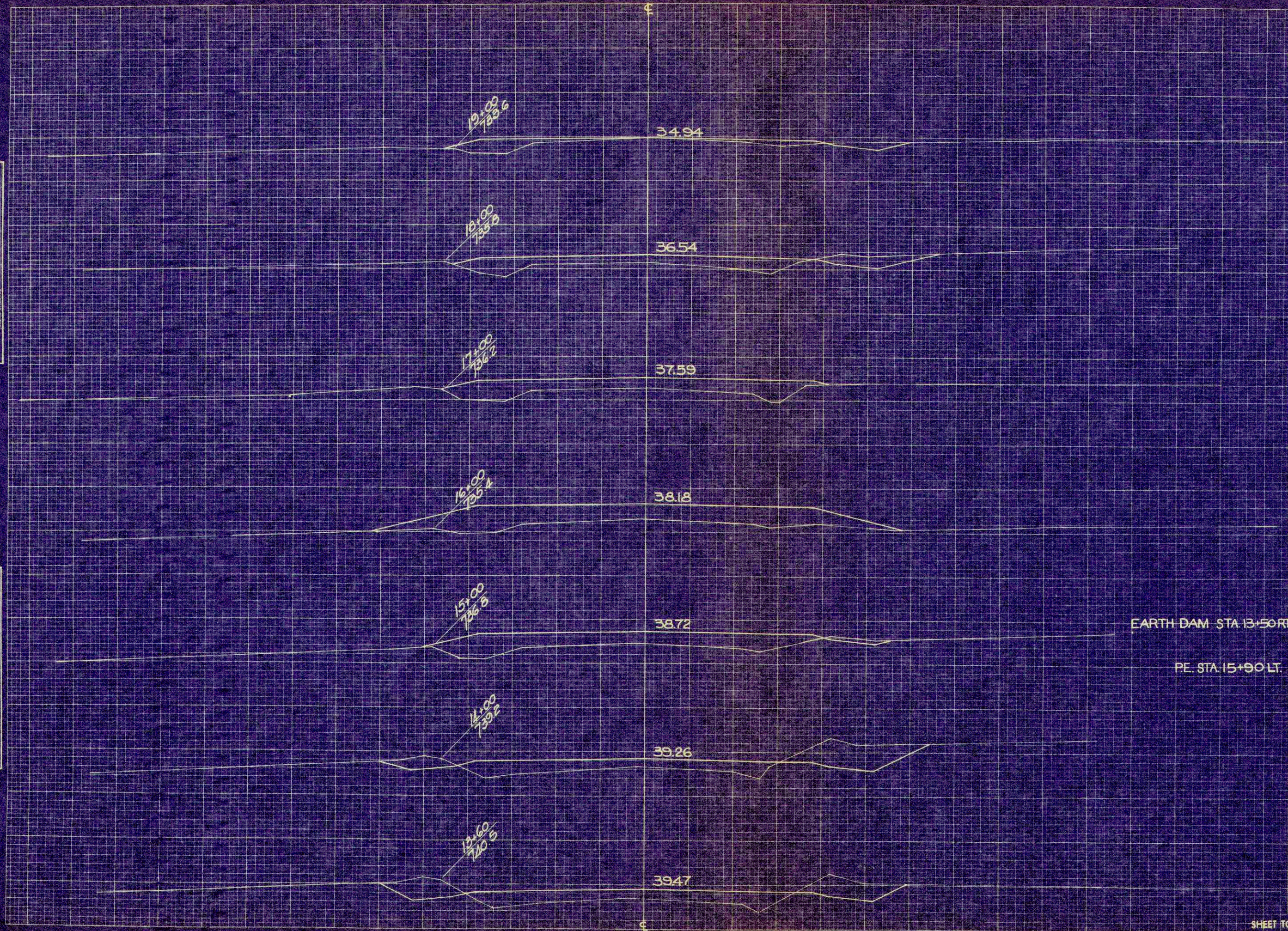
P. R. A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. SN	50665(1)	21	33

STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
6			
+50		57	78
7		78	72
8		56	213
9		0	311
10		0	407
11		0	481
12		0	500
13	2		389
			15
SHEET TOTAL		193	2466

P.R.A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. SN	S0665(1)	22	33

FINAL SURVEY  
 SURVEYED BY  
 PLOTTED BY  
 CHECKED BY  
 DATE

ORIGINAL SURVEY  
 SURVEYED BY  
 PLOTTED BY  
 CHECKED BY  
 DATE



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
13			
+60	44		117
14	57		56
15	83		215
16	4		333
17	4		330
18	28		209
19	28		124
			10
			60
SHEET TOTAL	248		1454





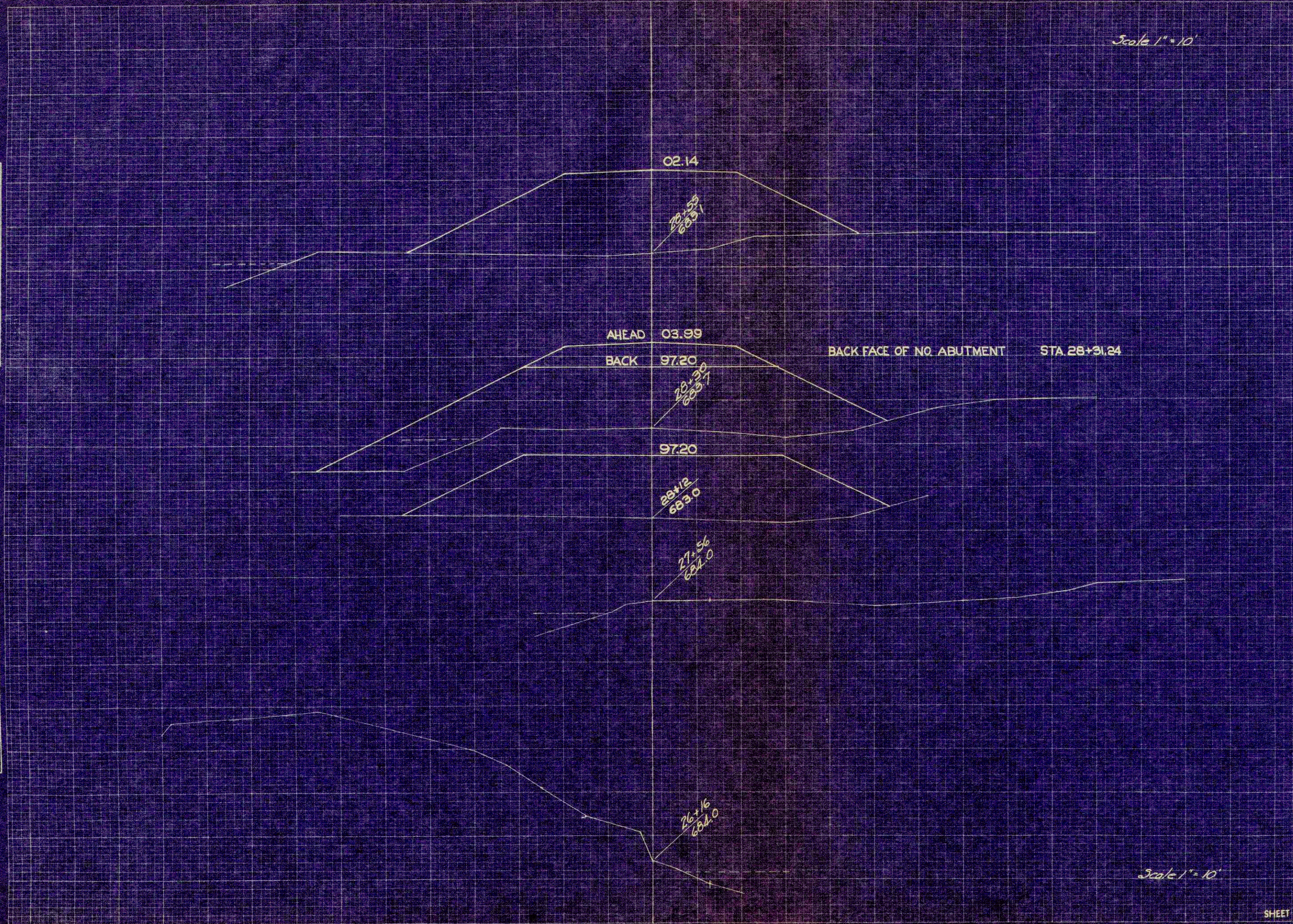


P. R. A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS SN	50865 (1)	25	33

Scale 1" = 10'

DATE  
BY  
CHECKED  
FINAL SURVEY PLOTTED  
NOTED FOR AREA  
NO.

DATE  
BY  
CHECKED  
ORIGINAL SURVEY PLOTTED  
NOTED FOR AREA  
NO.

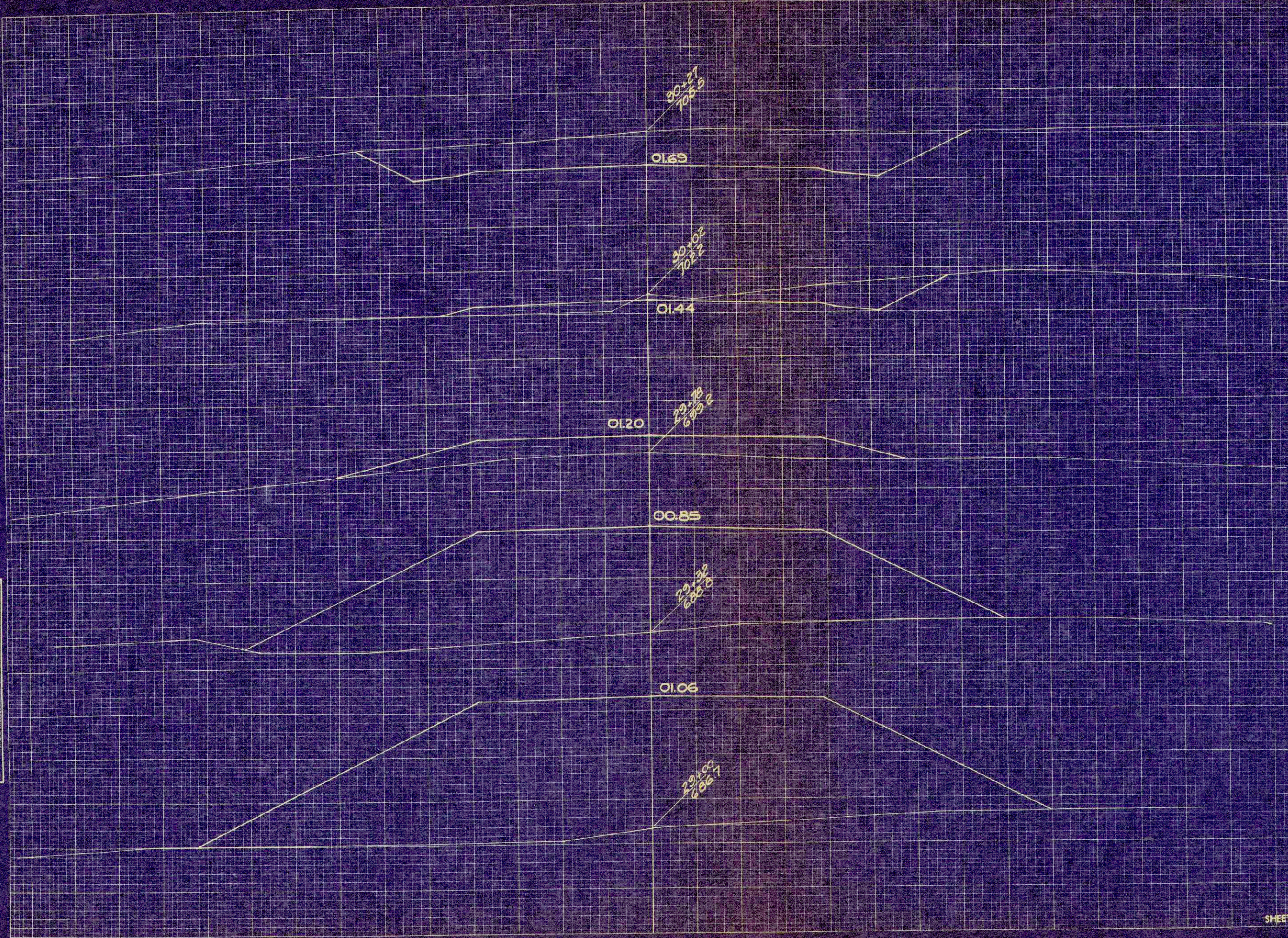


STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
26+16			
27+56			
27+94			4'3
28+12			825
28+30			
28+30			1259
28+55			
SHEET TOTAL			2497

Scale 1" = 10'

FINAL SURVEY  
 BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 NOTE BOOK AREA: \_\_\_\_\_  
 NO. 777

ORIGINAL SURVEY  
 BY: F.C.G.  
 DATE: \_\_\_\_\_  
 NOTE BOOK AREA: \_\_\_\_\_  
 NO. 777

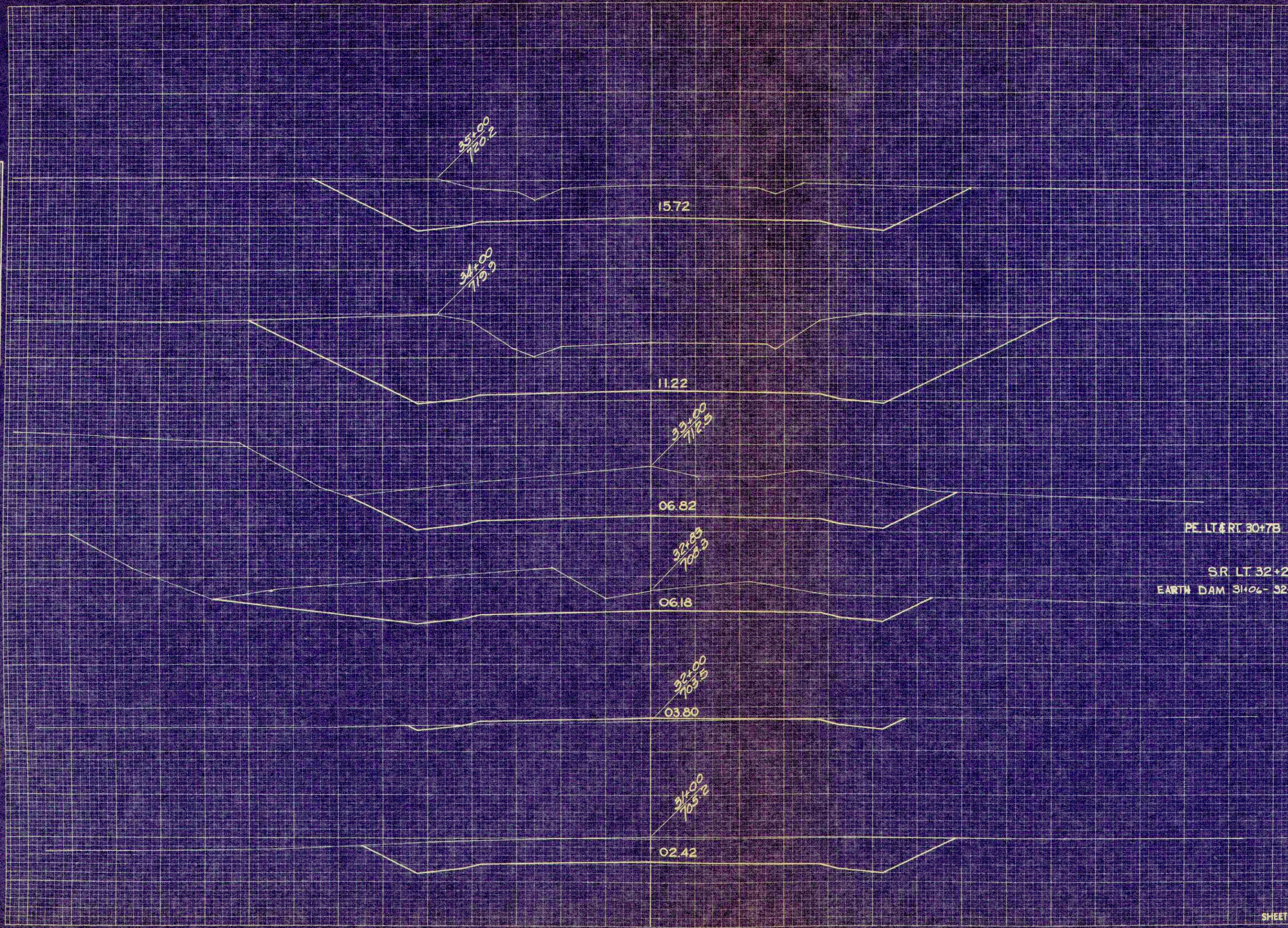


STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
28+55		0	1852
29		0	1007
30+02		0	726
27		24	59
		135	7
SHEET TOTAL		159	3651

P. R. A. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. SN	S0865 (1)	27	33

FINAL SURVEY  
 SURVEYED BY  
 PLOTTED BY  
 CHECKED BY  
 DATE

ORIGINAL SURVEY  
 SURVEYED BY  
 PLOTTED BY  
 CHECKED BY  
 DATE

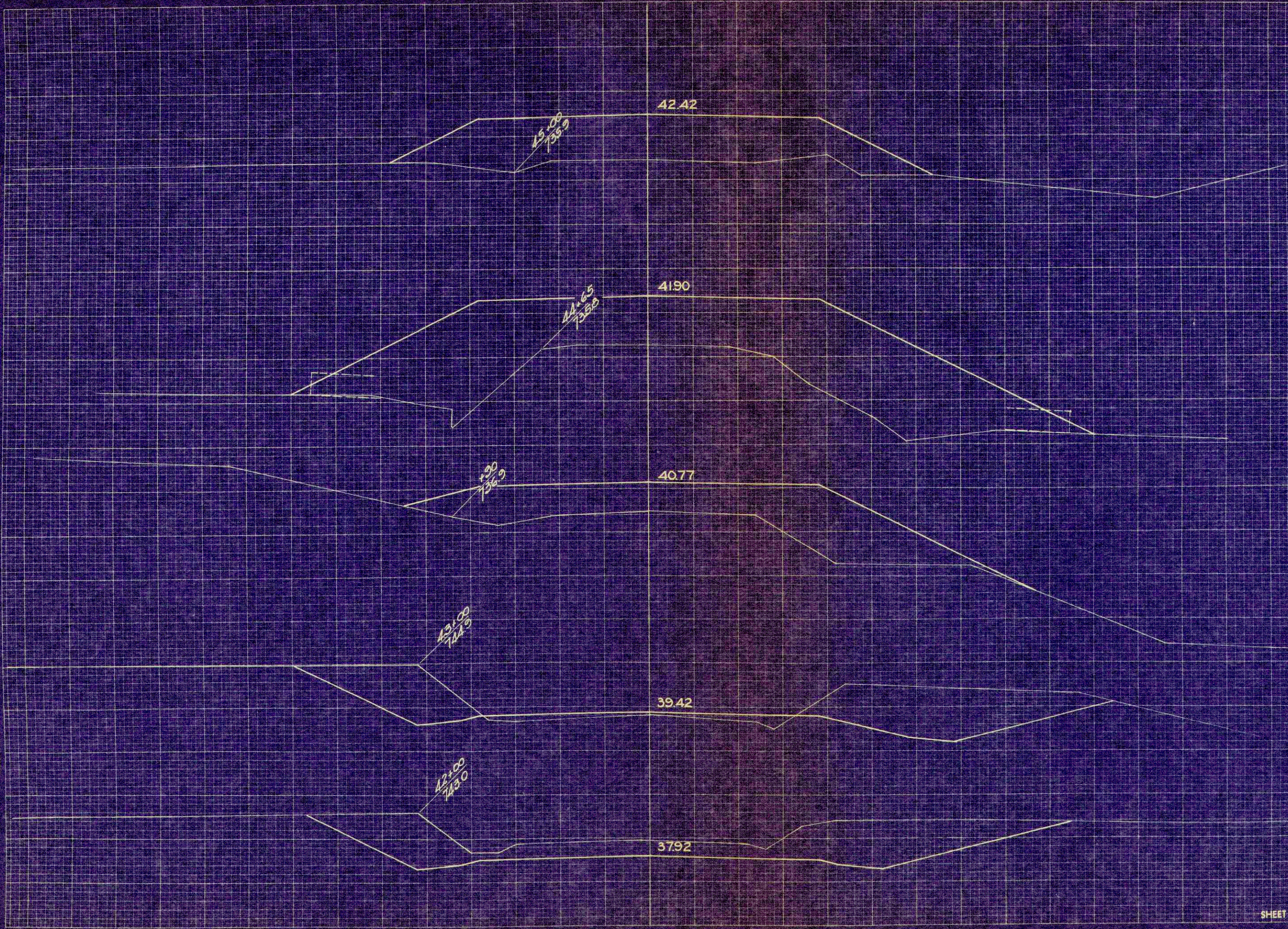


STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
30+27	574		9
31	361		13
32	359		0
33	170		0
34	1560		0
35	1502		0
	10		
			45
			35
SHEET TOTAL		4566	102



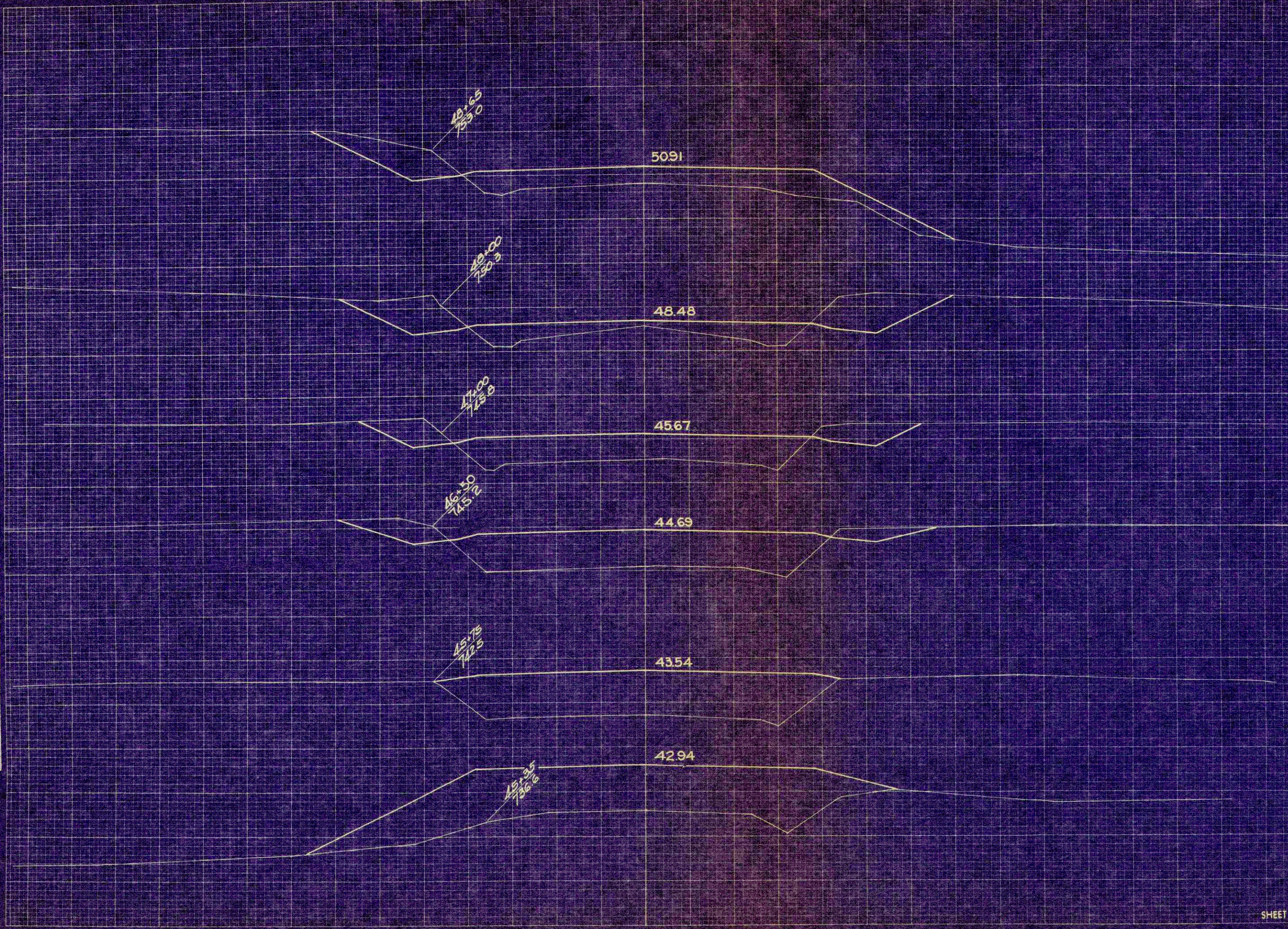
FINAL SURVEY  
 DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 NOTE: LOCAL AREAS CHECKED.

ORIGINAL SURVEY  
 DATE: 4/53  
 BY: FCG  
 CHECKED BY: \_\_\_\_\_  
 DATE: \_\_\_\_\_  
 NOTE: LOCAL AREAS CHECKED.



STATION	DISTANCE	YARDAGE	
		EXCAVATION	
		UNCL.	FILL
41			
	769		0
42			
	819		46
43			
	350		459
+90			
	0		1181
44 +65			
	0		561
45			

SHEET TOTAL      1938      2247



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
45	0		370
+35	0		380
+75	50		519
46	76		259
50	217		320
47	128		198
48			
+65			
SHEET TOTAL		471	2046

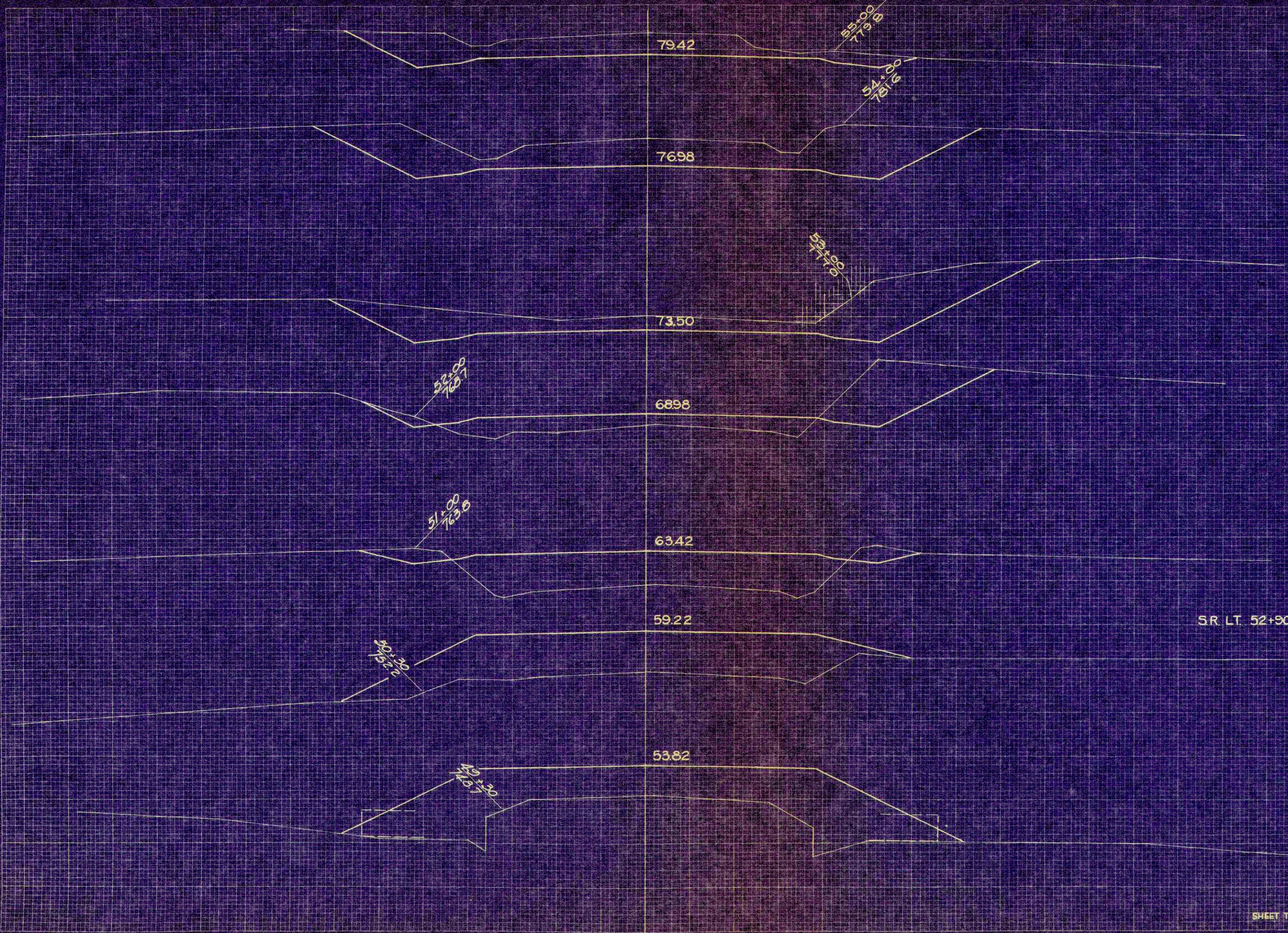
FINAL SURVEY SURVEYED BY DATE  
 PLOTTED BY DATE  
 TEMPERATURE DATE  
 NOTE BOOK NO. AREA  
 AREA CALCULATED

ORIGINAL SURVEY SURVEYED BY DATE  
 REVISIONS BY DATE  
 REVISIONS BY DATE  
 NO. 7771

B.P.R. DISTRICT OFFICE	PROJECT	SHEET NUMBER	TOTAL SHEETS
WIS. SN	S 0865 (1)	31	33

FINAL SURVEY  
 DATE: \_\_\_\_\_  
 BY: \_\_\_\_\_  
 CHECKED: \_\_\_\_\_  
 APPROVED: \_\_\_\_\_

ORIGINAL SURVEY  
 DATE: 4/63  
 BY: F.C.G.  
 CHECKED: \_\_\_\_\_  
 APPROVED: \_\_\_\_\_

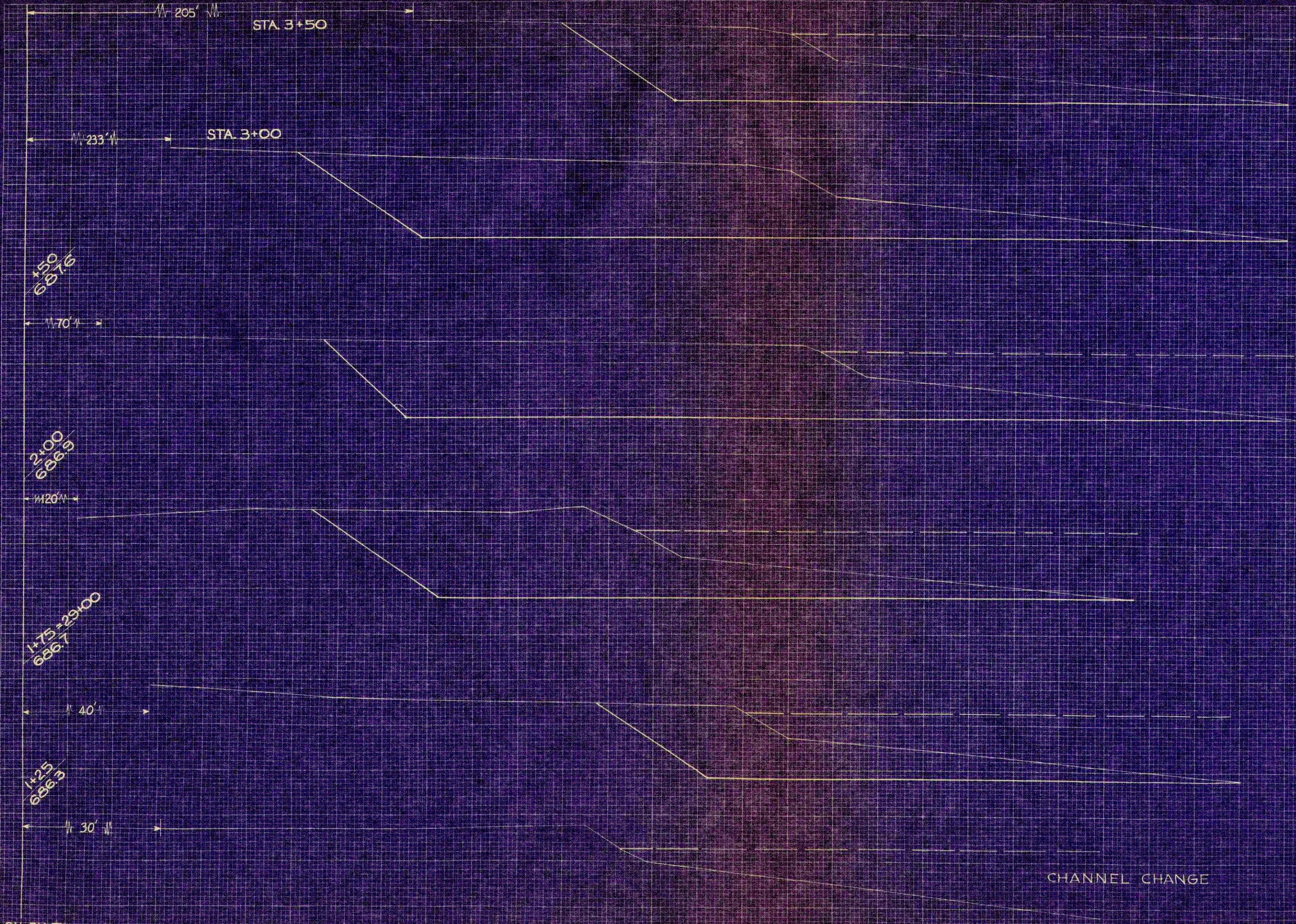


STATION	DISTANCE	YARDAGE	
		EXCAVATION	
		UNCL. EXCAVATION	FILL
48+65			
49+30	43		483
50+30	0		1037
51	24		559
52	178		459
53	522		131
54	793		0
55	626		0
56	213		0
	SR LT 52+90	150	15
<b>SHEET TOTAL</b>		<b>2549</b>	<b>2684</b>



ORIGINAL SURVEY SUBMITTED BY DATE  
 NO. 4887 JAMES CHECKED  
 NO. 7771 JAMES CHECKED

ORIGINAL SURVEY SUBMITTED BY DATE  
 NO. 4887 JAMES CHECKED  
 NO. 7771 JAMES CHECKED



WIS. SN	PROJECT	SHEET NUMBER	TOTAL SHEETS
5N	S 0865 (1)	32	33

STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
1	25	208	
2	75	150	
3	2	922	
4	50	1039	
5	3	787	
6	50		
7	70		

CH. CH. T.

SHEET TOTAL 4217

# GRADIENT

STAKE NO.	DIST. FROM STAKE TO C.L. OF NEW ROAD	DISTANCE C.L. TO SLOPE STAKE		STAKE NO.	DISTANCE FROM STAKE TO C.L. OF NEW ROAD	DISTANCE C.L. TO SLOPE STAKE	
		LEFT OF C.L.	RIGHT OF C.L.			LEFT OF C.L.	RIGHT OF C.L.
0+44	0	34	33	47+00	23 R	32	31
1+00	19 R	33	33	48+00	23 R	34	35
2+00	20 R	32	30	49+00	24 R	38	35
3+00	20 R	29	22	50+30	16 R	34	36
4+00	20 R	30	25	51+00	25 R	35	30
5+00	20 R	25	25	52+00	26 R	32	31
6+00	20 R	28	22	53+00	26 R	32	39
+50	22 R	32	33	54+00	21 L	36	44
7+00	22 R	30	31	55+00	22 L	38	38
8+00	22 R	23	26	56+00	21 L	34	30
9+00	22 R	25	27				
10+00	23 R	27	31				
11+00	23 R	29	33				
12+00	23 R	33	36				
13+00	23 R	28	25				
+60	23 R	30	30				
14+00	23 R	30	32				
15+00	24 R	25	28				
16+00	24 R	31	29				
17+00	23 R	23	21				
18+00	23 R	22	33				
19+00	22 R	23	30				
20+00	23 R	30	34				
+70	24 R	38	45				
21+35	24 R	39	44				
22+00	24 R	34	38				
23+00	23 R	38	42				
24+00	23 R	45	49				
25+00	37 R	48	45				
+58	0	46	BR				
+82	0	70					
+97	0						
26+16	0	BRIDGE					
27+46	0						
28+30	0	75	53				
+55	0	55	47				
29+00	0	51	45				
+32	0	46	40				
+78	0	35	29				
30+02	0	23	34				
+27	0	33	37				
31+00	0	32	35				
32+00	0	27	29				
+83	0	49	32				
33+00	0	34	35				
34+00	24 R	45	47				
35+00	25 R	38	36				
36+00	25 R	34	31				
37+00	25 R	36	32				
38+00	25 R	32	21				
39+00	25 R	30	30				
40+00	25 R	34	38				
41+00	26 R	36	48				
42+00	26 R	39	47				
43+00	26 R	40	52				
+90	22 R	28	44				
44+65	12 R	40	50				
45+00	15 R	29	32				
+35	18 R	38	28				
+75	24 R	24	22				
46+50	24 R	35	33				