

INDEX OF SHEETS

SHEET NO. 1 TITLE

SHEET NO. 2, 2A TYPICAL CROSS SECTIONS

SHEET NO. 3, 3A ESTIMATE OF QUANTITIES

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SHEET NO. 6 RIGHT OF WAY PLAT

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SHEET NO. 19-26 DRAINAGE STRUCTURES AND STANDARD DETAILS

SHEET NO. 27 CROSS SECTIONS

STATE OF WISCONSIN
WISCONSIN HIGHWAY COMMISSION

PLAN AND PROFILE OF PROPOSED
SPARTA - LA CROSSE ROAD
 LA CROSSE WEST SALEM ROAD
 LA CROSSE COUNTY

F.A.P. 307 B SEC. I.
 BEGINNING AT A POINT NEAR THE SW CORNER OF THE N.W. 1/4 OF THE SW 1/4 SEC. 11, T17N, R7W; THENCE NORTHEASTERLY TO A POINT NEAR THE N.E. CORNER OF THE N.W. 1/4 OF THE N.W. 1/4 OF SEC. 5, T17N, R6W.

FEDERAL AID PROJECTS 397-F, 307-B SEC. I, 307-E, 307-D SEC. III, 307-C

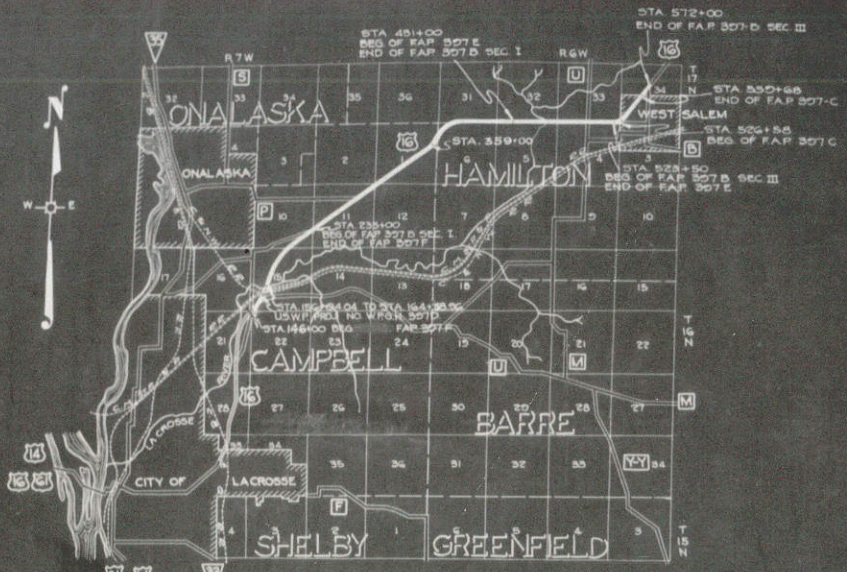
F.A.P. 307 B. SEC. III
 BEGINNING AT A POINT NEAR THE S.E. COR. OF SEC. 35, T17N, R6W; THENCE NORTHEASTERLY TO A POINT NEAR THE N.W. CORNER OF THE SW 1/4 OF THE NE 1/4 OF SEC. 34, T17N, R6W. EXCEPT THAT PORTION BETWEEN STAS 526+56 AND 550+66

F.A.P. 307 F
 BEGINNING AT A POINT NEAR THE SW COR. OF SEC. 15, T17N, R7W; THENCE NORTH AND NORTHEASTERLY TO A POINT NEAR THE NORTHEAST CORNER OF THE SE 1/4-SE 1/4, SEC. 10, T17N, R7W. EXCEPT THAT PORTION BETWEEN STAS 156+04.04 AND 164+58.56

F.A.P. 307 E
 BEGINNING AT A POINT NEAR THE N.E. CORNER OF THE N.W. 1/4 OF THE N.W. 1/4 OF SEC. 5, T17N, R6W; THENCE EASTERLY TO A POINT NEAR THE S.E. CORNER OF SEC. 35, T17N, R6W.

F.A.P. 307 C
 BEGINNING AT A POINT NEAR THE S.E. CORNER OF SEC. 35, T17N, R6W; THENCE NORTHEASTERLY TO A POINT NEAR THE CENTER OF SEC. 34, T17N, R6W.

PLAN 1 IN. = 100 FT.
 PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
 CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



CONVENTIONAL SIGNS

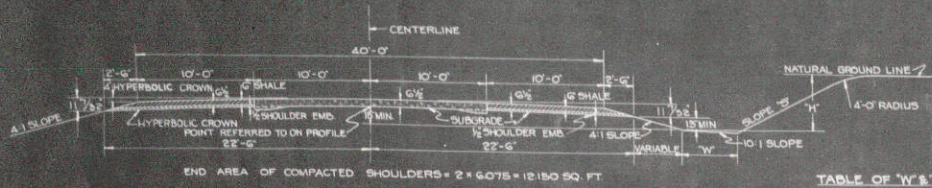
STATE LINE	-----	CULVERTS IN PLACE	-----
COUNTY LINE	-----	CULVERTS REQUIRED	-----
TOWNSHIP OR RANGE LINE	-----	DROP INLET	-----
SECTION LINE	-----	POWER POLE	-----
NEW RIGHT OF WAY LINE	-----	TELEPHONE OR TELEGRAPH POLE	-----
PRESENT RIGHT OF WAY LINE	-----	RIGHT OF WAY MARKERS	-----
WIRE FENCE	-----	REFERENCE STAKE FOR HUBS ONLY	-----
WIRE FENCE	-----	MARSH	-----
WIRE FENCE	-----	MEDGE	-----
WIRE FENCE	-----	TREES	-----
LOT LINE	-----	GROUND ELEVATION	-----
CORPORATE OR CITY LIMITS	-----	GRADE ELEVATION	-----
PROPERTY LINE	-----		
TRAVELED WAY OR P.E.	-----		
RAILROADS	-----		
BASE OR SURVEY LINE	-----		

LAYOUT

SCALE 0 MI. 1 MI. 2 MI.

F.A.P. 307 F	= 1.454 MI.
F.A.P. 307 B SEC. I	= 2.344 MI.
F.A.P. 307 E	= 1.351 MI.
F.A.P. 307 D SEC. III	= 1.708 MI.
F.A.P. 307 C	= 0.201 MI.
F.A.P. 307 B SEC. III	= 0.626 MI.
TOTAL	= 7.784 MI.

<p>WISCONSIN HIGHWAY COMMISSION MADISON, WIS.</p> <p>Surveyor: J. P. A. Note Book "DIA" 12, 13, 14, 15 Dir. Computer: E. W. J. M. O. Checkler, N. F. C. Dir. Checker: R. C. J. Correct: 7-18-36</p> <p>CORRECT: <i>T. M. Reynolds</i> DIVISION ENGINEER</p> <p>RECOMMENDED FOR APPROVAL: <i>Franklin</i> <i>A. H. Hilde</i> DISTRICT ENGINEER DISTRICT ENGINEER</p> <p>APPROVED: _____ DATE: 7/13/36 <i>E. L. Peden</i> STATE HIGHWAY ENGINEER</p> <p>CHECKED IN FIELD BY: <i>D. C. 406</i></p>	<p>U.S. DEPARTMENT OF AGRICULTURE BUREAU OF PUBLIC ROADS</p> <p>RECOMMENDED FOR APPROVAL: _____ DISTRICT ENGINEER</p> <p>APPROVED: _____ _____ CHIEF ENGINEER</p> <p>APPROVED: _____ _____ DIRECTOR</p> <p>DATE: _____</p>
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SECTION FOR 9'-6 1/2" CONCRETE PAVEMENT

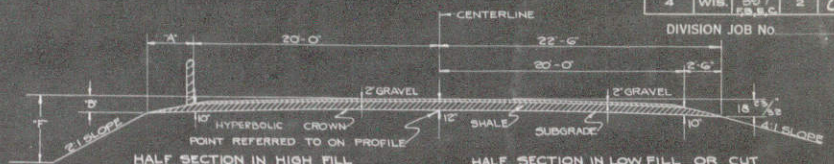
TABLE OF "W" & "H" VALUES

W = 3'-0" MIN WHERE "H" = 5 FT OR LESS
W = 4'-0" MIN WHERE "H" = 5 FT. TO 10 FT
W = 8'-0" MIN WHERE "H" = OVER 10 FEET

W = 0'-0" STA. 324+00-333+50 & 353+50-371+00
W = 0'-0" STA. 236+00-243+00 & 263+00-283+15
W = 10'-0" STA. 431+00-450+00 & 476+50-483+00

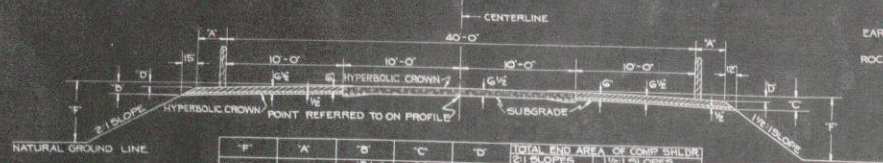
CUT SLOPE "S"

EARTH SLOPES 2:1 - SAND
1 1/2:1 - OTHER
ROCK SLOPES 1:1 - SHALE & SANDSTONE
3/4:1 - OTHER



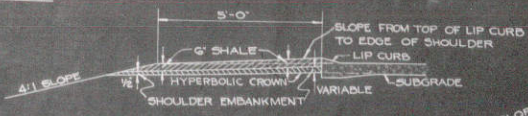
SECTION FOR GRAVEL SURFACING
STA. 164+58.96-176+37, 450+00-460+39 & 462+61-476+40

SHALE SURFACING PER MILE = 10,260 CY
GRAVEL SURFACING PER MILE = 18260 CY

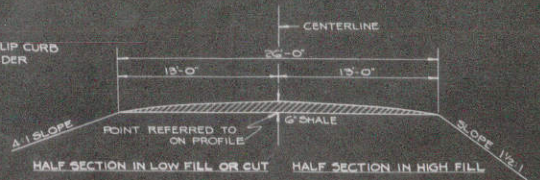


GUARD FENCE SECTION FOR 9'-6 1/2" CONCRETE PAVEMENT

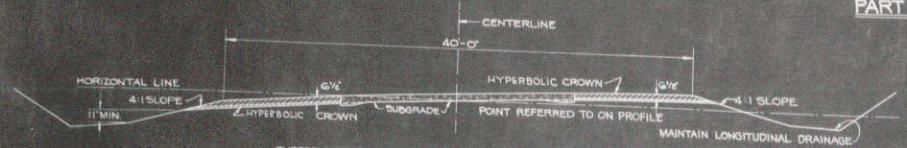
"F"	"A"	"B"	"C"	"D"	TOTAL END AREA OF COMP. SHOULDR (SQ. FT.)	NET SLOPES (SQ. FT.)
UNDER 5	2'-6"	11 1/4"	11 3/4"	4 3/4"	1417 S.F.	1404 S.F.
5 TO 10	3'-6"	11 1/4"	11 3/4"	5 1/4"	1525 S.F.	1512 S.F.
OVER 10	4'-6"	12 1/4"	12 3/4"	5 3/4"	1633 S.F.	1620 S.F.



PART SECTION OF PAVEMENT WITH LIP CURB



SECTION FOR SHALE SURFACING ON SIDE ROADS

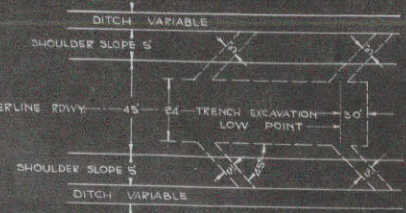


SUPERELEVATION = 0.01 FT. PER DEGREE OF CURVE PER FOOT OF WIDTH
MAXIMUM SUPERELEVATION = 0.10 FT. PER FOOT

TYPICAL SECTION ON CURVES
NOTE: WHERE NECESSARY TO MAINTAIN LONGITUDINAL DRAINAGE, SECTION WILL BE ROTATED ABOUT POINT OTHER THAN CENTERLINE AND/OR PROFILE GRADE ADJUSTED AS REQUIRED.



HALF-SECTION FOR SUBGRADE TREATMENT

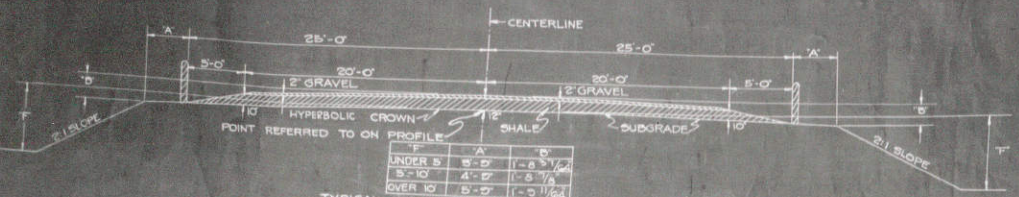


PLAN OF SUBGRADE TREATMENT

CONSTRUCT OUTLET TRENCHES ON EACH SIDE OF THE MAIN TRENCH AT 50' INTERVALS, AND AT THE EXTREME LOW POINTS AS SHOWN.
BACKFILL MAIN TRENCH AND OUTLET TRENCH WITH SELECTED MATERIAL FROM EXCAVATION WHICH IS SUITABLE FOR MATERIAL INTENDED FOR THIS PURPOSE.



SECTION FOR GRAVEL SURFACING STA. 146+00-156+04.04



SECTION FOR GRAVEL SURFACING STA. 146+00-156+04.04

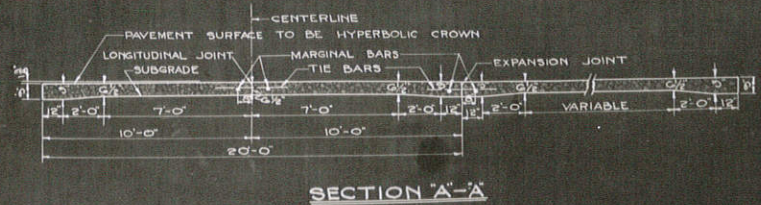
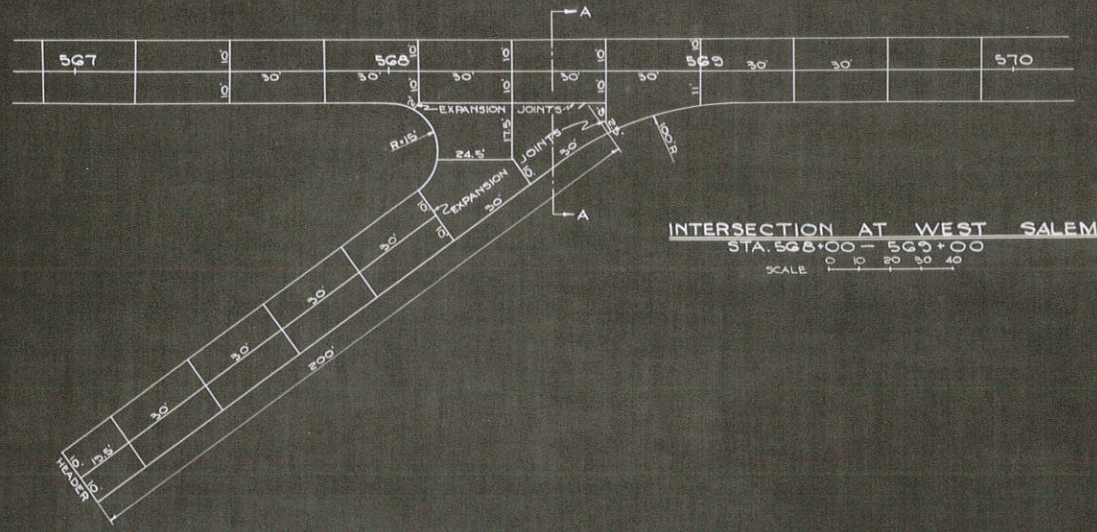
APPROVED APPLICABLE DETAIL DRAWINGS ARE:

- TYPICAL CROSS SECTIONS FOR 9'-6 1/2" CONG. PAVT. SD-1
- LONGITUDINAL JOINTS SD-2
- TRANSVERSE EXPANSION JOINTS SD-3
- CONCRETE SURFACE DRAINS AND LIP CURB SD-7
- PAVEMENT REINFORCEMENT SD-8
- CONCRETE HEADER BLOCKS SD-9
- WIRE CABLE GUARD FENCE SD-11
- MARKER POSTS SD-12
- BARRICADE SD-16

TYPICAL CROSS SECTION FOR 40 FT. & 30 FT. ROADBEDS 20 FT. CONCRETE & SHALE & GRAVEL SURFACING

FED. ROAD DIST. NO.	STATE	PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	507-13 S.E.C. 11	2A	27

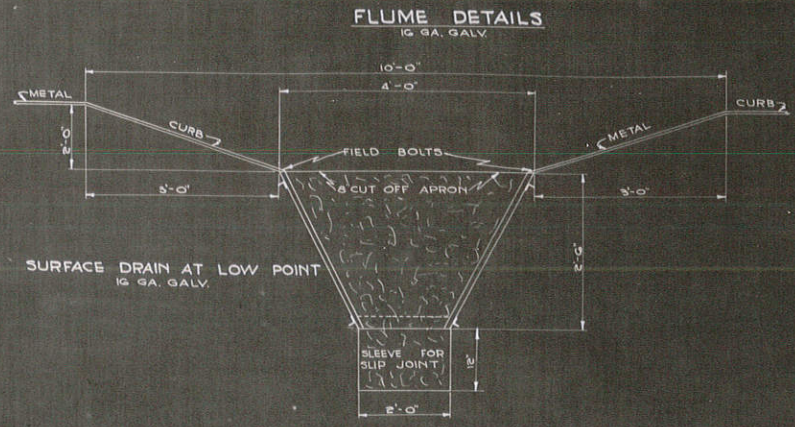
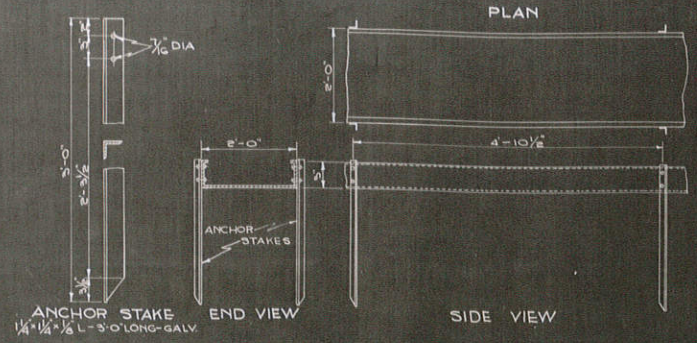
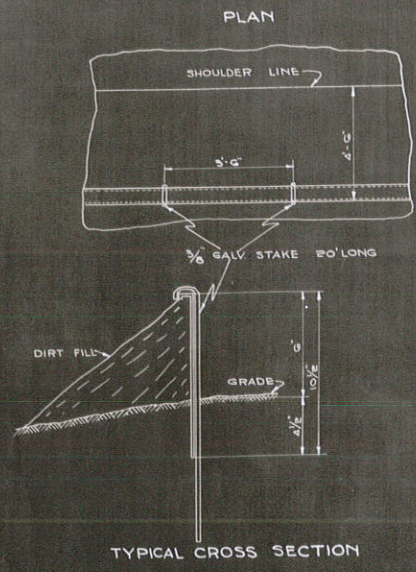
DIVISION JOB NO. 5510



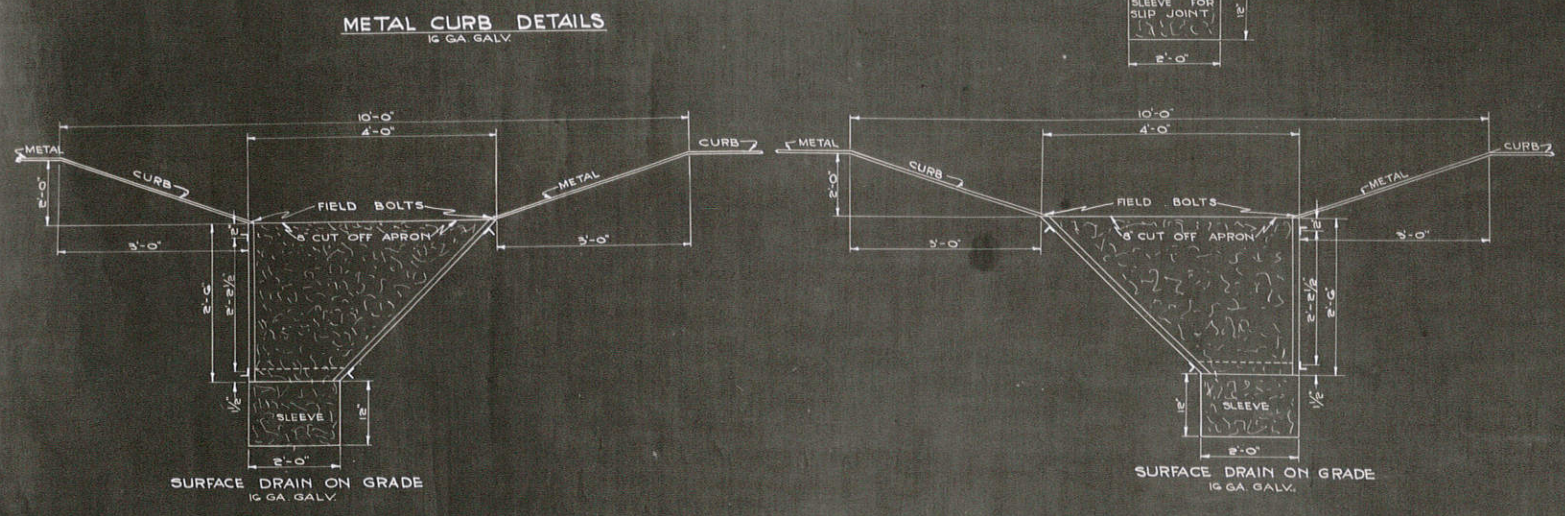
JOINT DETAILS
 FOR INTERSECTION AT
 WEST SALEM

FED. ROAD DIST. NO.	STATE	F.A. NO. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	2011 28	28	27

DIVISION JOB NO.



GENERAL NOTES
 ALTERNATE DESIGNS OR TYPES OF CURBS, FLUMES, AND SURFACE DRAINS SHALL MEET THE APPROVAL OF THE ENGINEER BEFORE FABRICATION IS ORDERED BY THE CONTRACTOR.



DETAILS FOR METAL SURFACE DRAINS

DETAILS OF METAL CURB AND METAL SURFACE DRAINS

DETAIL SUMMARY SHEET OF MISCELLANEOUS QUANTITIES

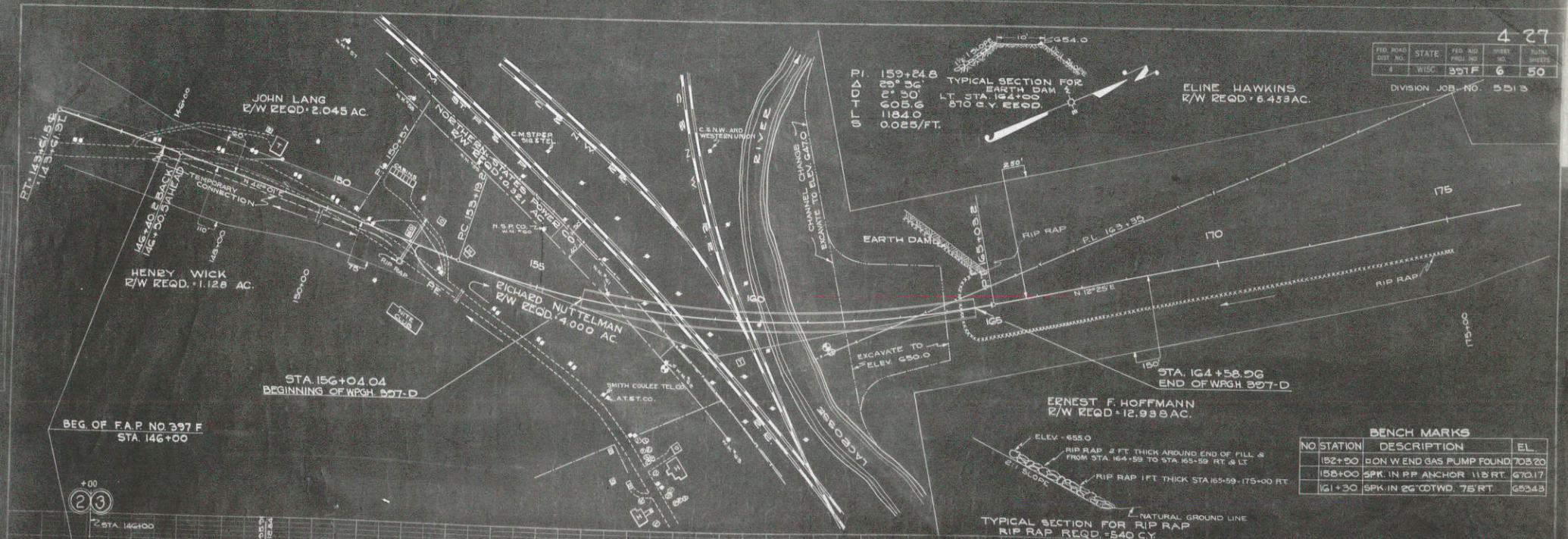
(P. C. CONCRETE PAVEMENT)

38 27

BORROW FOR BACKFILL				ADD. FINISHING RDWY.				SUBGRADE TREAT.				LIP CURB				CONCRETE SURF. DRAINS				WIRE FABRIC REINF.				GUARD FENCE								
MILE NO.	STATION TO STATION	C.Y.	MILE NO.	STATION TO STATION	SIDE	STA.	MILE NO.	STATION TO STATION	DEPTH	C.Y.	MILE NO.	STATION TO STATION	SIDE	TYPE-A LIN. FT.	TYPE-B LIN. FT.	TYPE-C LIN. FT.	MILE NO.	STATION	SIDE	TYPE	LENGTH OF FLUME	CONC. CU. YDS.	MILE NO.	STATION TO STATION	NO. YDS.	MILE NO.	STATION TO STATION	SIDE	TYPE	ANCHOR AGES		
F.A.P. 397 B SEC. I				F.A.P. 397 F				F.A.P. 397 B SEC. I																								
1	266+50-270+70	1070	3	152+50	RT.	1.0	1	266+50-270+70	2'	892																						
2	282+30-299+80	4293	4	232+50	LT.	2.0	2	282+30-299+80	2'	3577																						
2	316+50-317+50	314	4	234+00	RT.	2.0	2	316+50-317+50	2'	262																						
3 332+70-333+35				F.A.P. 397 B SEC. I				3 332+70-333+35																								
		239								199																						
4	391+75-394+50	744	1	284+25	RT.	1.0	4	391+75-394+50	2'	620																						
4	397+00-400+50	943	1	264+75	LT.	1.0	4	397+00-400+50	2'	786																						
4	406+50-409+50	795	3	324+00	LT.	2.0	4	406+50-409+50	2'	662																						
4	411+25-418+75	433	3	348+00	RT.	1.0	4	411+25-418+75	2'	361																						
4	423+50-426+00	678	3	379+90	RT.	2.0	4	423+50-426+00	2'	565																						
F.A.P. 397 E				F.A.P. 397 E				F.A.P. 397 E																								
1	463+75-464+50	234	4	386+00	LT.	5.0	1	463+75-464+50	2'	195																						
1	465+50-468+50	1085					1	465+50-468+50	3'	904																						
1	481+00-481+50	180	1	434+00	LT.	8.0	1	481+00-481+50	2'	150																						
1	483+50-486+50	1085	1	464+50	RT.	2.0	1	483+50-486+50	3'	904																						
			1	465+00	LT.	10.0																										
2	501+25-502+50	346	2	499+84	LT.	1.0	2	501+25-502+50	2'	288																						
2	503+50-505+25	456	2	499+84	RT.	1.0	2	503+50-505+25	2'	380																						
			2	522+00	RT.	2.0																										
F.A.P. 397 B SEC. III				F.A.P. 397 B SEC. III				F.A.P. 397 B SEC. III																								
7	559+68-560+70	335	7	568+50	RT.	2.0	7	559+68-560+70	2'	279																						
F.A.P. 397 C				F.A.P. 397 C				F.A.P. 397 C																								
6	530+40-532+70	578	6	529+00	RT.	3.0	6	530+40-532+70	2'	482																						
6	537+50-540+50	733	6	531+00	RT.	1.0	6	537+50-540+50	2'	611																						
6	544+50-545+50	287	6	531+00	LT.	1.0	6	544+50-545+50	2'	239																						
6	550+20-551+65	388	6	536+00	LT.	1.0	6	550+20-551+65	2'	323																						

METAL CURB (CONT.)				METAL FLUME (CONT.)				METAL SURF. DRAINS ON GRADE - (CONT.)			
MILE NO.	STATION TO STATION	SIDE	LIN. FT.	MILE NO.	STATION	SIDE	LIN. FT.	MILE NO.	STATION	SIDE	LIN. FT.
F.A.P. 397 E				F.A.P. 397 E				F.A.P. 397 E			
1	449+82-455+97	✓	615	1	456+00	✓	6	1	456+00	✓	1
1	449+82-455+97	✓	615	1	456+00	✓	6	1	456+00	✓	1
1	456+01-460+32	✓	431	1	460+35	✓	40	1	460+35	✓	1
1	456+01-460+32	✓	431	1	460+35	✓	46	1	460+35	✓	1
1	462+64-464+97	✓	233	1	465+00	✓	26	1	465+00	✓	1
1	462+64-464+97	✓	233	1	465+00	✓	50	1	465+00	✓	1
1	469+01-472+47	✓	346	1	469+00	✓	32	1	469+00	✓	1
1	469+01-472+47	✓	346	1	469+00	✓	34	1	469+00	✓	1
1	472+51-476+38	✓	367	1	472+50	✓	48	1	472+50	✓	1
1	472+51-476+38	✓	367	1	472+50	✓	44	1	472+50	✓	1

PLAN
SECTION
PROFILES
DETAILS
CONNECTIONS
ADJUSTMENTS
REVISIONS

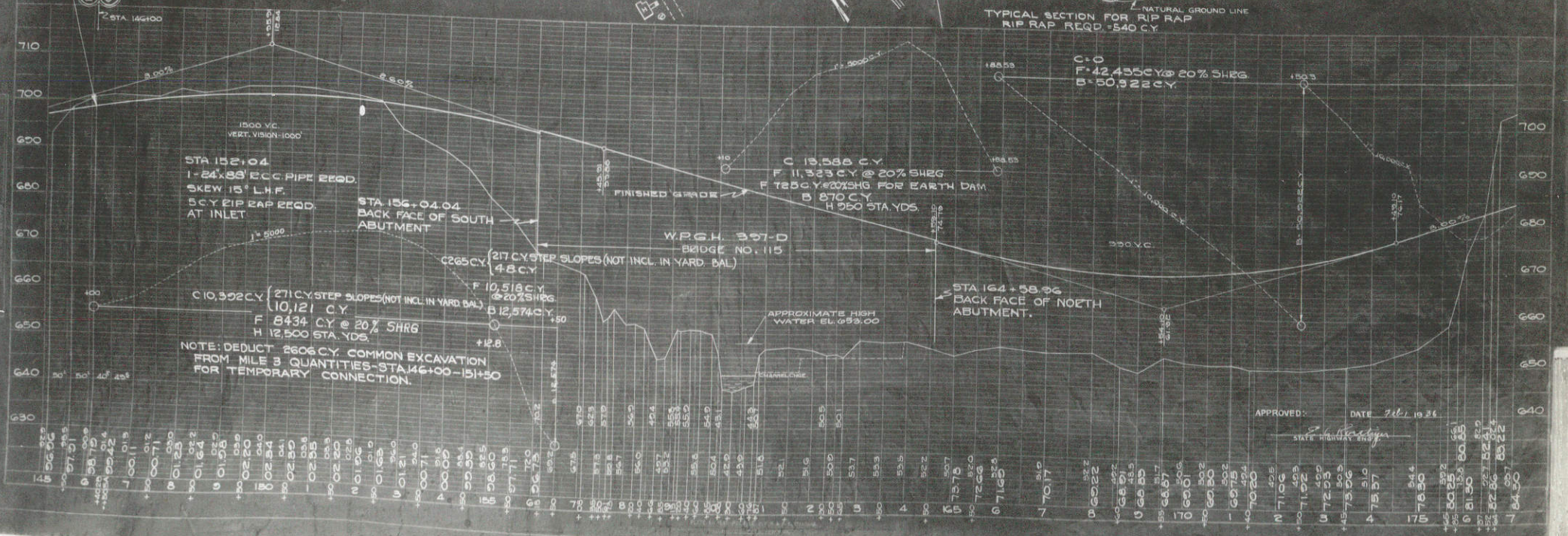


ELEV. 655.0
 RIP RAP 2 FT THICK AROUND END OF FILL & FROM STA. 164+52 TO STA. 163+59 RT. & LT.
 RIP RAP 1 FT THICK STA. 165+59-175+00 RT.

BENCH MARKS

NO. STATION	DESCRIPTION	EL.
152+00	BON W END GAS PUMP FOUND	703.20
158+00	SPK. IN PP ANCHOR 113 RT.	670.17
161+30	SPK. IN 26' COTWD. 75 RT.	653.43

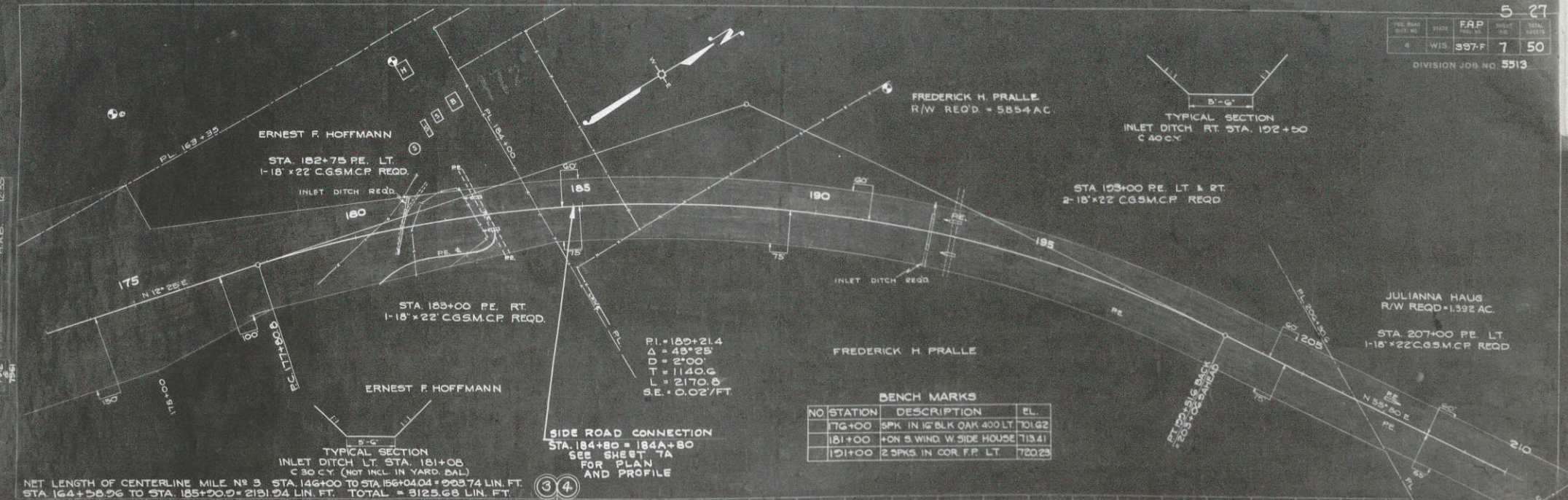
TYPICAL SECTION FOR RIP RAP
 RIP RAP REQD. 540 C.Y.



APPROVED: *[Signature]* DATE 7/17/1936
 STATE HIGHWAY ENG.

NO. ROAD DIST. NO.	PLAN	F.A.P. PROJ. NO.	SHEET NO.	TOTAL SHEETS
6	WIS.	397-F	7	50

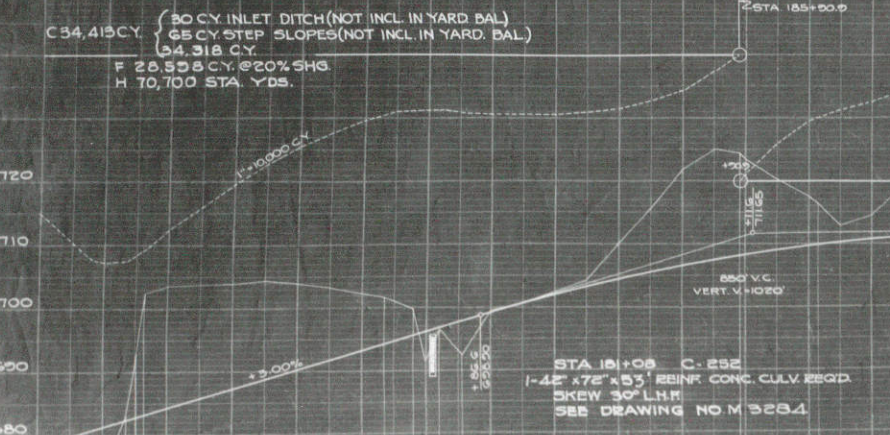
DIVISION JOB NO. 5313



BENCH MARKS

NO.	STATION	DESCRIPTION	EL.
176	+00	SPK IN 16' BLK OAK 400 LT	701.62
181	+00	+ON S. WIND. W. SIDE HOUSE	713.41
191	+00	2 SPKS IN COR. F.P. LT	720.23

NET LENGTH OF CENTERLINE MILE NO. 3 STA. 146+00 TO STA. 156+00 = 10000.00 LIN. FT.
 STA. 164+56.26 TO STA. 185+00.00 = 20543.74 LIN. FT. TOTAL = 31243.74 LIN. FT.



C 16,580 CY (40CY INLET DITCH INCL IN YARD BAL)
 F 13,816 CY @ 20% SHS
 H 131,750 STA. YDS.

(80 CY INLET DITCH (NOT INCL. IN YARD BAL))
 C 34,413 CY
 F 28,558 CY @ 20% SHS
 H 70,700 STA. YDS.

APPROVED: DATE: 7-2-1936

9-35
 175
 180
 185
 190
 195
 200
 205
 210

9-35
 175
 180
 185
 190
 195
 200
 205
 210

PLAN
 SHEET NO. 2
 PROJECT NO. 197F
 DIVISION JOB NO. B513

C.D. 1978
 1151

1151
 200



BENCH MARKS

NOTATION	DESCRIPTION	E.L.
16+00	SPK. IN 1 1/2" BLK OAK 400' LT.	113.41
15+00	NON SWIND W/SIDE HOUSE	113.41
10+00	2 SPKS. IN COR. RP. LT.	120.23

STA 181+03 DOWN DRAIN LT.
 GROUTED RUBBLE PAVING REQD.
 SEE TYPICAL SECTION SHEET NO. 2-A
 STA 176+08 DOWN DRAIN LT.
 GROUTED RUBBLE PAVING REQD.
 SEE TYPICAL SECTION SHEET NO. 2-A
 STA 181+09 DITCH PROTECTION RT.
 GROUTED RUBBLE PAVING REQD.
 SEE TYPICAL SECTION SHEET NO. 2-B

ERNEST F. HOFFMANN
 GOOD CONDITION - RESET
 1-1/2" 22' CGS M.C.P. IN PLACE
 STA 182+80 RE.LT.
 P.I. 180+21.8
 Δ = 43° 25'
 D = 2° 00'
 T = 1140.6
 L = 2170.8
 SE = 0.02/FT.

STA 185+90.9 TO 194+00
 C = 387.0' C.Y.
 F = 1.22 C.Y. @ 30% 185 C.Y.
 HAUL 279 C.Y. TO SIDE ROAD RT. STA 255+40
 STA 185+90.9 TO 185+90.9
 STA 185+90.9 TO 185+90.9
 EQUIVALENT TO MILE NO. 4
 MILE NO. 1
 OF F.A.P. 587 F.

STA 197+00 TO 207+00
 C = 493 C.Y.
 F = 0 C.Y.
 HAUL 398 C.Y. TO MARSH RT. STA.
 258+40 TO 261+50

STA 192+50 CULV. NO. 251
 1-42" x 72" x 56" REINF. CONC. CULV. IN PLACE

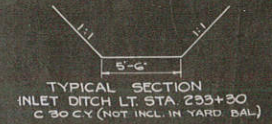
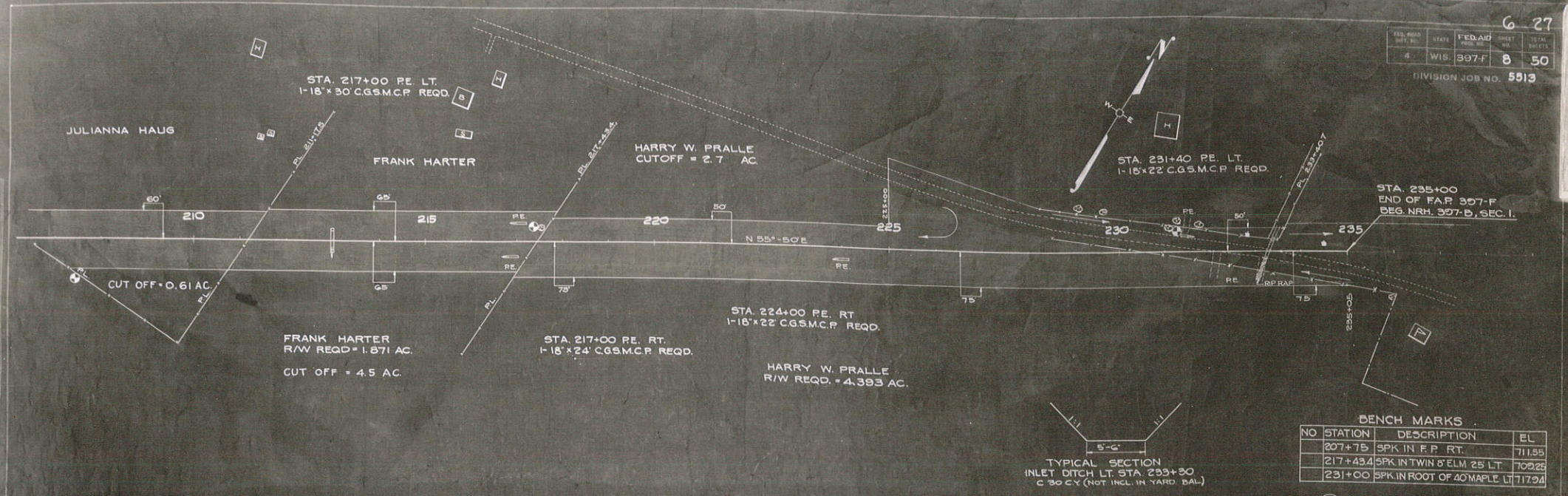
STA 184+08 CULV. NO. 252
 1-42" x 72" x 53" REINF. CONC. CULV. IN PLACE
 SKEW 30° L.H.F.

DIVISION JOB NO. B513

NO.	DATE	BY	WISC.
1	10/27/78	J.P.	197F

J.R.A.
 E.C.V.
 H.A.B.
 7853

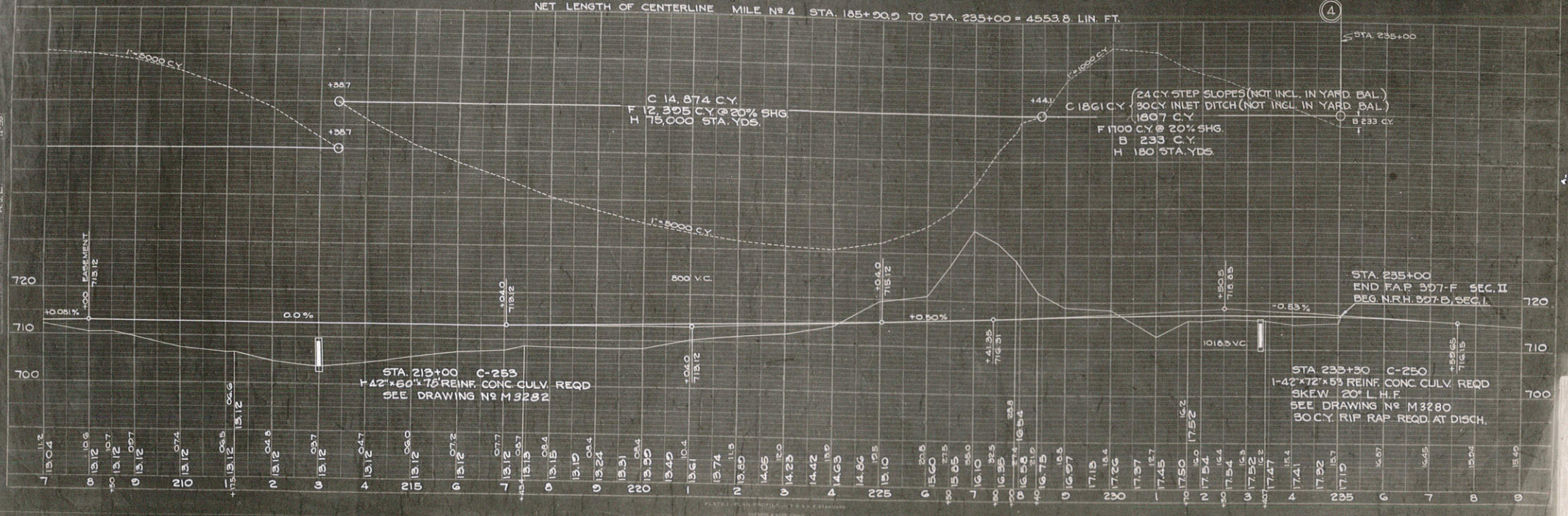
J.R.A.
 E.C.V.
 H.A.B.
 7853



BENCH MARKS

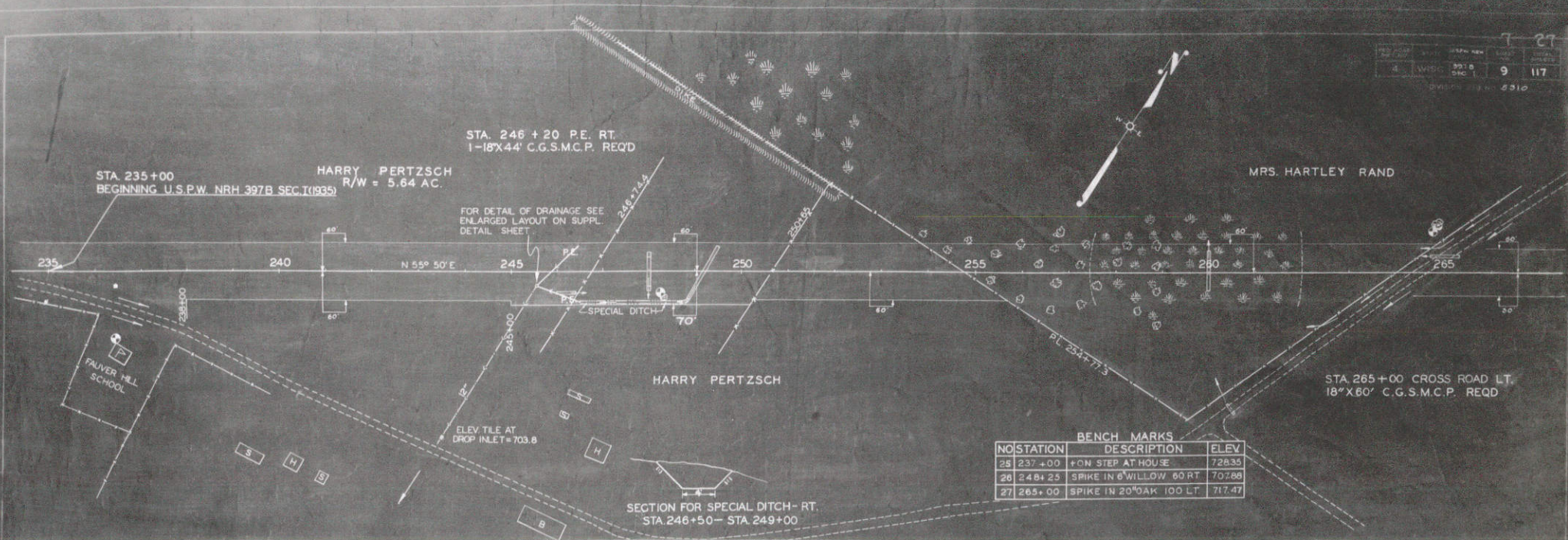
NO	STATION	DESCRIPTION	EL
	207+75	SPK. IN T.P. RT.	711.55
	217+43.4	SPK. IN TWIN 8' ELM. 25' LT.	709.28
	231+00	SPK. IN ROOT OF 40' MAPLE LT	717.94

NET LENGTH OF CENTERLINE MILE # 4 STA. 185+00.0 TO STA. 235+00 = 4553.8 LIN. FT.



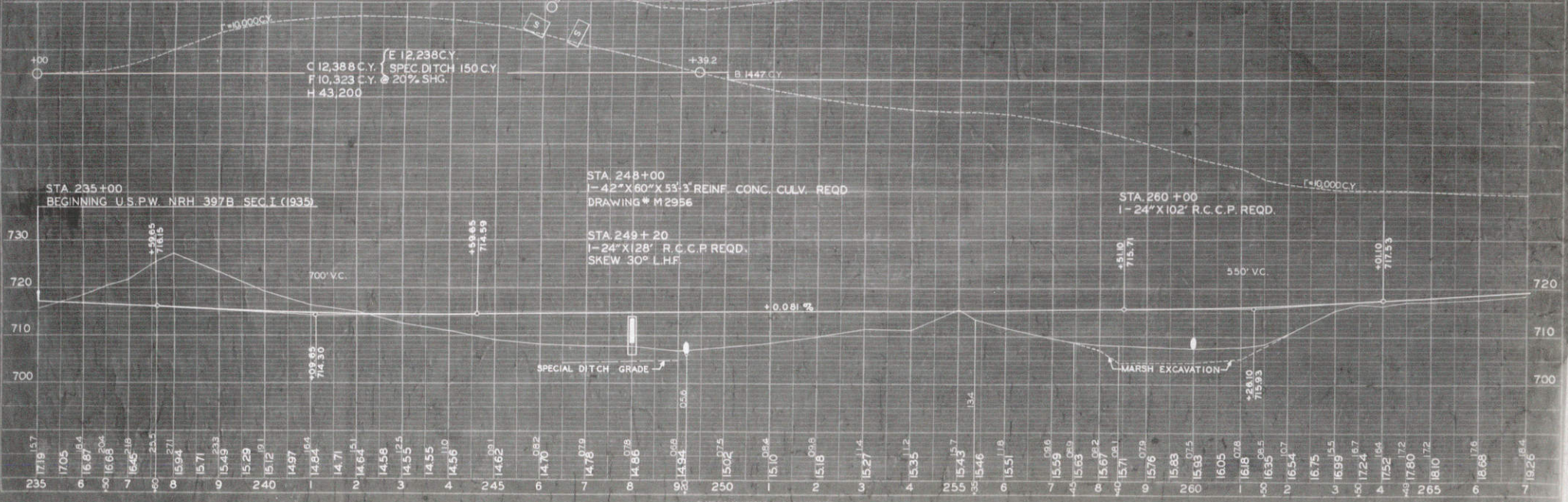
STA. 213+00 C-253
 1-42"x60"x75 REINF. CONC. CULV. REQD
 SEE DRAWING NO M3292

STA. 233+30 C-250
 1-42"x72"x53 REINF. CONC. CULV. REQD
 SKEW 20° L.H.F.
 SEE DRAWING NO M3280
 30 CY. RIP RAP REQD. AT DISCH.



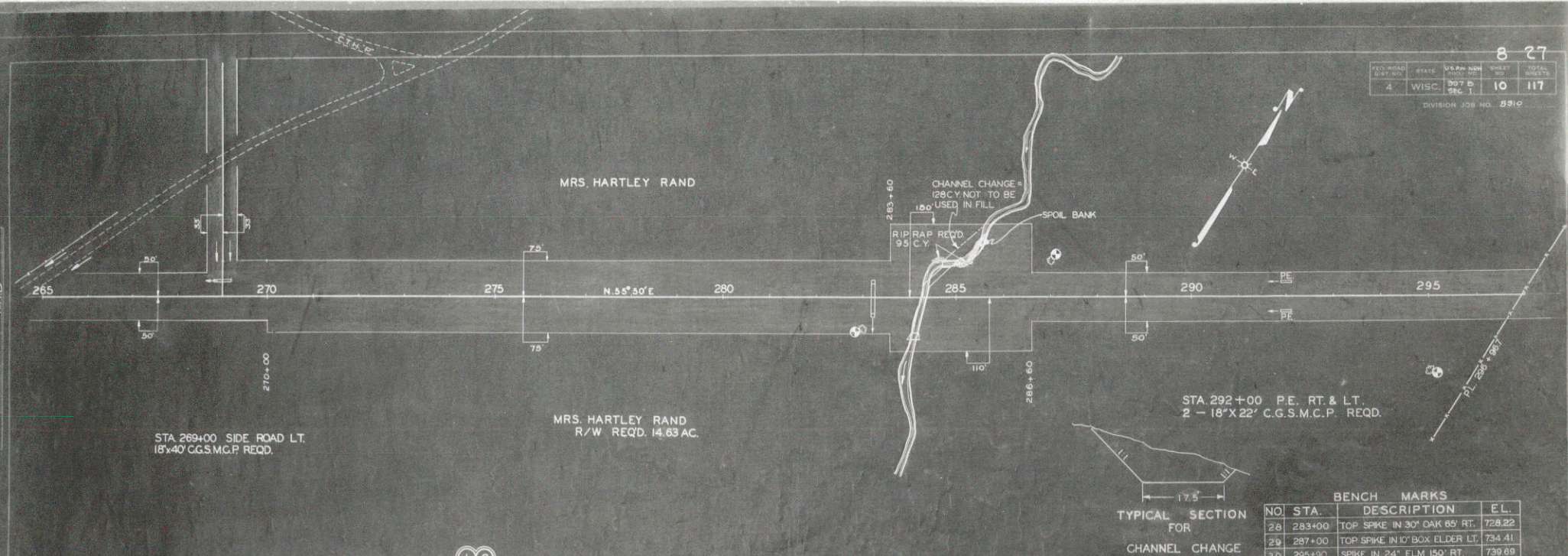
NO	STATION	DESCRIPTION	ELEV
25	237+00	+ON STEP AT HOUSE	728.35
26	248+25	SPIKE IN 6" WILLOW 60 RT.	707.89
27	265+00	SPIKE IN 20" OAK 100 LT.	717.47

SECTION FOR SPECIAL DITCH - RT.
 STA 246+50 - STA 249+00



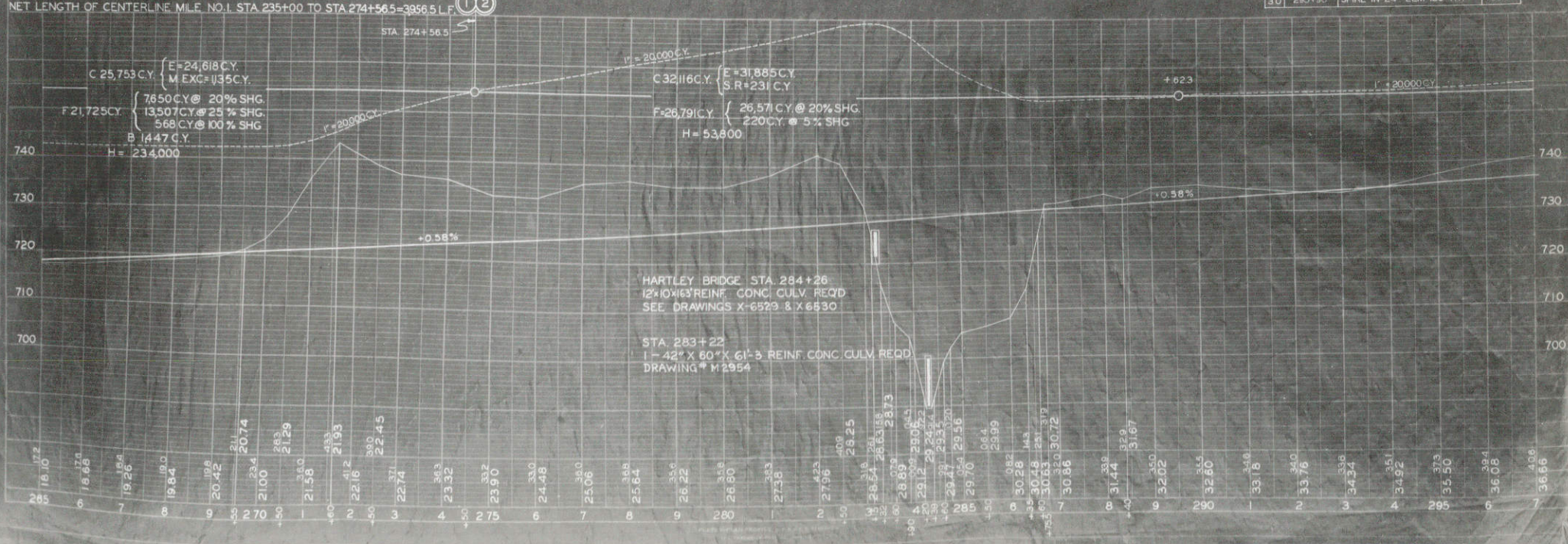
7885
 7886
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 7889

7890
 7891
 7892
 7893
 7894



DATE	1/15/54
BY	J.P.C.
CHECKED	L.P.C.
SCALE	AS SHOWN
PROJECT	STATE HIGHWAY 10
SECTION	1
SHEET	10
TOTAL SHEETS	117

DATE	1/15/54
BY	J.P.C.
CHECKED	L.P.C.
SCALE	AS SHOWN
PROJECT	STATE HIGHWAY 10
SECTION	1
SHEET	10
TOTAL SHEETS	117

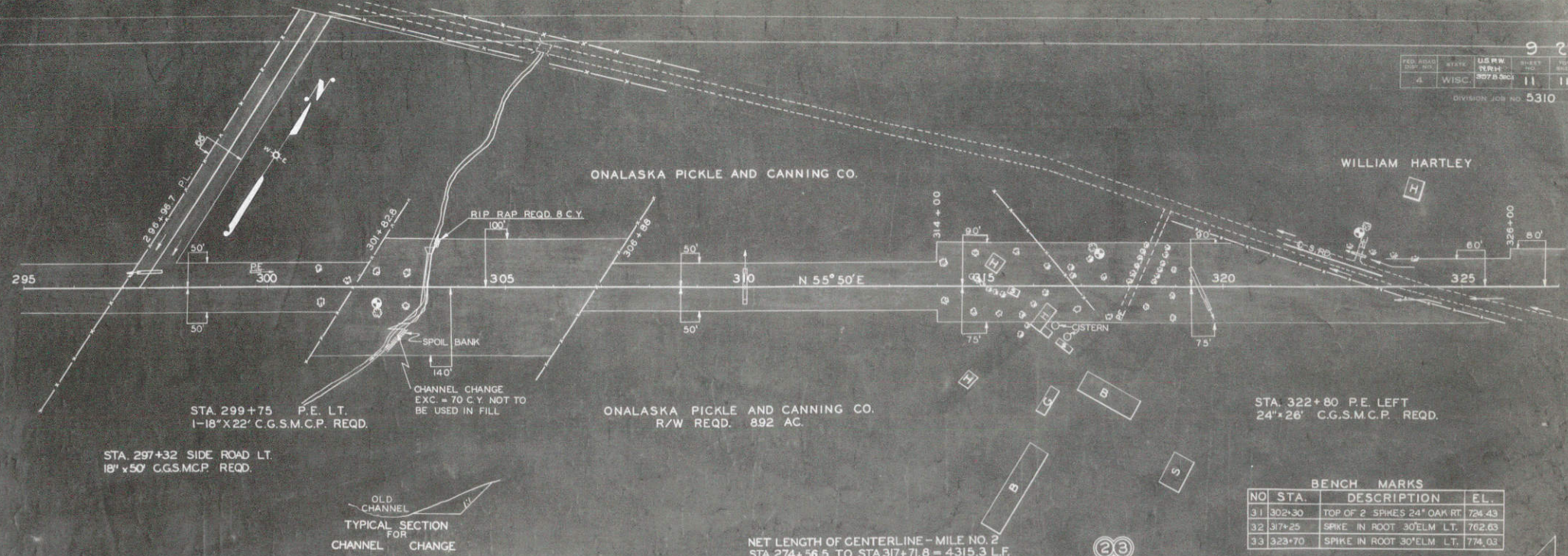


NET LENGTH OF CENTERLINE MILE, NO. I. STA 235+00 TO STA 274+56.5 = 3956.51 F

STA.	EL.	DESCRIPTION
265	717.2	
6	18.10	
7	18.66	
8	19.26	
9	19.84	
10	20.42	
11	20.74	
12	21.00	
13	21.29	
14	21.58	
15	21.93	
16	22.16	
17	22.45	
18	22.74	
19	23.02	
20	23.32	
21	23.59	
22	23.90	
23	24.18	
24	24.48	
25	24.74	
26	25.06	
27	25.38	
28	25.64	
29	25.96	
30	26.22	
31	26.80	
32	27.35	
33	27.96	
34	28.25	
35	28.51	
36	28.63	
37	28.73	
38	28.89	
39	29.06	
40	29.12	
41	29.24	
42	29.33	
43	29.42	
44	29.56	
45	29.70	
46	29.82	
47	29.99	
48	30.28	
49	30.48	
50	30.73	
51	30.93	
52	30.72	
53	30.96	
54	31.44	
55	31.67	
56	32.02	
57	32.60	
58	33.18	
59	33.76	
60	34.24	
61	34.92	
62	35.50	
63	36.08	
64	36.56	

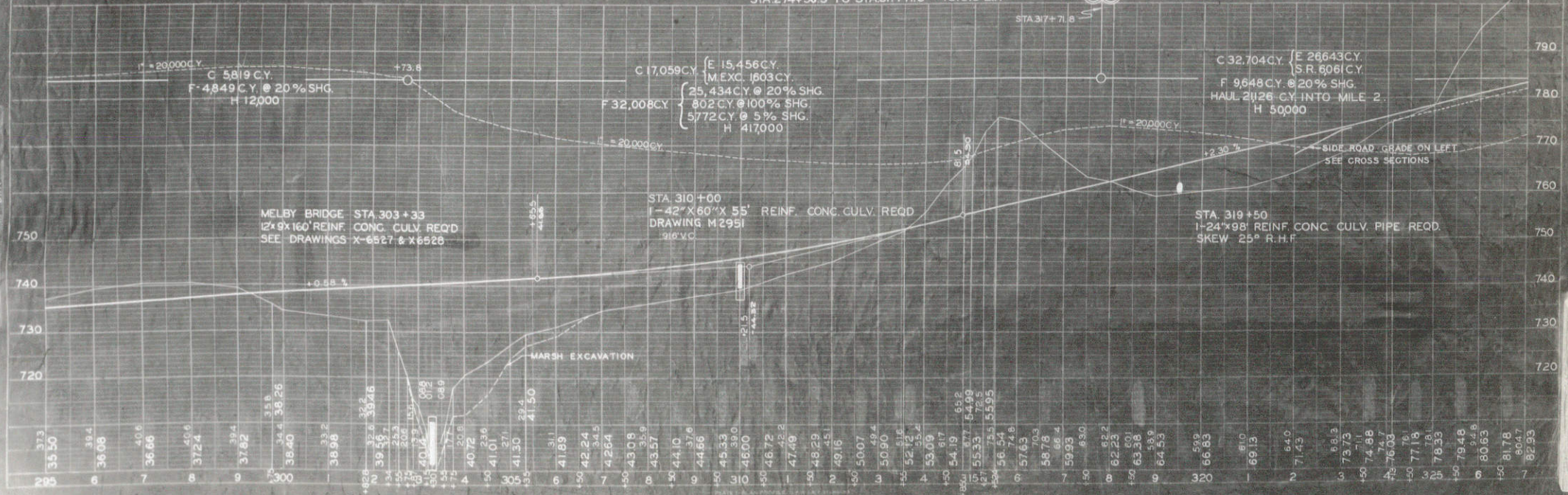
BENCH MARKS

NO.	STA.	DESCRIPTION	EL.
28	283+00	TOP SPIKE IN 30" OAK 65' RT.	728.22
29	287+00	TOP SPIKE IN 10" BOX ELDER LT.	734.41
30	295+90	SPIKE IN 24" ELM 150' RT.	739.89



BENCH MARKS

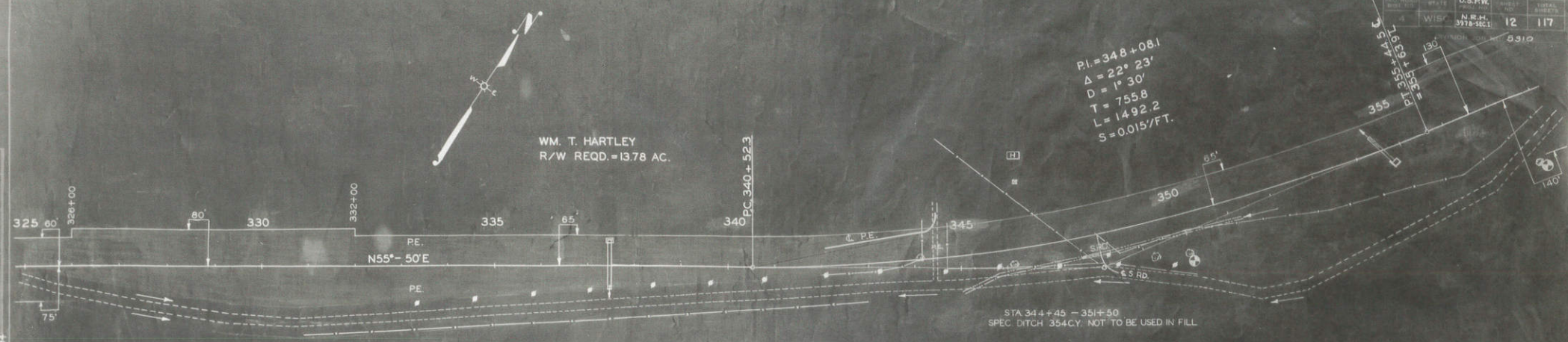
NO.	STA.	DESCRIPTION	E.L.
31	302+30	TOP OF 2 SPIKES 24" OAK RT.	724.43
32	317+25	SPIKE IN ROOT 30'ELEM. LT.	762.63
33	323+70	SPIKE IN ROOT 30'ELEM. LT.	774.03



DATE: 11/11/50
 DRAWN BY: J. J. ...
 CHECKED BY: ...

DATE: 11/11/50
 DRAWN BY: J. J. ...
 CHECKED BY: ...

U.S.P.W.	SHEET NO.	TOTAL SHEETS
WIS.	12	117
N.E.H. 3978-SEC.11		



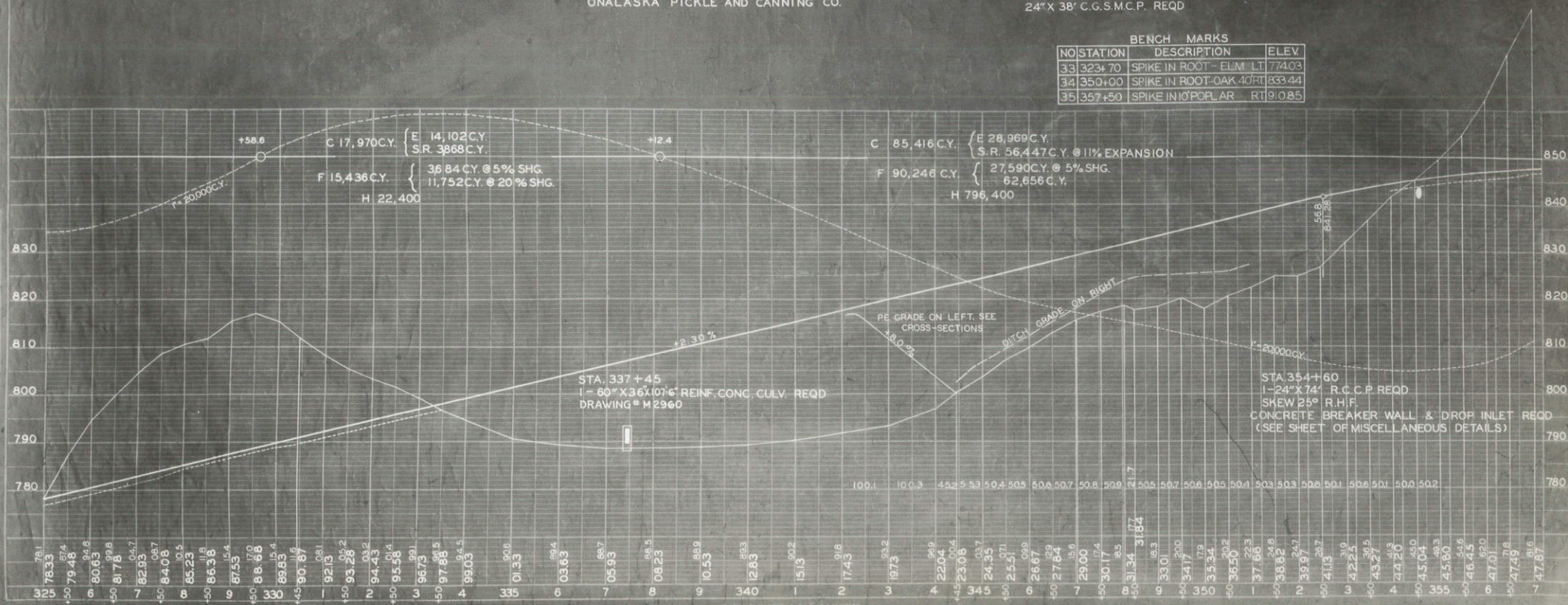
WM. T. HARTLEY
R/W REQD.=13.78 AC.

ONALASKA PICKLE AND CANNING CO.

STA. 348+00 S.RD.RT.
24" X 38' C.G.S.M.C.P. REQD

BENCH MARKS

NO	STATION	DESCRIPTION	ELEV
33	323+70	SPIKE IN ROOT - ELM LT	774.03
34	350+00	SPIKE IN ROOT - OAK 40 FT	833.44
35	357+50	SPIKE IN ROOT - POPLAR RT	910.85



STA. 337+45
1-60" X 36" X 10' 6" REINF. CONC. CULV. REQD
DRAWING # M2960

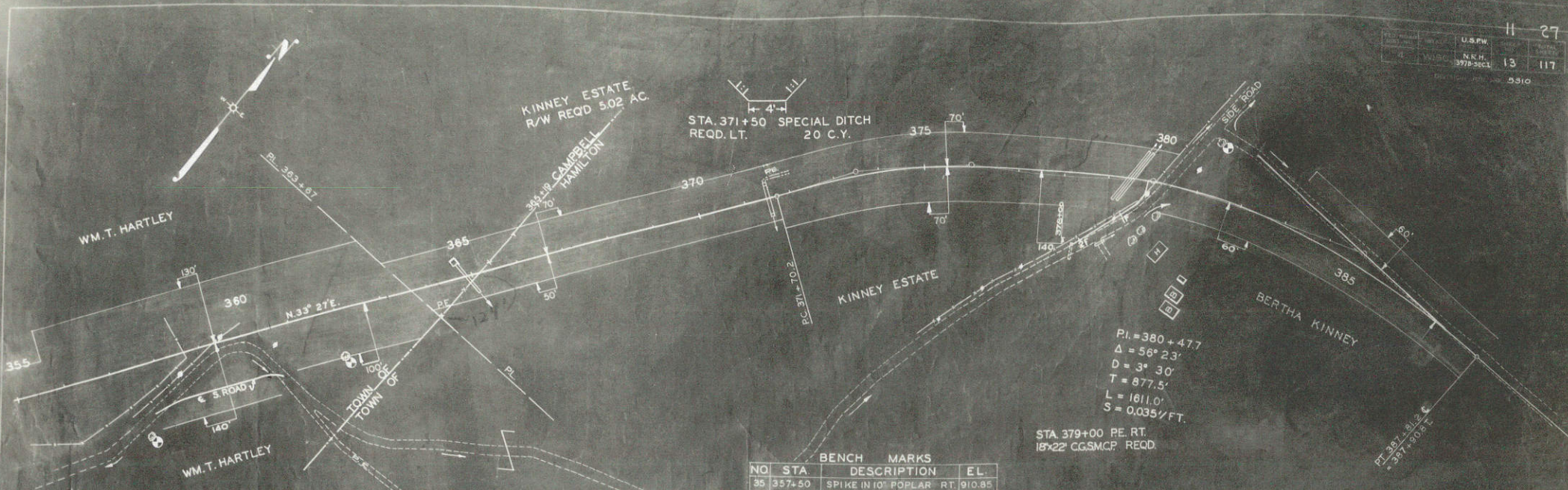
STA. 354+60
1-24" X 38" R.C.P. REQD
SKEW 25° R.H.F.
CONCRETE BREAKER WALL & DROP INLET REQD
(SEE SHEET OF MISCELLANEOUS DETAILS)

FILE NO. 7892

J.P.A.	C.S.
S.W.	
J.T.O.	
M.A.B.	

FILE NO. 7892

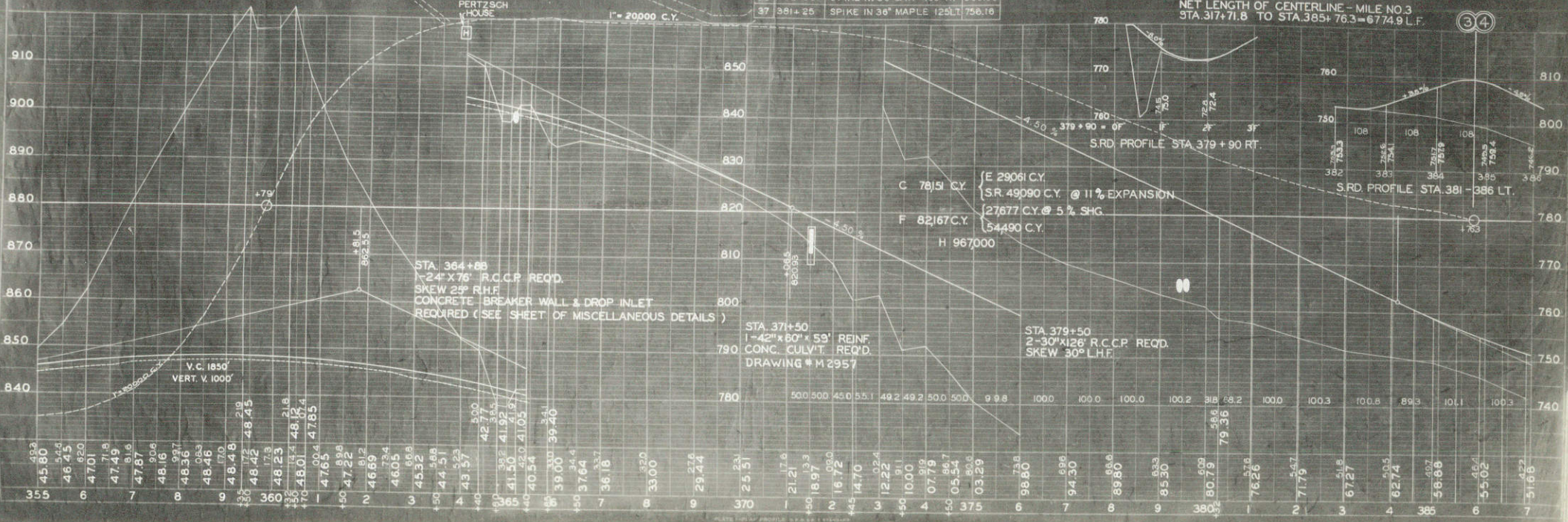
J.P.A.	Z.S.
S.W.	
J.T.O.	
M.A.B.	



BENCH MARKS

NO	STA	DESCRIPTION	EL.
35	357+50	SPIKE IN 10' POPLAR RT.	910.85
36	362+00	SPIKE IN 30' OAK 100' RT.	900.00
37	381+25	SPIKE IN 36' MAPLE 125' LT.	796.16

NET LENGTH OF CENTERLINE - MILE NO. 3
 STA. 317+71.8 TO STA. 385+76.3 = 6774.9 L.F.



DATE: 1/28/51
 DRAWN BY: J.P.C.
 CHECKED BY: J.P.C.
 APPROVED BY: J.P.C.

DATE: 1/28/51
 DRAWN BY: J.P.C.
 CHECKED BY: J.P.C.
 APPROVED BY: J.P.C.