

INDEX OF SHEETS

SHEET NO. 1	TITLE
SHEET NO. 2-26	TYPICAL CROSS SECTIONS & MISCELLANEOUS DETAILS
SHEET NO. 3	ESTIMATE OF QUANTITIES
SHEET NO. 3A-3B	MISCELLANEOUS QUANTITIES
SHEET NO. 4	RIGHT OF WAY PLAT
SHEET NO. 5-27	PLAN AND PROFILE STA. 13+30 TO STA. 66+55
SHEET NO. 28-82	DRAINAGE STRUCTURES AND STANDARD DETAILS
SHEET NO. 83-110	CROSS SECTIONS

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	397-G	1	110

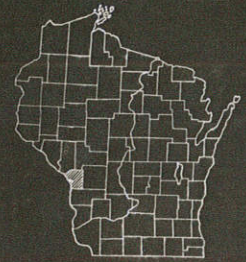
DIVISION JOB NO. 5545

STATE OF WISCONSIN
WISCONSIN HIGHWAY COMMISSION

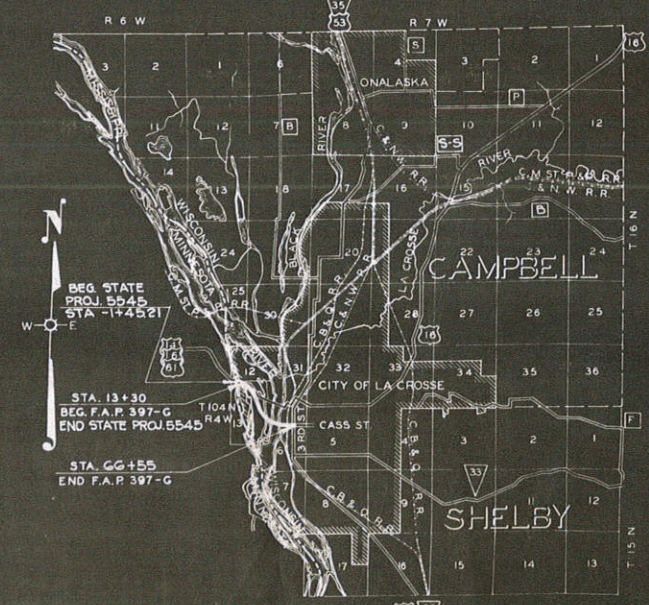
PLAN AND PROFILE OF PROPOSED
SPARTA-LA CROSSE ROAD
MISSISSIPPI RIVER BRIDGE
LA CROSSE COUNTY

397 G FEDERAL AID PROJECT 397 G

BEGINNING AT A POINT NEAR THE CENTER OF LOT 6, SEC. 13, T104 N, R 4 W, 5 TH P.M. AND EXTENDING SOUTHEASTERLY TO A POINT NEAR THE INTERSECTION OF THIRD AND CASS STREETS IN THE CITY OF LA CROSSE



SCALES: PLAN 1 IN. = 100 FT. 5/16" = 20' 1" = 10'
 PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT. W. 1/20 FT. HOR. 1/2 FT. VERT. 1/4 IN. = 1 FT.



STATE PROJECT 5545
 BEGINNING AT THE EAST ABUTMENT OF THE WEST CHANNEL BRIDGE AND EXTENDING EASTERLY TO A POINT NEAR THE CENTER OF LOT 6, SEC. 13, T104N, R4W, 5TH P.M.

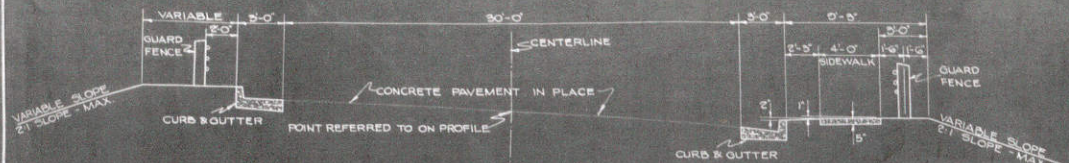
CONVENTIONAL SIGNS

STATE LINE	---	CULVERTS IN PLACE	—
COUNTY LINE	---	CULVERTS REQUIRED	—
TOWNSHIP OR RANGE LINE	---	DROP INLET	—
SECTION LINE	---	POWER POLE	—
NEW RIGHT OF WAY LINE	---	TELEPHONE OR TELEGRAPH POLE	—
PRESENT RIGHT OF WAY LINE	---	RIGHT OF WAY MARKERS	—
WIRE FENCE	---	REFERENCE STAKE FOR HUBS ONLY	—
WOVEN	---	MARSH	—
BARBED	---	HEDGE	—
LOT LINE	---	TREES	—
CORPORATE OR CITY LIMITS	---	GROUND ELEVATION	—
PROPERTY LINE	---	GRADE ELEVATION	—
TRAVELED WAY OR P.E.	---		
RAILROADS	---		
BASE OR SURVEY LINE	---		

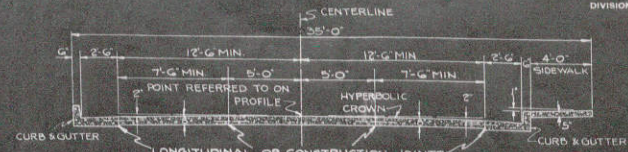
LAYOUT SCALE 0 MI. 1 MI. 2 MI.

TOTAL NET LENGTH OF CENTERLINE	1306 MI.
WEST APPROACH	0.469 MI.
BRIDGE	0.470 MI.
EAST APPROACH	0.055 MI.
STATE PROJECT	0.263 MI.

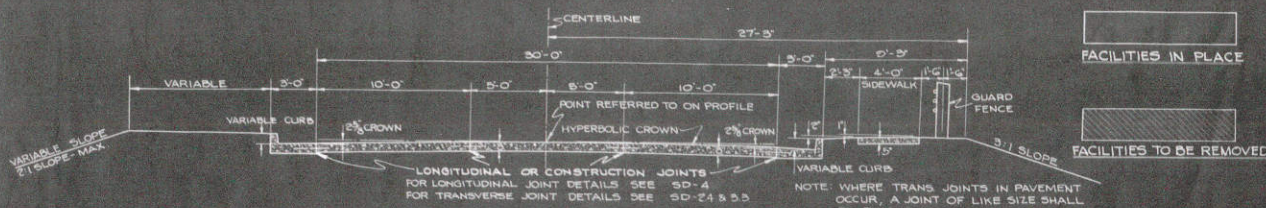
WISCONSIN HIGHWAY COMMISSION MADISON, WIS.		U.S. DEPARTMENT OF AGRICULTURE BUREAU OF PUBLIC ROADS	
Surveyor: J. R. A.	Note Book: 8050-8058	RECOMMENDED FOR APPROVAL:	
Div. Computer: J. R. A.	M. D. Checker: W. F. H.	_____	
Div. Checker: R. G. J.	Correct: 6-7-31	DISTRICT ENGINEER	
CORRECT:		_____	
T. M. Keywold		CHIEF ENGINEER	
RECOMMENDED FOR APPROVAL:		APPROVED:	
Frank Brown		_____	
CONV. ENGINEER		DIRECTOR	
APPROVED:		DATE: _____	
E. G. Ruetz		DATE: _____	
STATE HIGHWAY ENGINEER			
CHECKED IN THE FIELD BY: A. C. Clark			



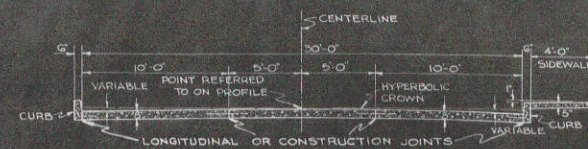
TYPICAL SECTION STA 0+00 TO STA. 13+32.1



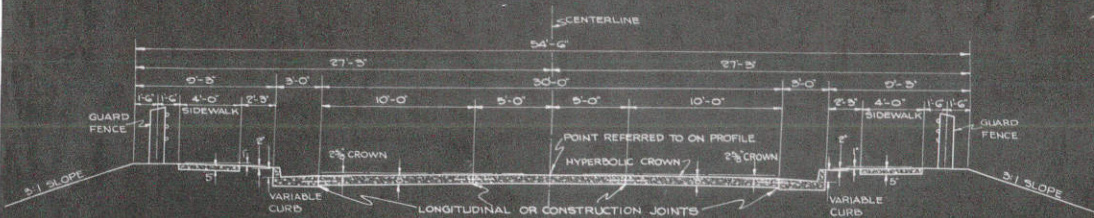
SECTION FOR CONCRETE PAVEMENT STA. 0A+68 BACK LT. STA. 10+50



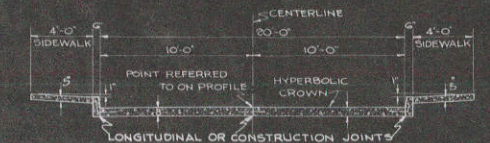
TYPICAL SECTION STA. 13+32.1 TO STA. 10+50



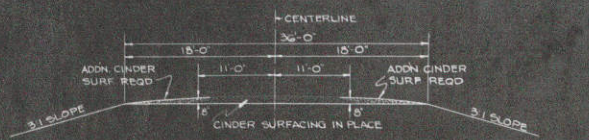
SECTION FOR CONCRETE PAVEMENT STA. 0A+68 AHEAD LT. STA. 10+50



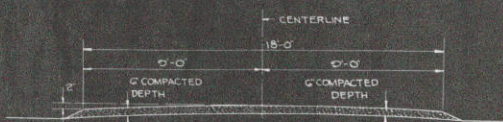
TYPICAL SECTION STA. 10+50 TO STA. 37+50



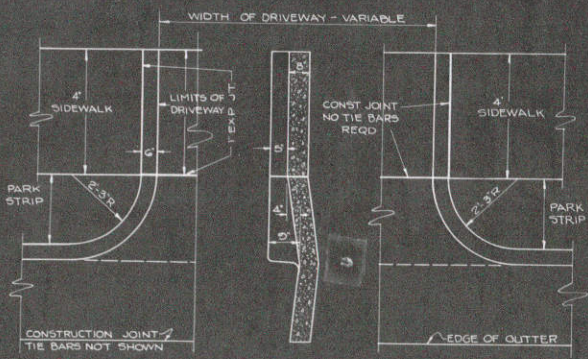
SECTION FOR CONCRETE PAVEMENT STA. 0S+48 RT. STA. 25+50



SECTION WITH CINDER SURFACING STA. 0S+48 TO 3S+00 AT SIDE ROAD RT. STA. 25+50



CINDER SURFACING SECTION RT. & LT. STA. 43+10



MODIFIED PLAN & SECTION AT PRIVATE ENTRANCES

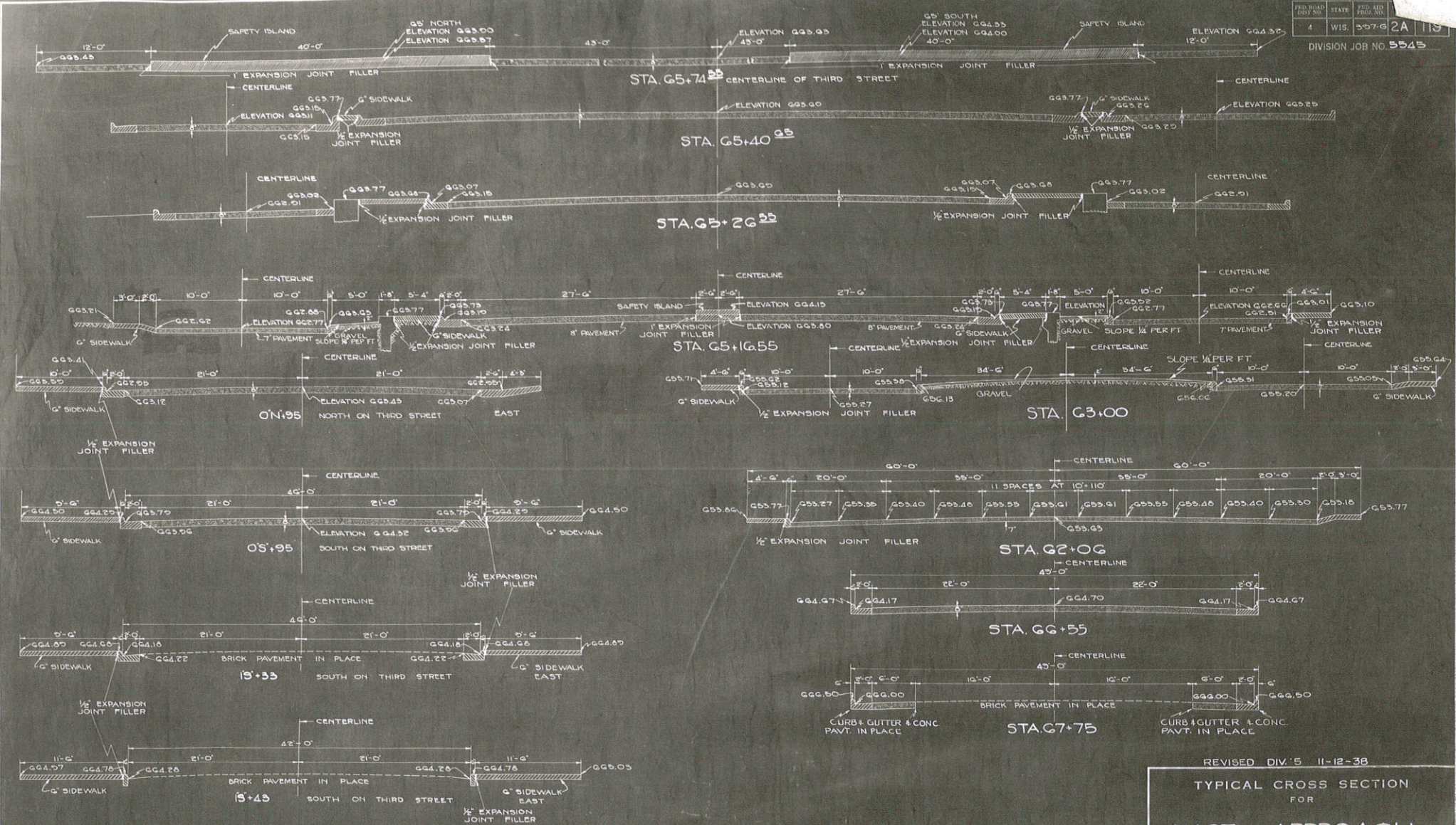


CURB & GUTTER DETAIL
 6" CURB = 0.0518 CY CONC. PER LIN. FT.
 5" CURB = 0.0364 CY CONC. PER LIN. FT.

CURB DETAIL
 6" CURB = 0.0208 CY CONC. PER LIN. FT.
 5" CURB = 0.0251 CY CONC. PER LIN. FT.
 11 1/2" CURB = 0.0285 CY CONC. PER LIN. FT.

- APPROVED APPLICABLE DETAIL DRAWINGS ARE:
- TRANSVERSE EXPANSION JOINTS SD-24 & 25
 - LONGITUDINAL JOINT DETAILS SD-4
 - TRANSVERSE CONSTRUCTION JOINTS SD-5
 - PAVEMENT REINFORCEMENT SD-5.2
 - CONCRETE HEADER BLOCK SD-5.4
 - WIRE CABLE GUARD FENCE SD-15.1
 - CONSTRUCTION BARRICADE SD-16.1
 - MARKER POSTS SD-12.2

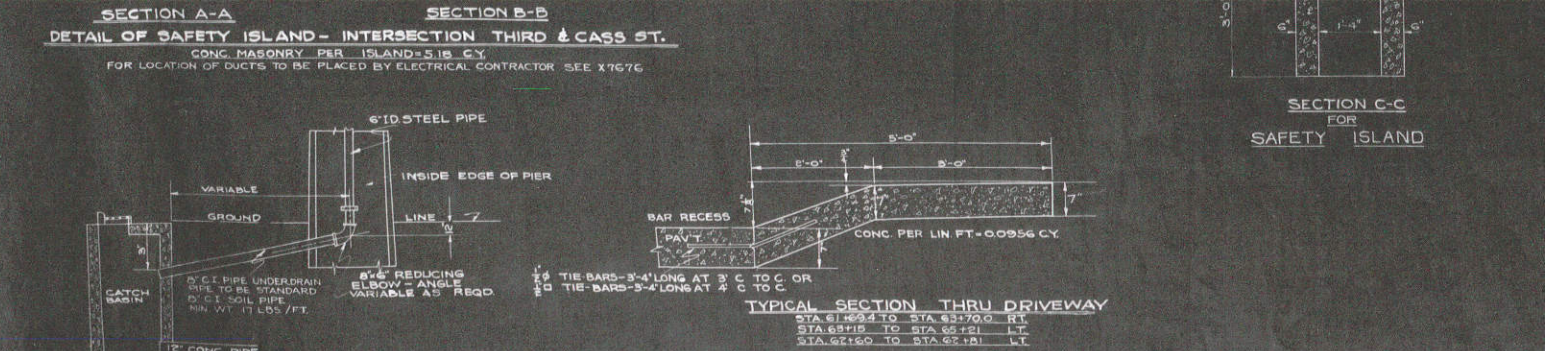
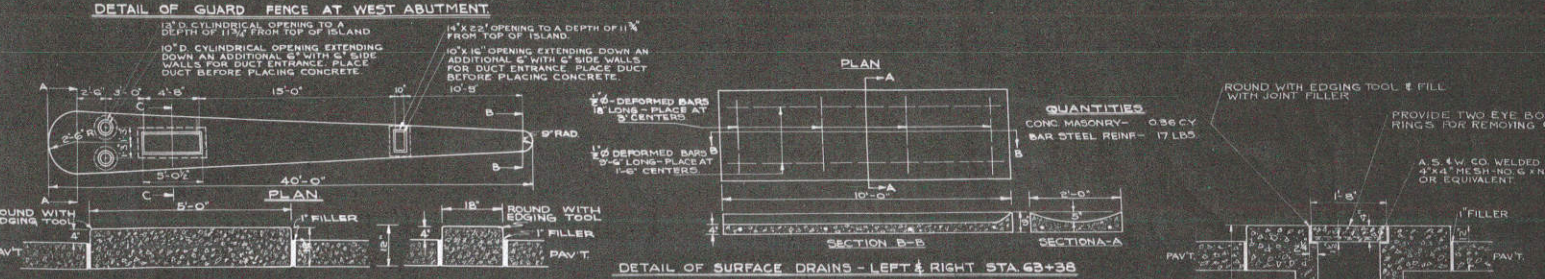
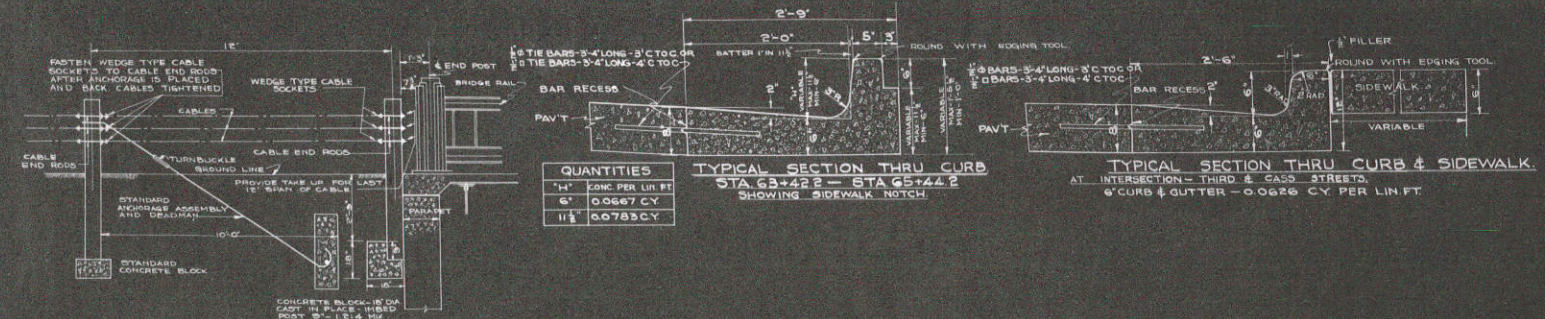
TYPICAL CROSS SECTION FOR 30' CONCRETE PAVEMENT AND MISCELLANEOUS DETAILS



REVISED DIV. 5 11-12-38
TYPICAL CROSS SECTION
FOR
**EAST APPROACH
PAVING**

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	397-6	2B	113

DIVISION JOB NO. 5545



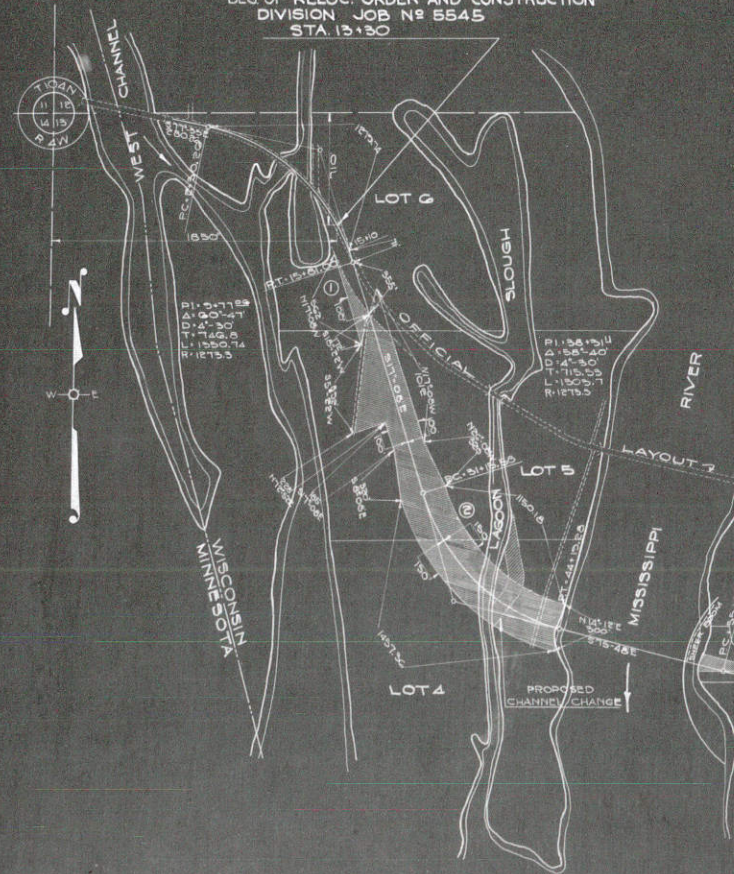
REVISED DIV 5 11/12/30

MISCELLANEOUS DETAILS.

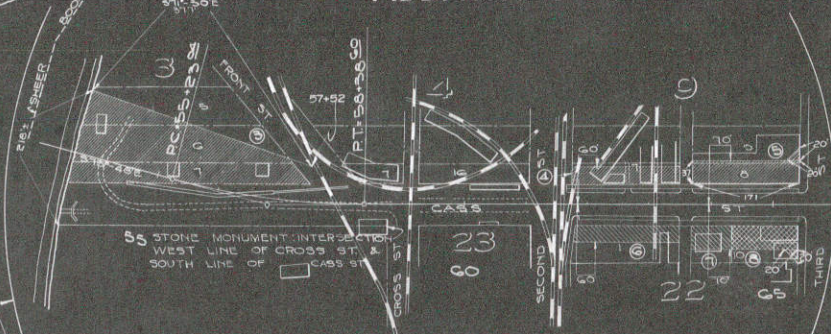
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC.	3976	4	110

DIVISION JOB NO. 5545

BEG OF RELOC. ORDER AND CONSTRUCTION
DIVISION JOB NO. 5545
STA. 13+30



DUNN, DOUSMAN & CAMERON
ADDITION



P. CAMERON ADDITION

SCALE: 0 100 200 300

STA. 66+07.55
END OF RELOC. ORDER
DIV. JOB NO. 5545

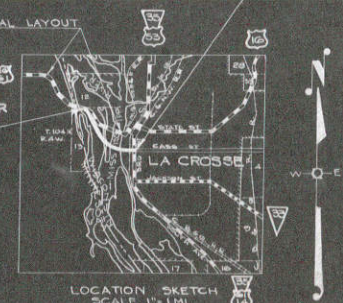
END OF CONSTRUCTION
STA. 67+83.5

STA. 13+30
BEG. OF RELOC. ORDER
DIV. JOB NO. 5545

LA CROSSE

INTERSECTION OF CENTERLINE OF
CASS STREET AND EAST
LINE OF THIRD STREET.

OFFICIAL LAYOUT



SCHEDULE OF R/W REQUIRED

PAR	OWNER	DESCRIPTION	ACRES	FENCING	RECORD
1	AUGUST MADER	LOTS 5, 6, SEC. 13, T104N, R4W	0.502		
2	PETTIBONE PARK COMM.	LOTS 4, 5, 6, SEC. 13, T104N, R4W	15.011		
3	LA CROSSE WOOL CO.	LOTS 5, 6, 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.773		
4	C. B. & Q. R. R.	LOT 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.106		
5	EARL F. SCHULTZ	LOT 8, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.149		
6	SEGELKE KOHLHAUS	LOT 1, BLK. 22, P. CAMERON ADD.	0.106		
7	KATIE HENDEL	LOT 10, BLK. 22, P. CAMERON ADD.	0.055		
8	LIZZIE KANARD	LOT 10, BLK. 22, P. CAMERON ADD.	0.050		
9A	ABE WILLIAMS (LEASEE)	FR. LOTS 5, 6, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.			
4A	WHITEBREAST COAL CO. (LEASEE)	LOT 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.			

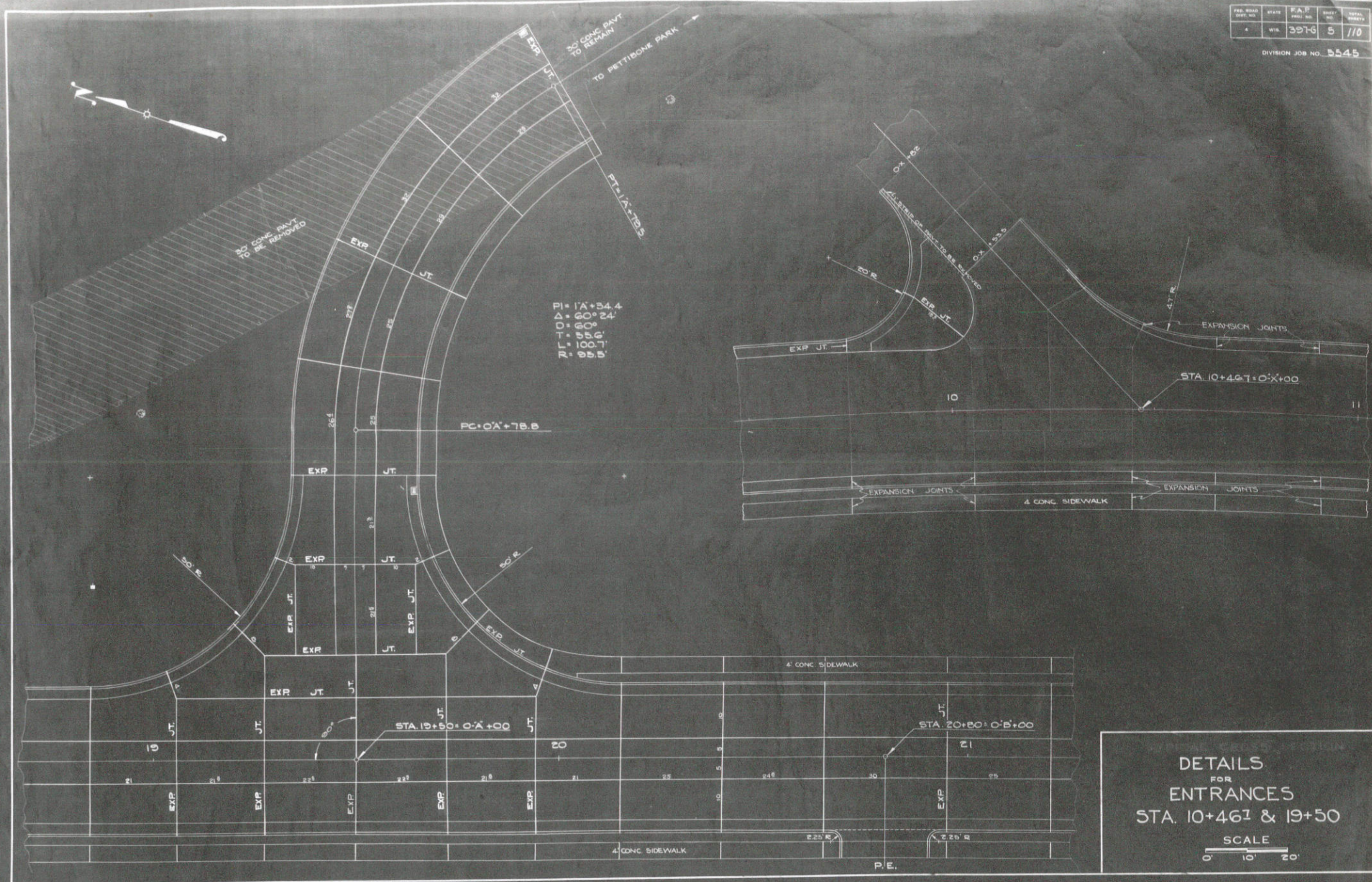
END OF RELOCATION ORDER
DIV. JOB NO. 5545
STA. 66+07.55

REVISED APR. 23, 1937
REVISED: NOV. 10, 1936.

PLAT OF RIGHT OF WAY REQUIRED
DIVISION JOB NO. 5545
LA CROSSE ~ LA CROSSE ROAD
U.S.H. NO. 16 & 61 LA CROSSE COUNTY
SCALE
0 400 800 1200

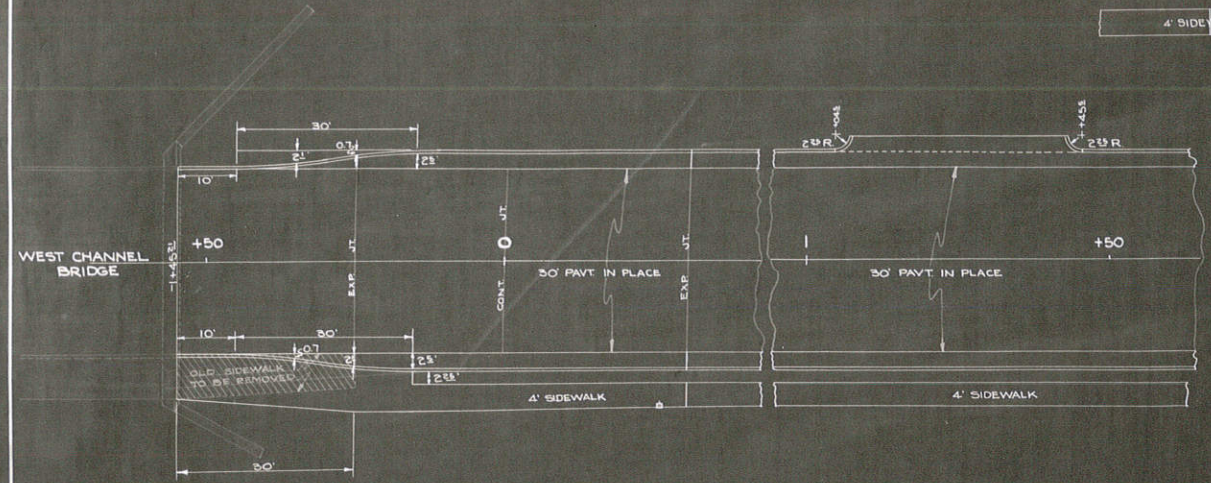
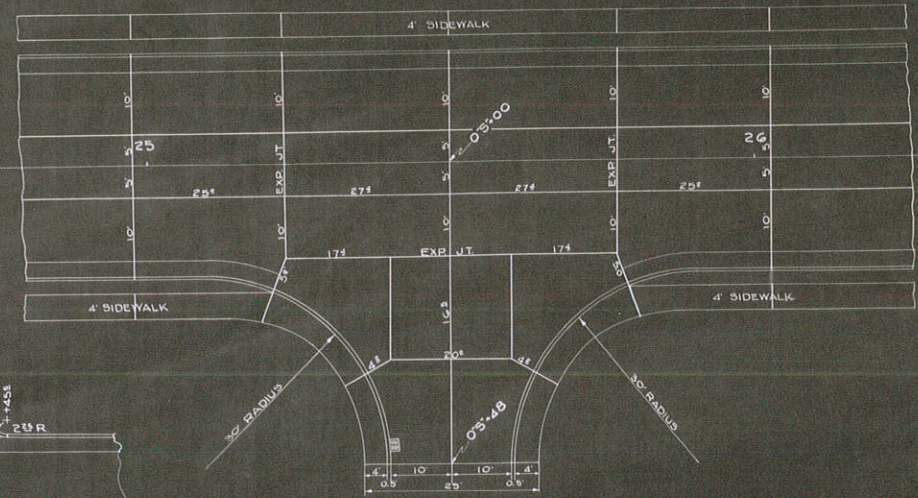
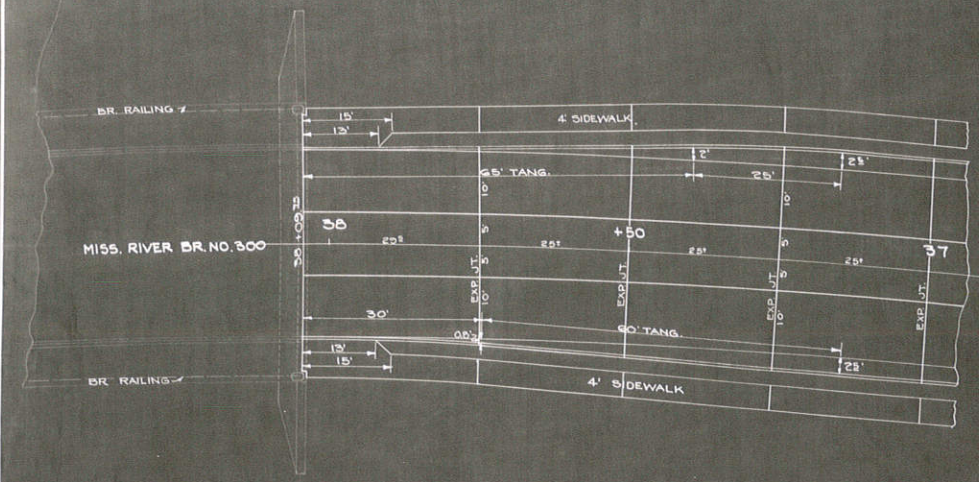
FED. ROAD DIST. NO.	STATE	F.A.P. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	W.V.	397-6	5	110

DIVISION JOB NO. 5545



DETAILS
FOR
ENTRANCES
STA. 10+467 & 19+50

SCALE
0' 10' 20'

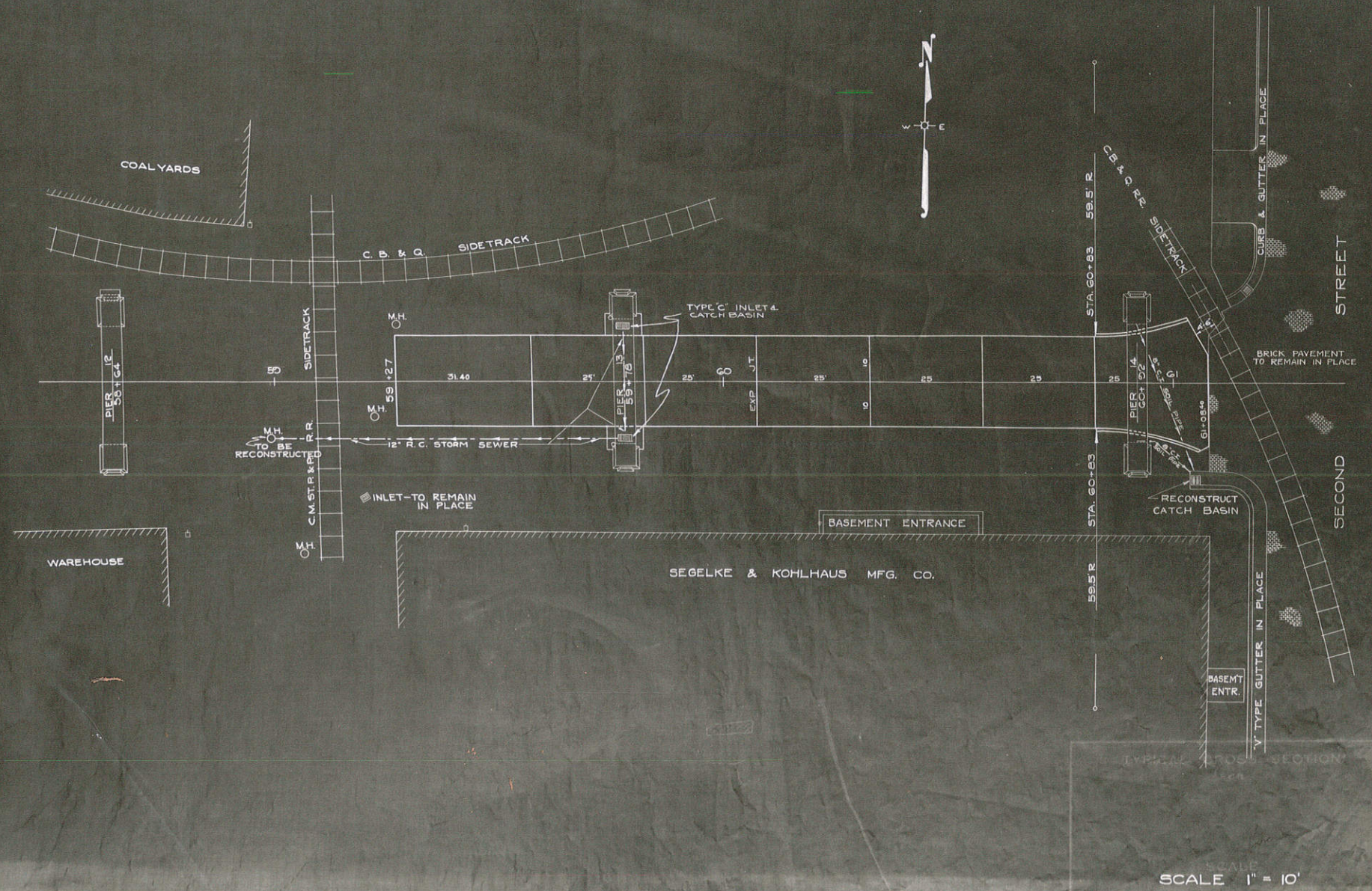


DETAILS
FOR
BRIDGE APPROACHES
AND
ENTRANCE STA. 25+50

SCALE
0' 10' 20'

FED. ROAD DIST. NO.	STATE	PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	W.V.	3974	7	110

DIVISION JOB NO. 5545

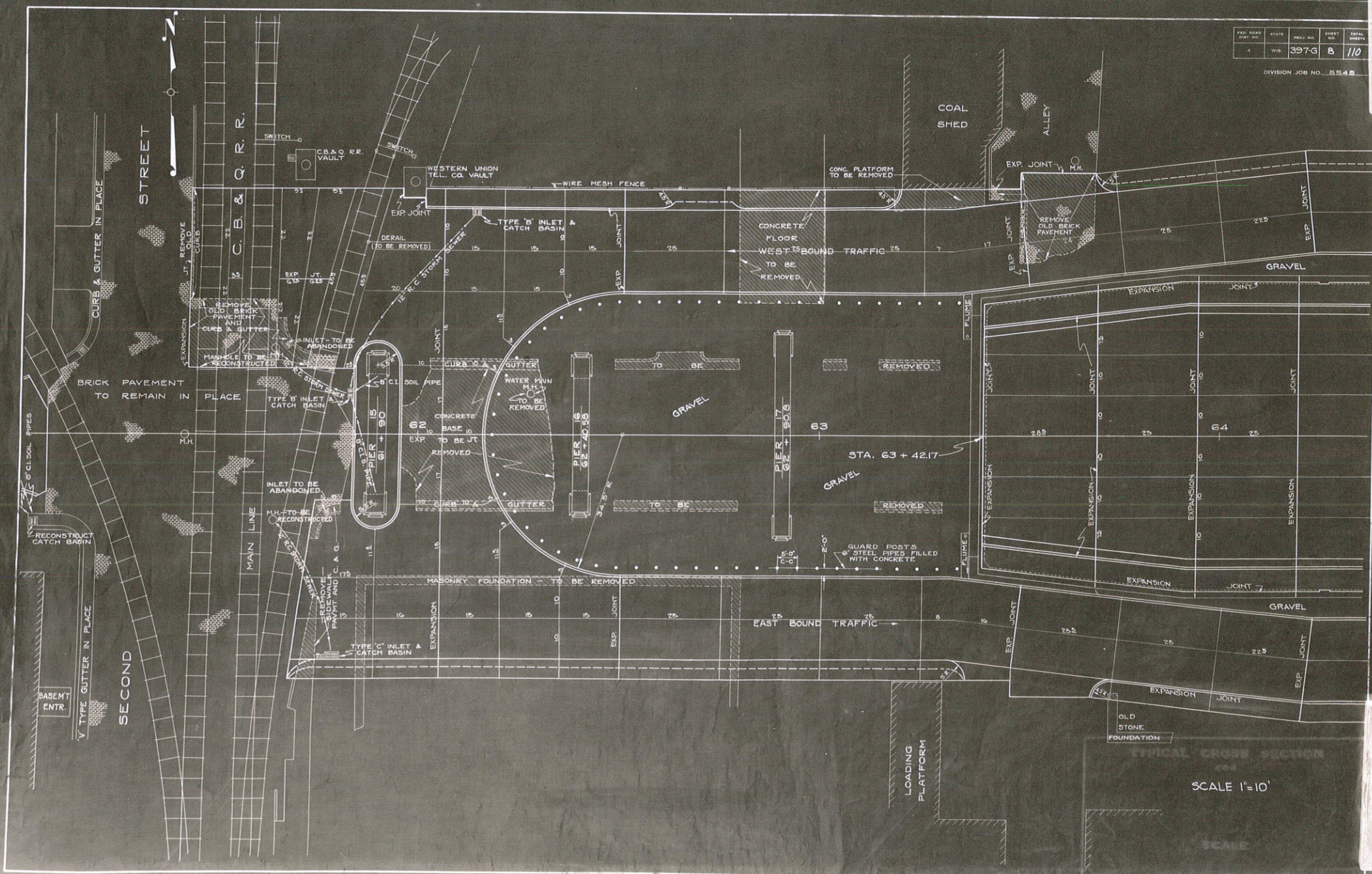


TYPICAL CROSS SECTION

SCALE 1" = 10'

FED. ROAD DIST. NO.	STATE	PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	397-G	8	110

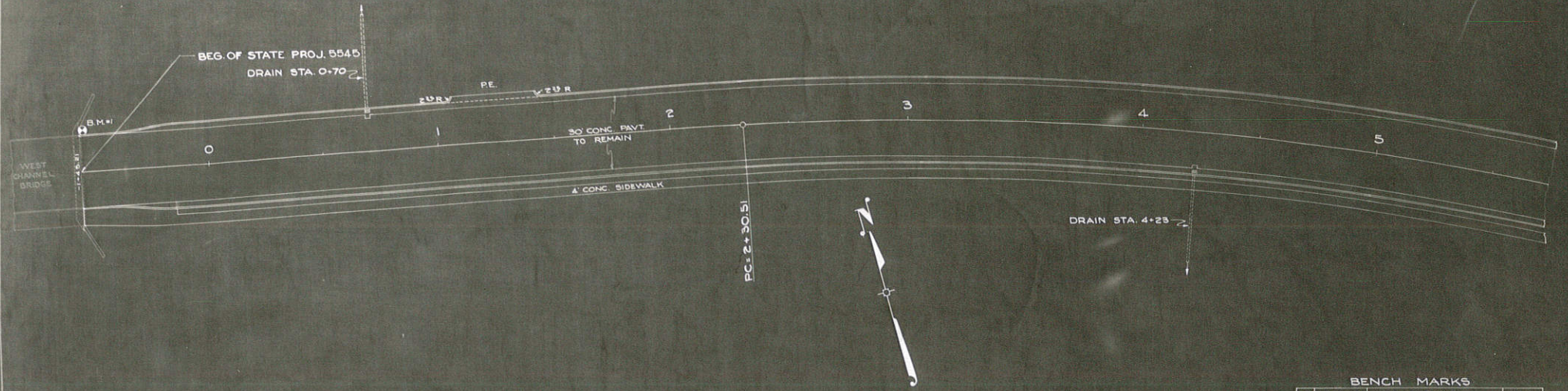
DIVISION JOB NO. 5542



TYPICAL CROSS SECTION
SCALE 1"=10'

SCALE

PLAN
 DATE: 10-24-50
 DRAWN BY: W. J. J. JR.
 CHECKED BY: W. J. J. JR.
 DISK



BENCH MARKS

NO.	STATION	DESCRIPTION	EL.
1	1+45	TOP NO. PARAPET WALL	648.98

SCALE
 VERT 1" = 10'
 HOR 1" = 40'

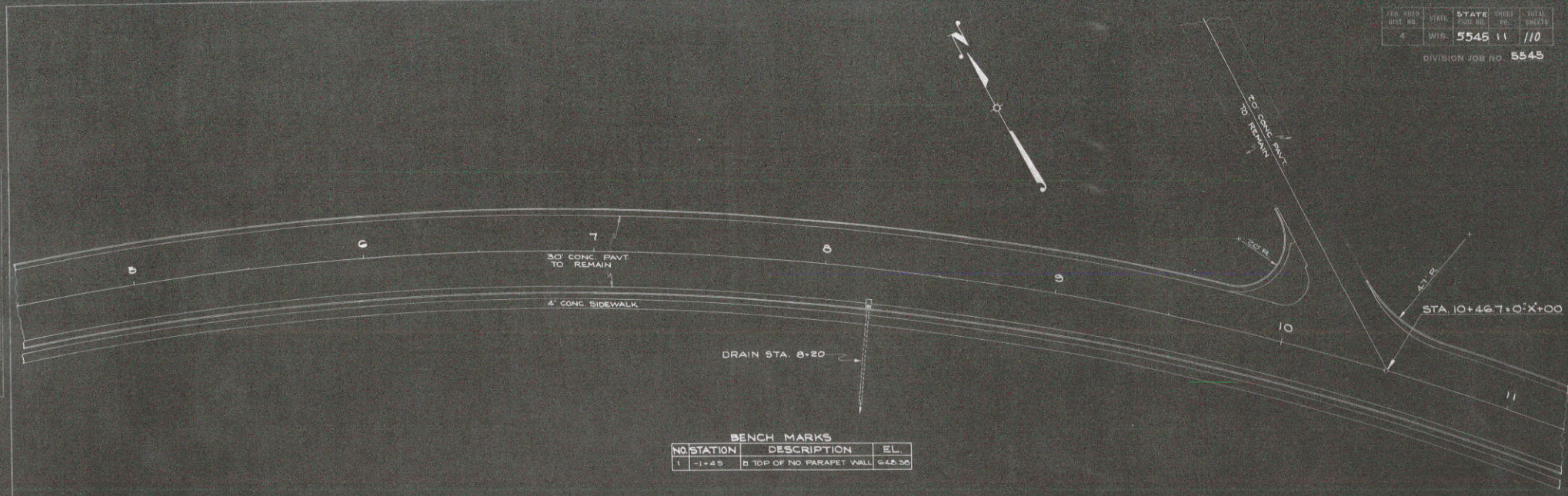
STATION	RIGHT SIDEWALK (BACK EDGE)	RIGHT CURB	RIGHT GUTTER	RIGHT APRON IN PLACE	CENTERLINE IN PLACE	LEFT APRON IN PLACE	LEFT GUTTER	LEFT CURB
0+00	45.56	45.47	45.36	45.14	45.14	45.14	45.14	45.20
0+25	45.60	45.37	45.11	45.10	45.10	45.10	45.10	45.15
0+50	45.54	45.30	45.07	45.04	45.04	45.04	45.04	45.10
0+75	45.45	45.24	45.05	45.03	45.03	45.03	45.03	45.08
1+00	45.45	45.18	45.03	45.02	45.02	45.02	45.02	45.08
1+25	45.34	45.09	45.04	45.03	45.03	45.03	45.03	45.08
1+50	45.24	45.00	45.00	45.00	45.00	45.00	45.00	45.05
1+75	45.14	44.91	44.91	44.91	44.91	44.91	44.91	44.96
2+00	45.04	44.82	44.82	44.82	44.82	44.82	44.82	44.87
2+25	44.94	44.73	44.73	44.73	44.73	44.73	44.73	44.78
2+50	44.84	44.64	44.64	44.64	44.64	44.64	44.64	44.69
2+75	44.74	44.55	44.55	44.55	44.55	44.55	44.55	44.60
3+00	44.64	44.46	44.46	44.46	44.46	44.46	44.46	44.51
3+25	44.54	44.37	44.37	44.37	44.37	44.37	44.37	44.42
3+50	44.44	44.28	44.28	44.28	44.28	44.28	44.28	44.33
3+75	44.34	44.19	44.19	44.19	44.19	44.19	44.19	44.24
4+00	44.24	44.10	44.10	44.10	44.10	44.10	44.10	44.15
4+25	44.14	44.01	44.01	44.01	44.01	44.01	44.01	44.06
4+50	44.04	43.92	43.92	43.92	43.92	43.92	43.92	43.97
4+75	43.94	43.83	43.83	43.83	43.83	43.83	43.83	43.88
5+00	43.84	43.74	43.74	43.74	43.74	43.74	43.74	43.79

PROFILE
 DATE: 10-24-50
 DRAWN BY: W. J. J. JR.
 CHECKED BY: W. J. J. JR.
 DISK

RLR
 W. J. J. JR.
 10-24-50

PLAN
 WITH 800' DISTANCE
 1" = 80.0'

PLAN
 WITH 800' DISTANCE
 1" = 80.0'



BENCH MARKS		
NO.	STATION	DESCRIPTION
1	1+4.5	TOP OF NO. PARAPET WALL
2	6+4.0	30'

SCALE
 VERT 1" = 2'
 HOR 1" = 20'

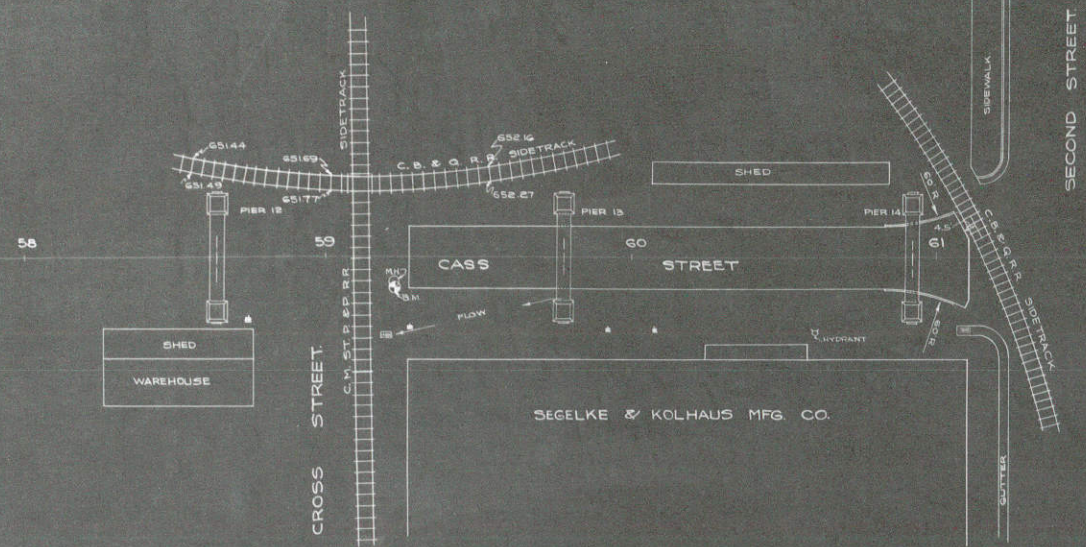
STATION	RT. SIDEWALK BACK EDGE	RT. CURB	RT. GUTTER	RT. APRON IN PLACE	CENTERLINE IN PLACE	LT. APRON IN PLACE	LT. GUTTER	LT. CURB
5	4727	4702	4630	4640	4710	4765	4776	4805
+25	4724	4699	4625	4634	4702	4760	4773	4804
+50	4720	4695	4620	4628	4704	4754	4767	4797
+75	4717	4692	4617	4625	4706	4751	4764	4794
6	4710	4685	4610	4618	4708	4746	4759	4789
+25	4705	4680	4605	4613	4710	4741	4754	4784
+50	4700	4675	4600	4608	4712	4739	4752	4782
+75	4695	4670	4595	4603	4714	4736	4749	4779
7	4688	4663	4588	4596	4716	4733	4746	4776
+25	4683	4658	4583	4591	4718	4730	4743	4773
+50	4678	4653	4578	4586	4720	4731	4744	4774
+75	4673	4648	4573	4581	4722	4733	4746	4776
8	4666	4641	4566	4574	4724	4735	4748	4778
+25	4661	4636	4561	4569	4726	4737	4750	4780
+50	4656	4631	4556	4564	4728	4739	4752	4782
+75	4651	4626	4551	4559	4730	4741	4754	4784
9	4644	4619	4544	4552	4732	4743	4756	4786
+25	4639	4614	4539	4547	4734	4745	4758	4788
+50	4634	4609	4534	4542	4736	4747	4760	4790
+75	4629	4604	4529	4537	4738	4749	4762	4792
10	4622	4597	4522	4530	4740	4751	4764	4794
+25	4617	4592	4517	4525	4742	4753	4766	4796
+50	4612	4587	4512	4520	4744	4755	4768	4798
+75	4607	4582	4507	4515	4746	4757	4770	4800
11	4600	4575	4500	4508	4748	4759	4772	4802
+25	4595	4570	4495	4503	4750	4761	4774	4804
+50	4590	4565	4490	4498	4752	4763	4776	4806
+75	4585	4560	4485	4493	4754	4765	4778	4808

PETTIBONE PARK ENTRANCE
 (SEE SPEC PROFILE SHEET)

PROJECT NO.	STATE	FED. PROJ. NO.	SHEET NO.	TOTAL SHEETS
110	IA	397 G	18	110
DIVISION JOB NO. 5545				

DATE	BY	CHECKED
3-27	J.A.P.	J.A.P.
3-27	J.A.P.	J.A.P.

DATE	BY	CHECKED
3-27	J.A.P.	J.A.P.
3-27	J.A.P.	J.A.P.



DETAILS 20' PAVEMENT 2ND & CASS STREETS WEST.

BENCH MARKS			
NO	STATION	DESCRIPTION	EL.
1	59+22.3	TOP OF MAN-HOLE 8 FT	651.72

2A
Z STA. 59+50

SCALE VERT. 1" = 2'
HORIZ. 1" = 20'

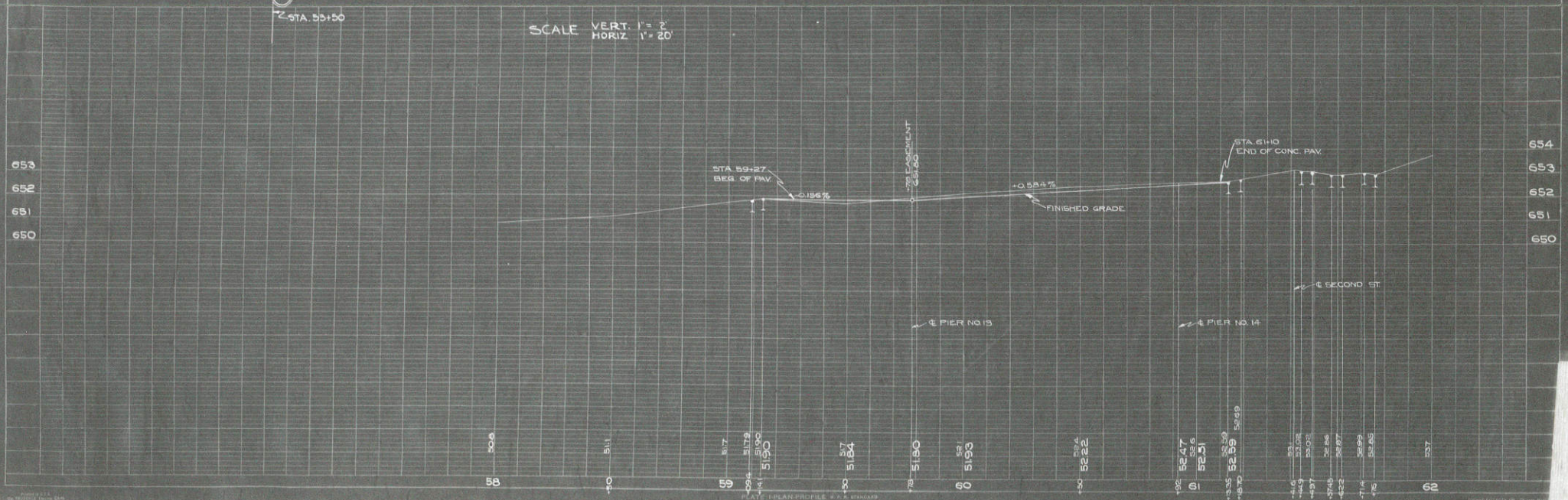
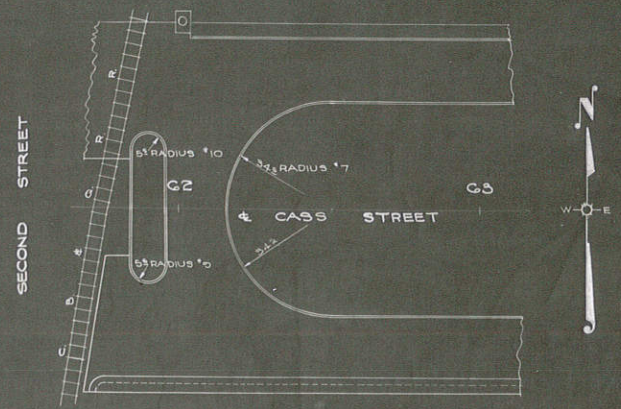


PLATE 1 PLAN PROFILE - P.A. STANLEY

REVISED - DIV. 5 11-12-35

PLAN
 DATE: 10/20/55
 BY: L.C. R. G. G.
 CHECKED: J.S.S.

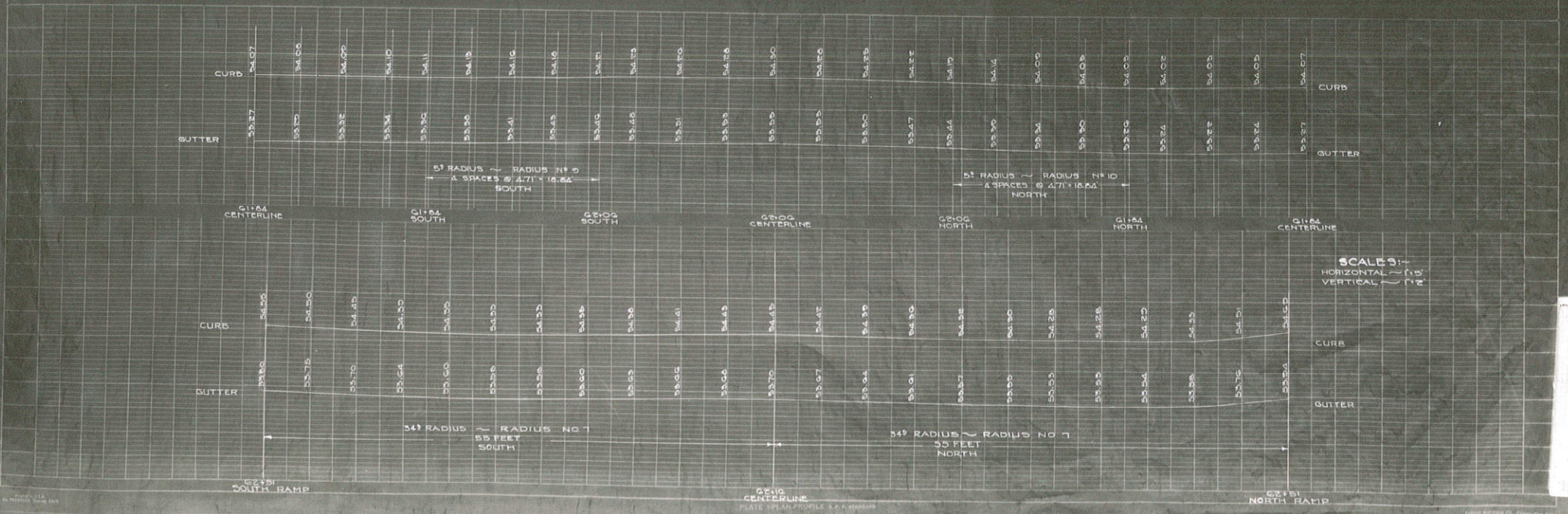
PROFILE
 DATE: 10/20/55
 BY: L.C. R. G. G.
 CHECKED: J.S.S.



DETAILS
 JUNCTION EAST OF SECOND ST.
 SCALE 1" = 20'

BENCH MARKS

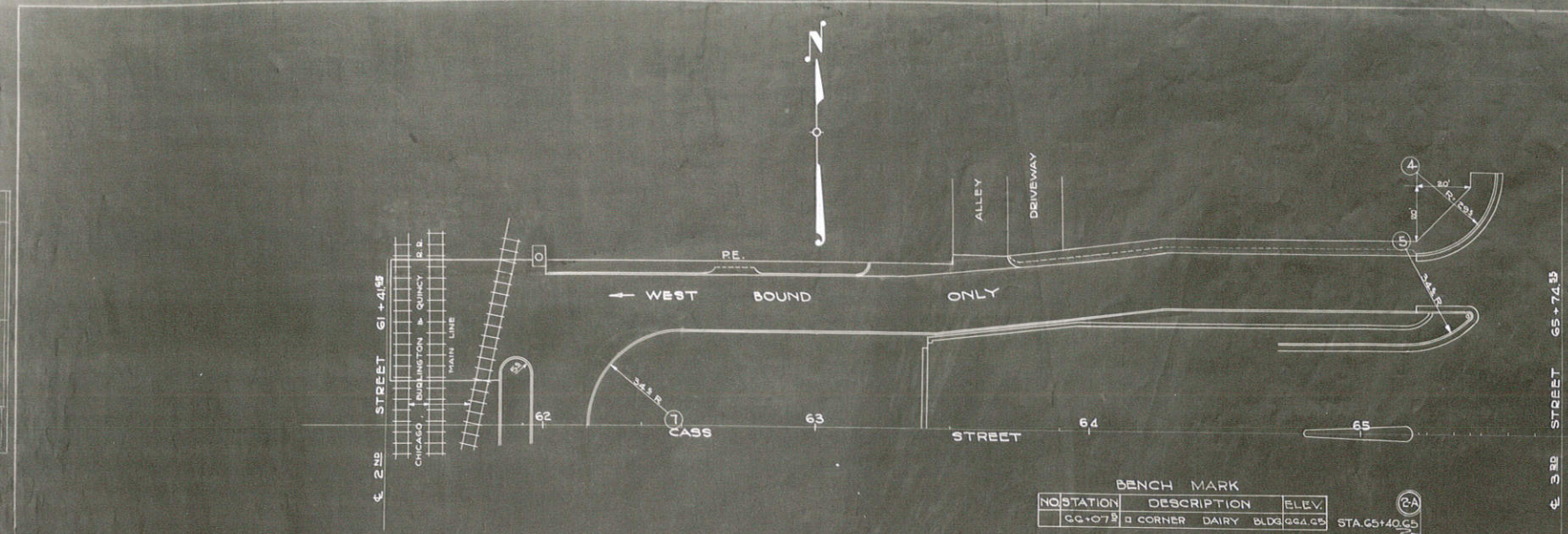
NO.	STATION	DESCRIPTION	ELEV.
1	66+07.5	W CORNER DAIRY BLDG	664.65



SCALES:—
 HORIZONTAL ~ 1" = 5'
 VERTICAL ~ 1" = 2'

PLAN
 DRAWN BY: LCA-CCG
 CHECKED BY: LCA-CCG
 DATE: 11/12/35

PROFILE
 DRAWN BY: BURECK
 CHECKED BY: BURECK
 DATE: 11/12/35



BENCH MARK

NO.	STATION	DESCRIPTION	ELEV.
66	61.07	CORNER DAIRY BLDG	66.49
67	65.40	65.40	65.40

SCALES
 HORIZONTAL 1" = 20'
 VERTICAL 1" = 2'

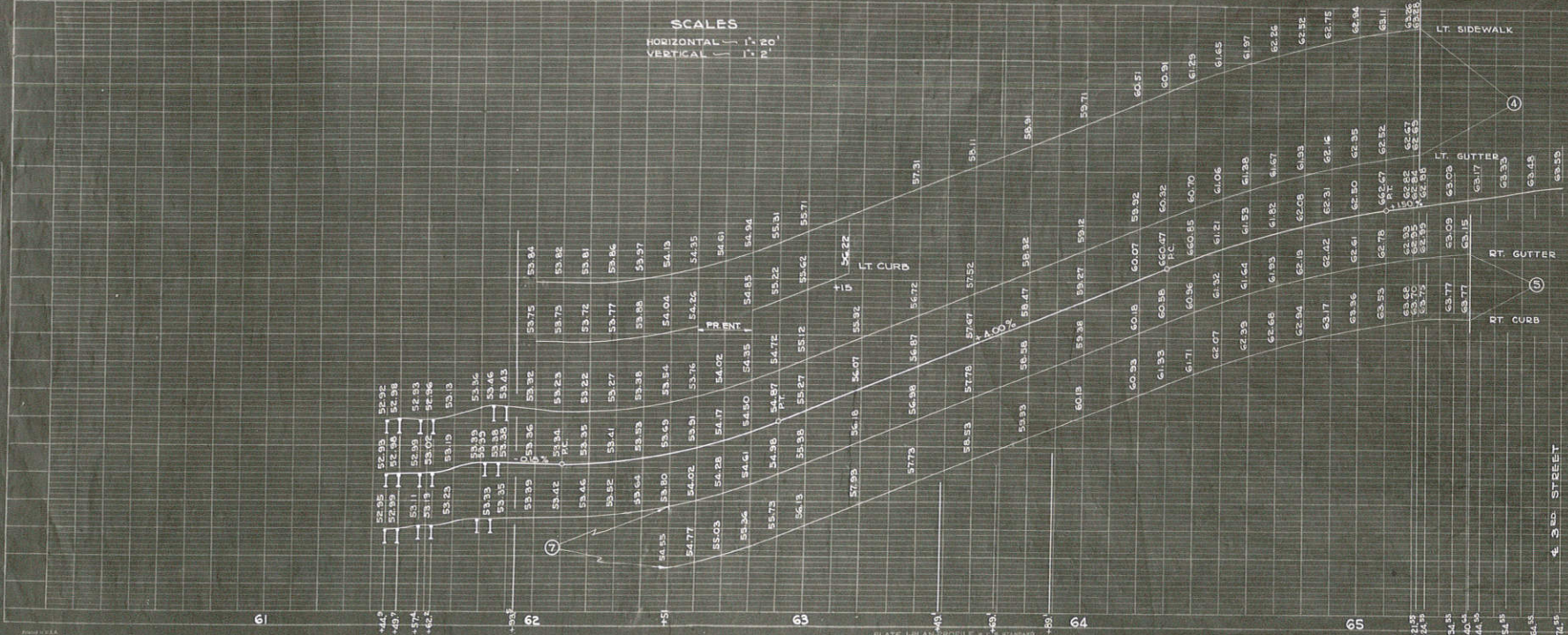


PLATE I-PLAN PROFILE & ELEVATIONS

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	307-G	22	110

DIVISION JOB NO. 5545

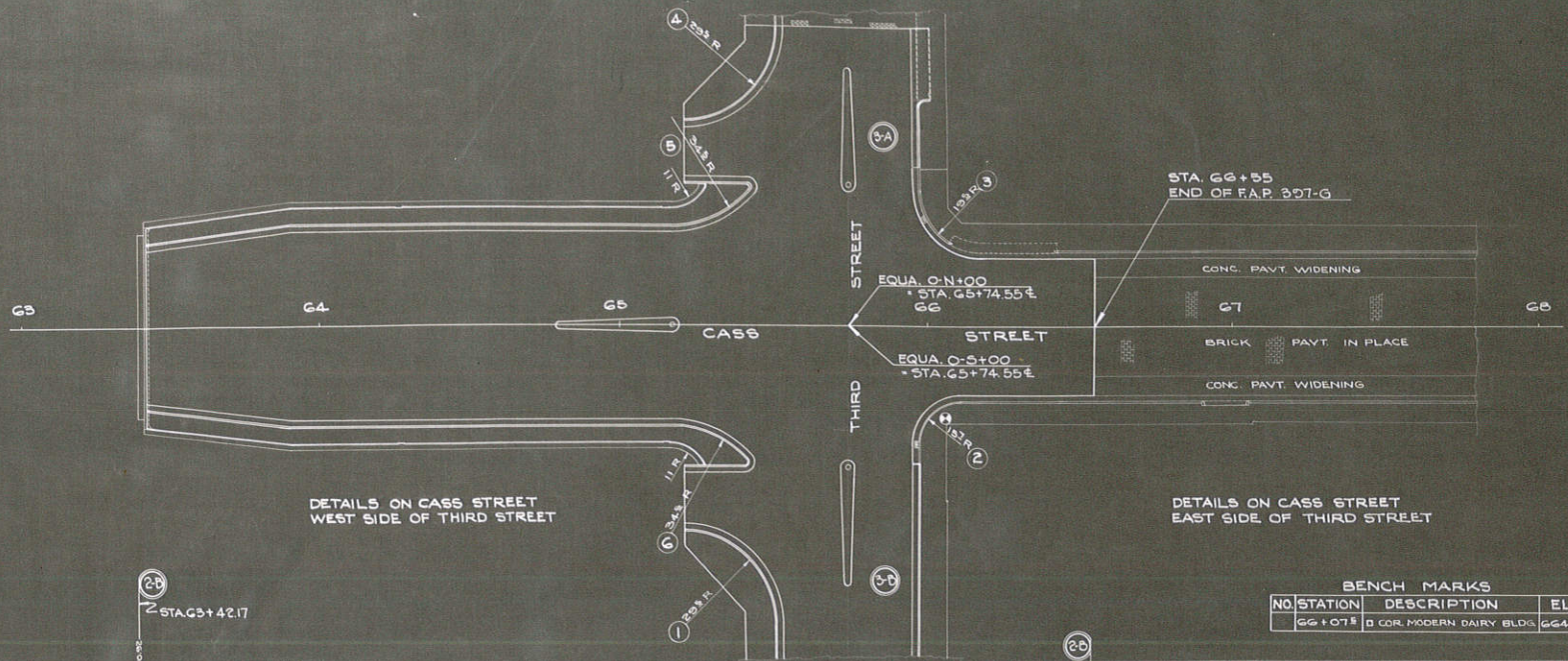


PLAN

DATE	BY

PROFILE

DATE	BY

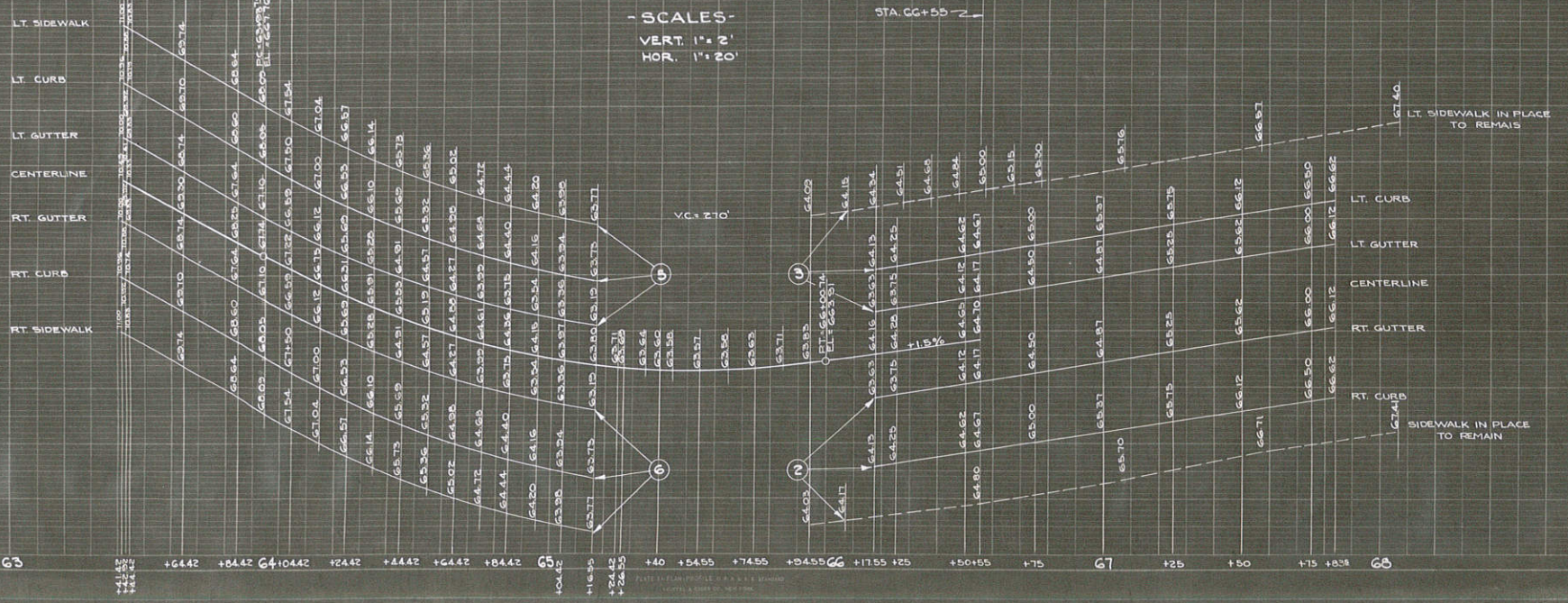


DETAILS ON CASS STREET WEST SIDE OF THIRD STREET

DETAILS ON CASS STREET EAST SIDE OF THIRD STREET

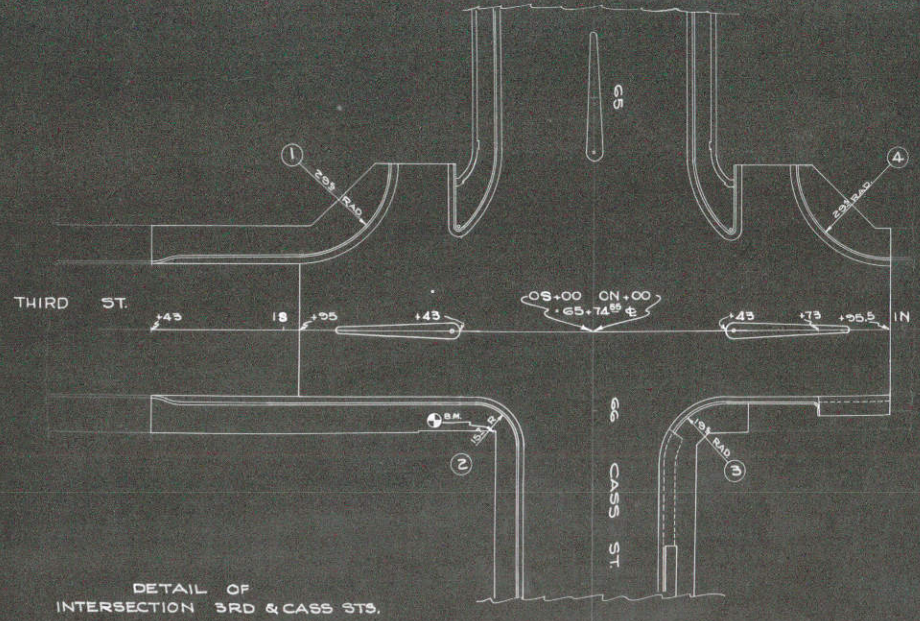
BENCH MARKS

NO.	STATION	DESCRIPTION	EL.
1	66+07±	COR. MODERN DAIRY BLDG.	664.65



- SCALES -
VERT. 1" = 2'
HOR. 1" = 20'

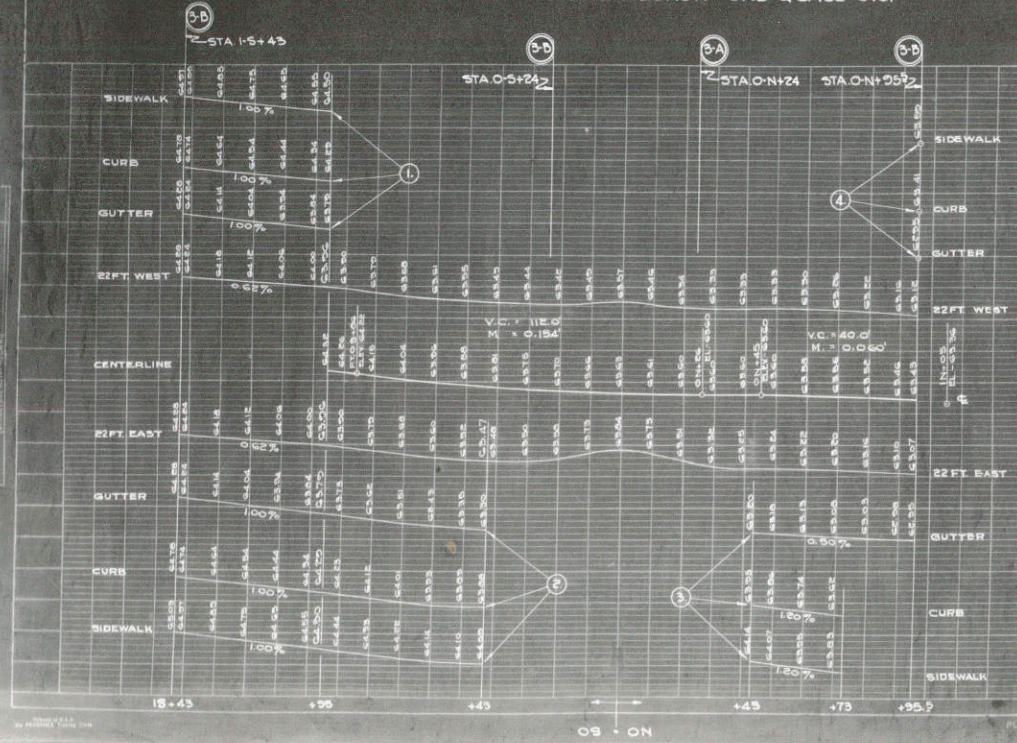
63 64 65 66 67 68



DETAIL OF INTERSECTION 3RD & CASS STS.

BENCH MARKS

NO. STATION	DESCRIPTION	EL.
66+07.5	D CORNER DAIRY BLDG 664.65	



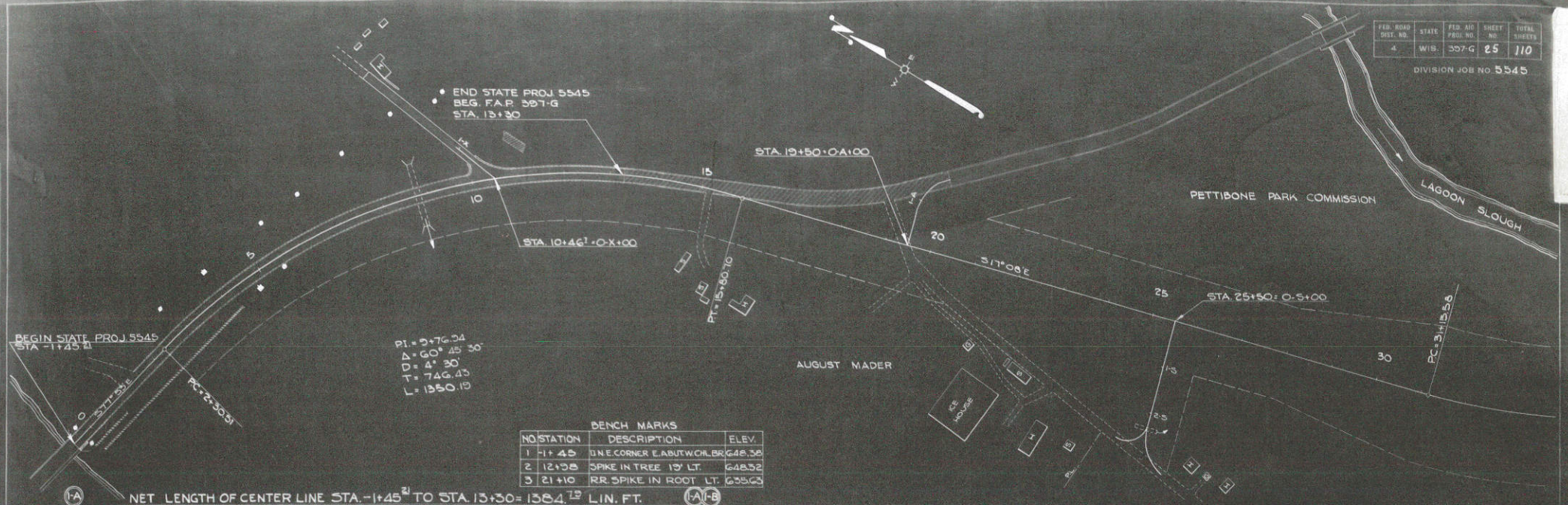
SCALES
 HORIZONTAL — 1" = 10'
 VERTICAL — 1" = 2'

PLAN
 DIVISION
 DISTRICT
 PROJECT NO.
 SHEET NO.

PROFILE
 DIVISION
 DISTRICT
 PROJECT NO.
 SHEET NO.

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	307-G	25	110

DIVISION JOB NO 5545



PI = 2+76.24
 I = 60° 45' 30"
 L = 474.43
 L = 1350.19

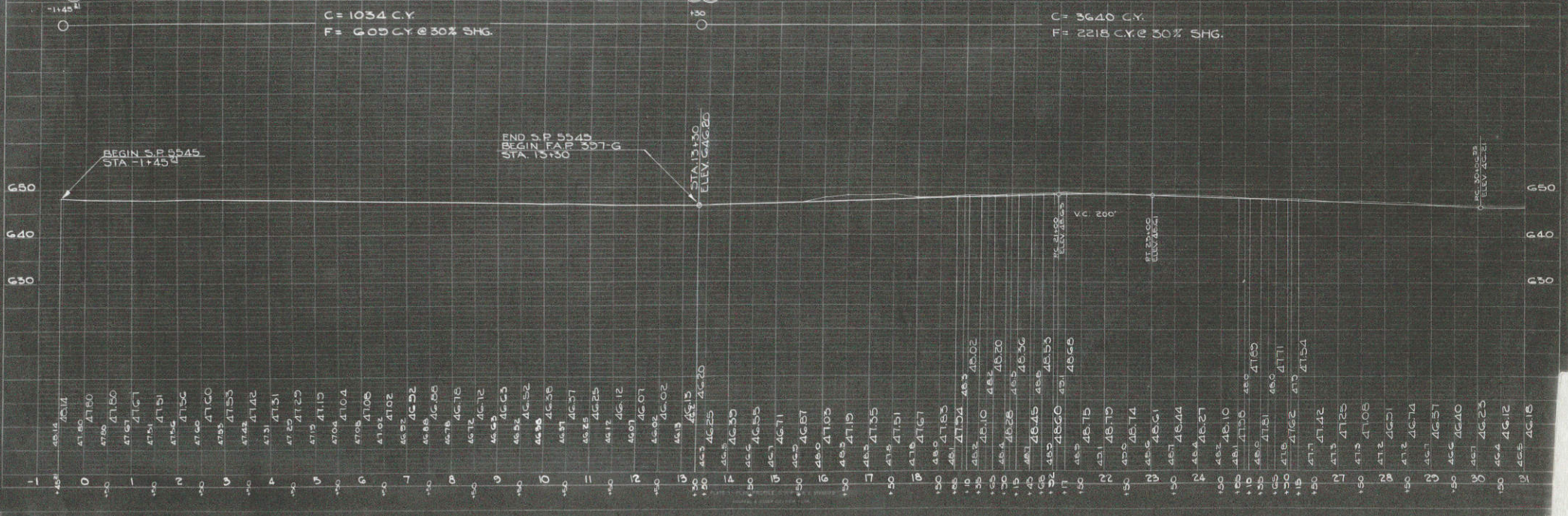
BENCH MARKS

NO	STATION	DESCRIPTION	ELEV.
1	-1+45	D.N.E. CORNER E. ABUT. WCHL BR.	648.38
2	12+38	SPIKE IN TREE 13' LT.	648.52
3	21+10	RR SPIKE IN ROOT LT.	655.63

NET LENGTH OF CENTER LINE STA. -1+45 TO STA. 13+30 = 1384.19 LIN. FT.

C = 1034 C.Y.
 F = 605 C.Y. @ 30% SHG.

C = 3640 C.Y.
 F = 2218 C.Y. @ 30% SHG.



PLAN

DATE	BY	CHECKED

PROFILE

DATE	BY	CHECKED

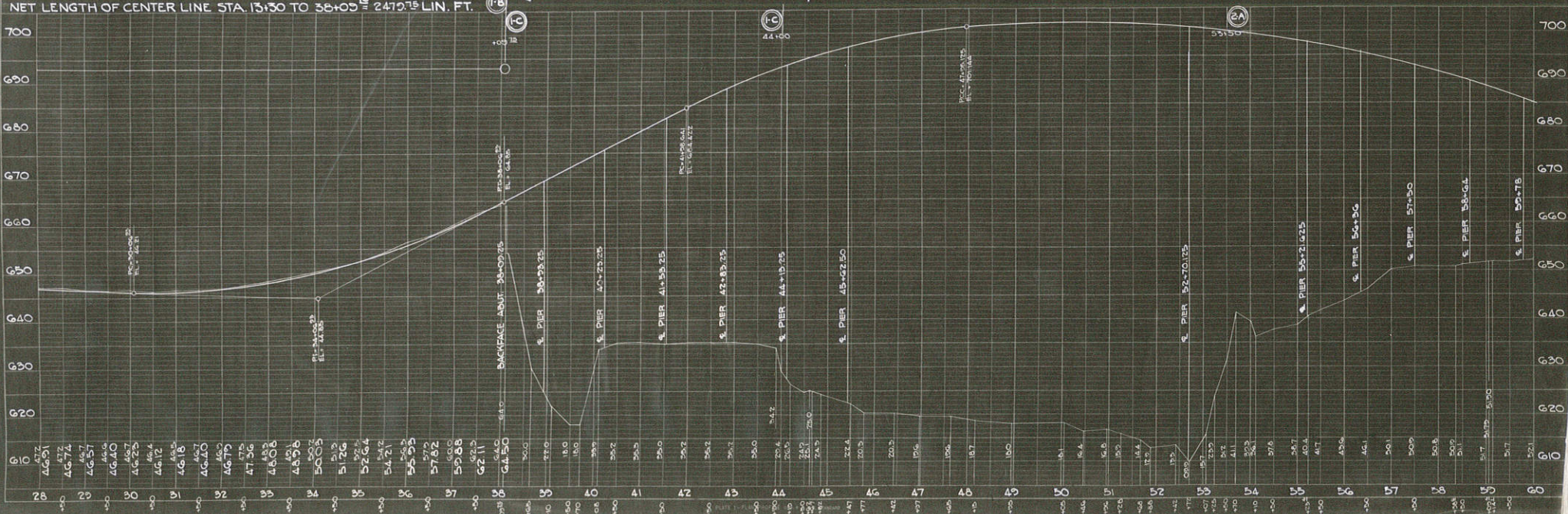
PLAN	DATE	BY	CHKD.
DATE	BY	CHKD.	BY
DATE	BY	CHKD.	BY
DATE	BY	CHKD.	BY

PROFILE	DATE	BY	CHKD.
DATE	BY	CHKD.	BY
DATE	BY	CHKD.	BY
DATE	BY	CHKD.	BY



FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	307-G	26	110

BENCH MARKS		
NO.	STATION	DESCRIPTION
4	38+0.5	+ ON TOP NO. WING WALL

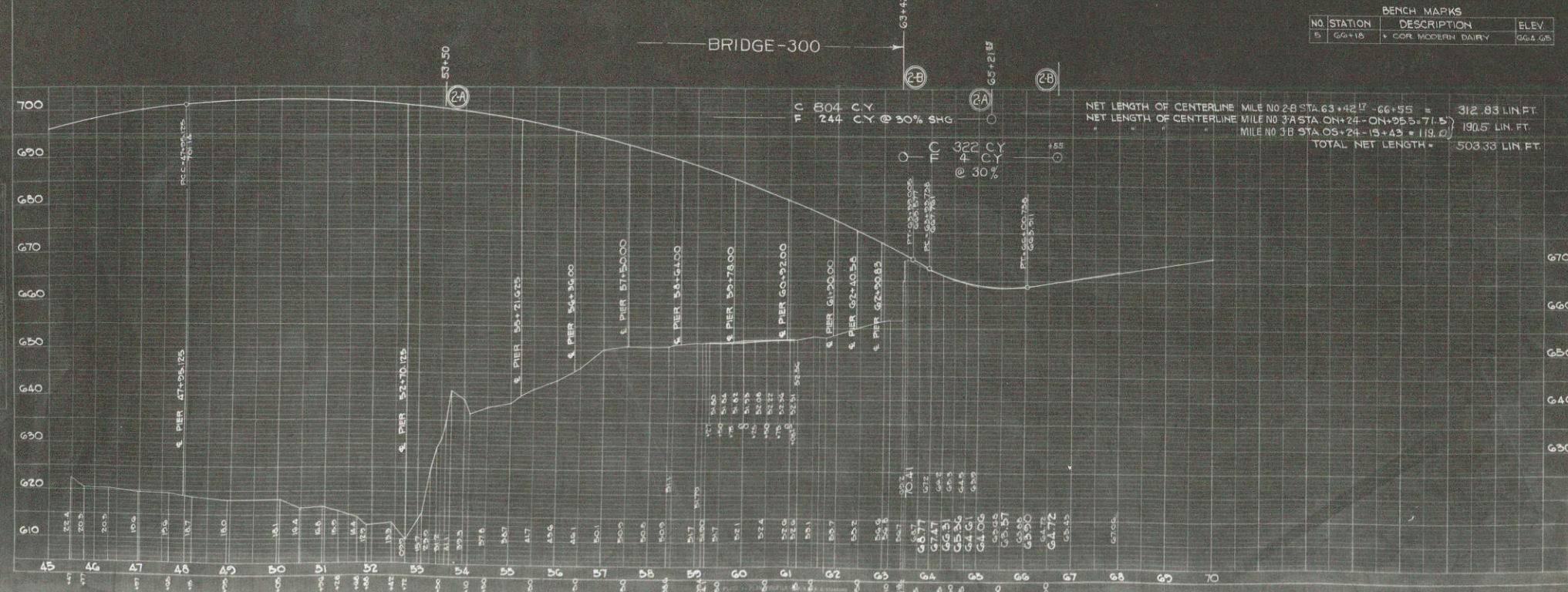
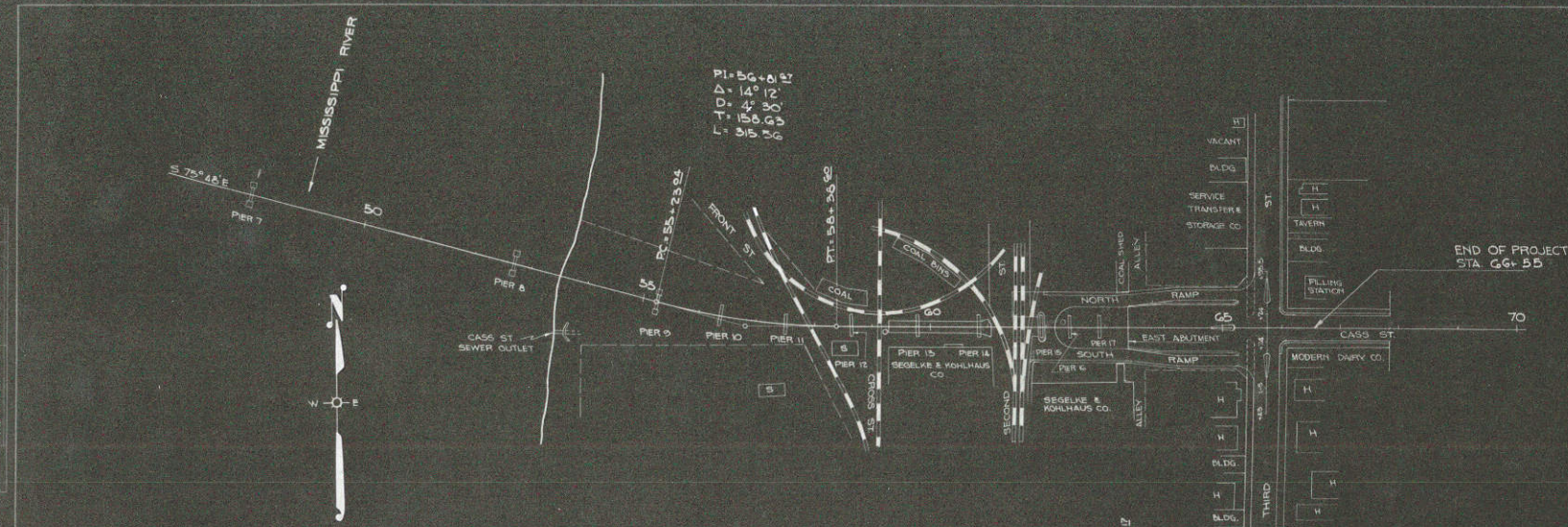


FED. ROAD DIST. NO.	STATE	PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	397-C	27	110

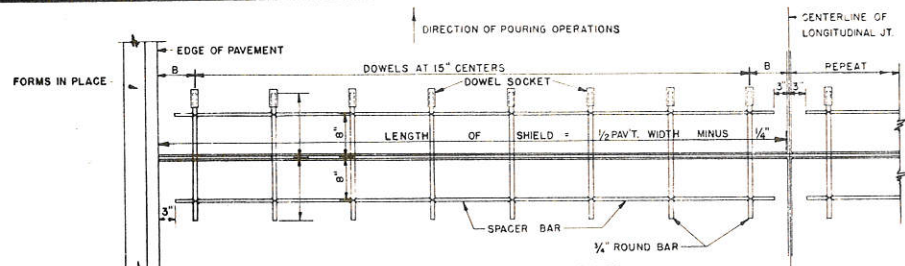
DIVISION JOB NO. 5545

PLAN	DATE	BY	CHECKED
BRIDGE-300			

PROFILE	DATE	BY	CHECKED
BRIDGE-300			

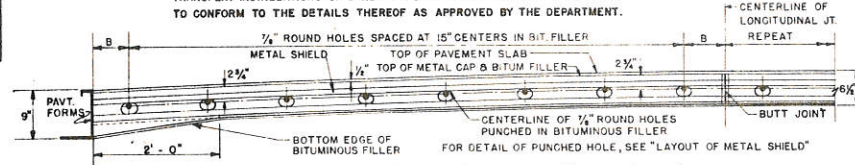


BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
5	66+16	+ CORN. MODERN DAIRY	644.06

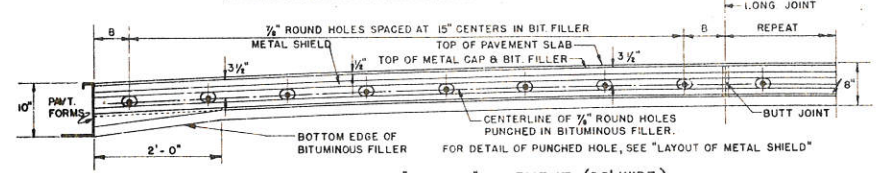


WIDTH OF PAVEMENT	VALUES OF B
18'	9"
20'	7 1/2"
22'	6"

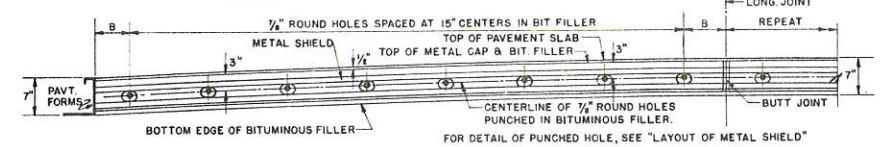
PLAN OF TRANSVERSE JOINT
 NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPARTMENT.



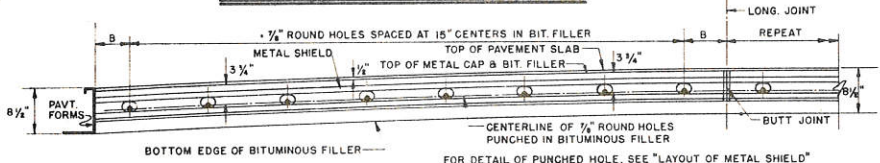
JOINT LAYOUT FOR "9-6 1/2-9" PAVEMENT (18 OR 20' WIDE)



JOINT LAYOUT FOR "10-8-10" PAVEMENT (22' WIDE)



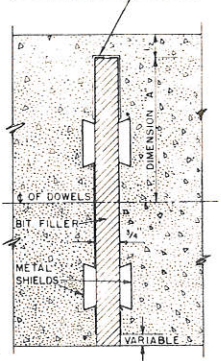
JOINT LAYOUT FOR UNIFORM 7" PAVEMENT



JOINT LAYOUT FOR UNIFORM 8 1/2" PAVEMENT

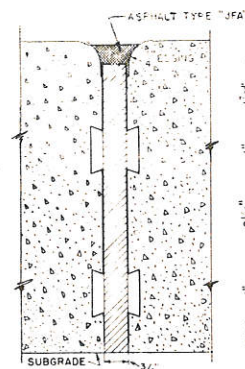
ELEVATIONS OF TRANSVERSE JOINTS

AUXILIARY CAP TO BE REMOVED DURING FINISHING OPERATIONS.

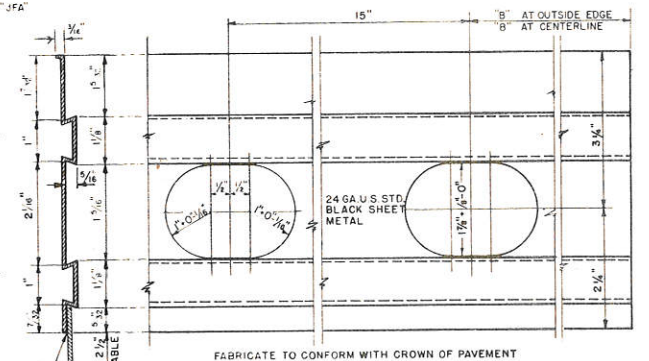


INSTALLATION PRACTICE

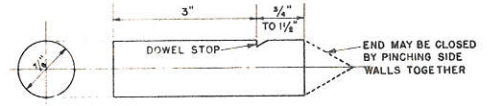
- DIMENSION X
- "9-6 1/2-9" PAVEMENT = 2 1/4"
- "10-8-10" PAVEMENT = 3 1/4"
- UNIF. 7" PAVEMENT = 3"
- UNIF. 8 1/2" PAVEMENT = 3 3/4"



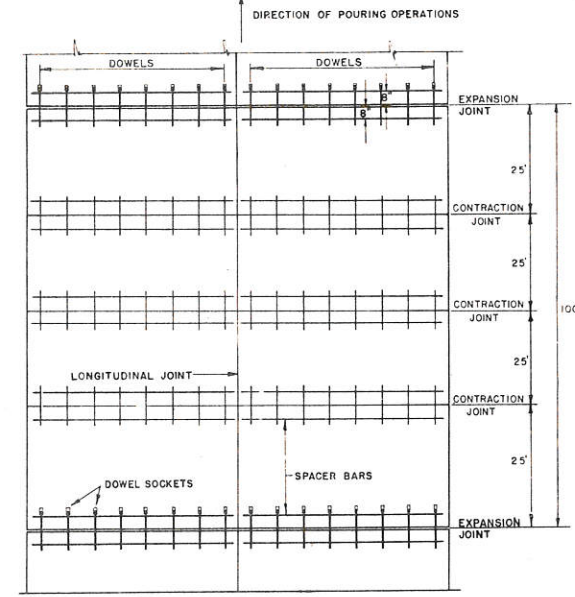
COMPLETED



LAYOUT OF METAL SHIELD



DOWEL SOCKET DETAIL



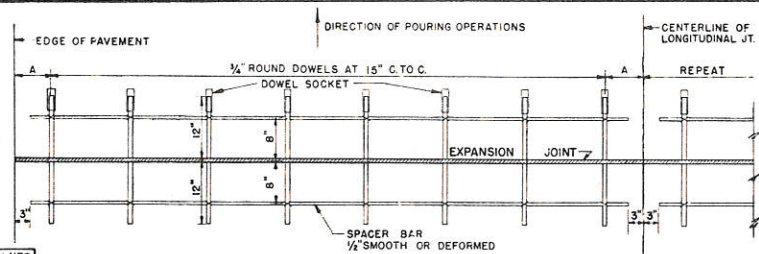
PLAN SHOWING JOINT LAYOUT

GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS. MARGINAL OR SPACER BARS SHALL BE ONE-HALF INCH ROUND AND DOUBLE CROSSWIRED TO THE DOWELS, I.E. WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION. USE NO 16 GA LOOP TIE WIRE. NO LONGITUDINAL BARS, REINFORCEMENT OR PARTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT. THE TERM "BITUMINOUS FILLER" USED HERE REFERS TO AN APPROVED PREMOLDED BITUMINOUS EXPANSION JT. FILLER, 3/4" THICK, SHAPED TO CONFORM TO PERTINENT CROSS SECTION OF PAVEMENT SLAB AND CUT 1/2" BELOW CROWN OF PAVEMENT. ALL DOWELS SHALL BE TREATED TO BREAK BOND. LENGTH OF SHIELDS SHALL BE EQUAL TO ONE-HALF THE WIDTH OF PAVEMENT MINUS ONE-QUARTER INCH. ENDS OF SHIELDS TO BE CLOSED BY GRIPPING IN A MANNER SATISFACTORY TO THE ENGINEER. METHOD OF FASTENING BUTT JOINTS TOGETHER SHALL MEET WITH THE APPROVAL OF THE ENGINEER. MODIFICATIONS OF DESIGN WILL BE ACCEPTABLE SUBJECT TO THE APPROVAL OF THE ENGINEER. METAL SHIELDS ARE NOT TO BE HELD TOGETHER BY MEANS OF NAILS OR OTHER DEVICES WHICH WOULD TEND TO HOLD SHIELDS TOGETHER PERMANENTLY AFTER CONCRETE HAS SET.

**STANDARD DESIGN
 TRANS. EXPANSION JOINT FOR CONC. PAVEMENT
 PREMOLDED BITUMINOUS TYPE**

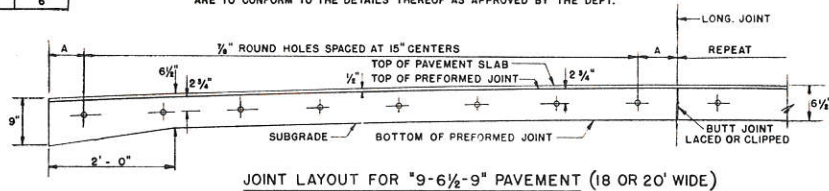
STATE HIGHWAY COMMISSION OF WISCONSIN
 RECOMMENDED FOR APPROVAL
Frank Kraus
 ENGINEER
 APPROVED
E.L. Rustin
 STATE HIGHWAY ENGINEER
 DATE 3/28/38



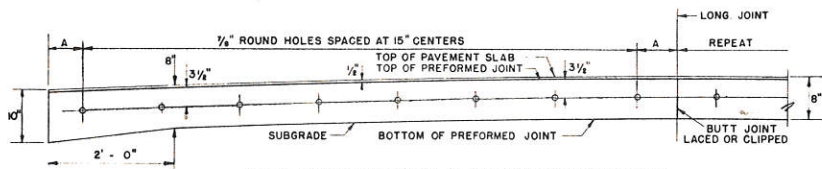
WIDTH OF PAVEMENT	VALUES OF "A"
18'	9"
20'	7 1/2"
22'	6"

PLAN OF TRANSVERSE JOINT

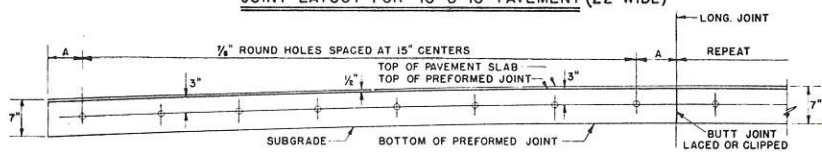
NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPT.



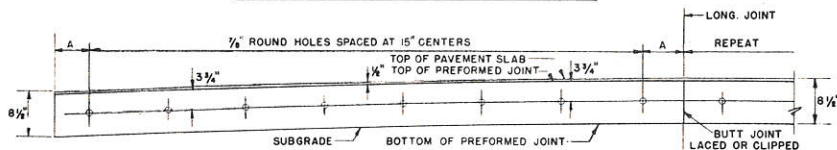
JOINT LAYOUT FOR "9-6 1/2-9" PAVEMENT (18 OR 20' WIDE)



JOINT LAYOUT FOR "10-8-10" PAVEMENT (22' WIDE)

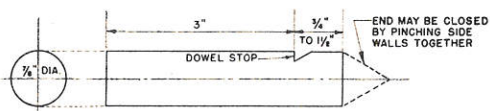


JOINT LAYOUT FOR UNIFORM 7" PAVEMENT

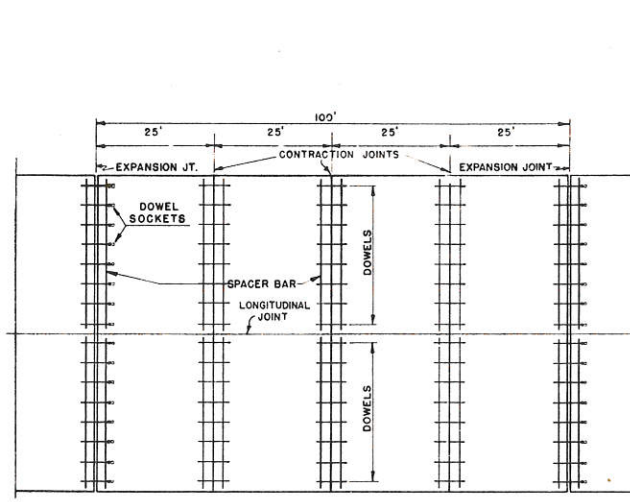


JOINT LAYOUT FOR UNIFORM 8 1/2" PAVEMENT

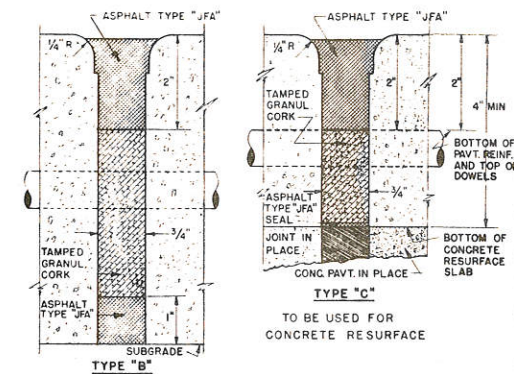
ELEVATIONS OF TRANSVERSE JOINTS



DOWEL SOCKET DETAIL



PLAN SHOWING JOINT LAYOUT FOR JOINT SPACING ON CONCRETE RESURFACE AND WIDENING SEE STD. SPECIFICATIONS



GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.

SPACER BARS SHALL BE 1/2" ROUND; SMOOTH OR DEFORMED, BARS WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION USE NO. 16 GAUGE LOOP TIE WIRE.

NO LONGITUDINAL BARS, REINFORCEMENT OR PARTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT.

ALL DOWELS SHALL BE TREATED TO BREAK BOND.

DOWELS FOR CONTRACTION JOINTS IN CONCRETE RESURFACING SHALL BE PLACED 2" BELOW TOP OF NEW SLAB AND DIRECTLY BENEATH PAVEMENT REINFORCEMENT.

STANDARD DESIGN FOR TRANSVERSE JOINTS RESILIENT TYPE CAST IN PLACE

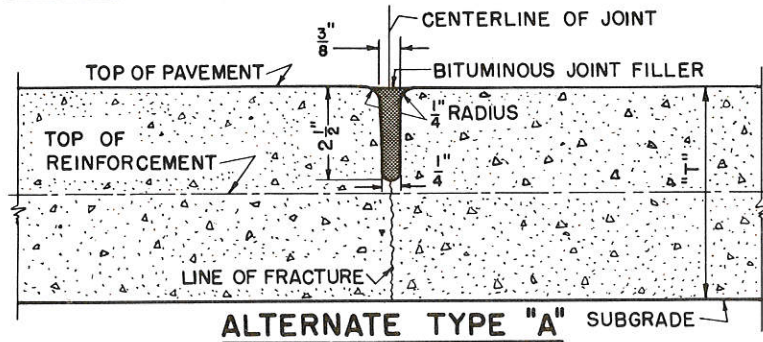
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:

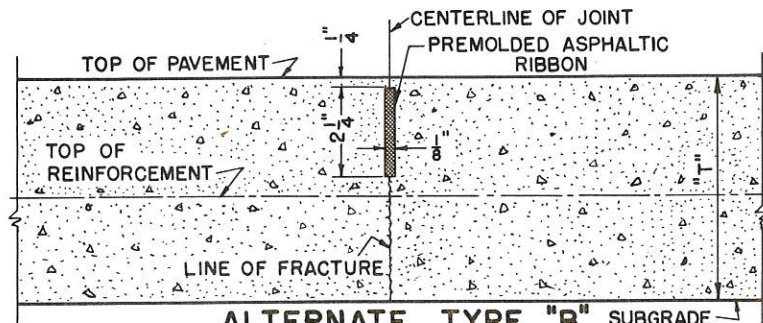
APPROVED: *[Signature]* 3/28/38

DRAWN: N.F.C. 3/28/38

CHKD: N.F.C. 3/28/38

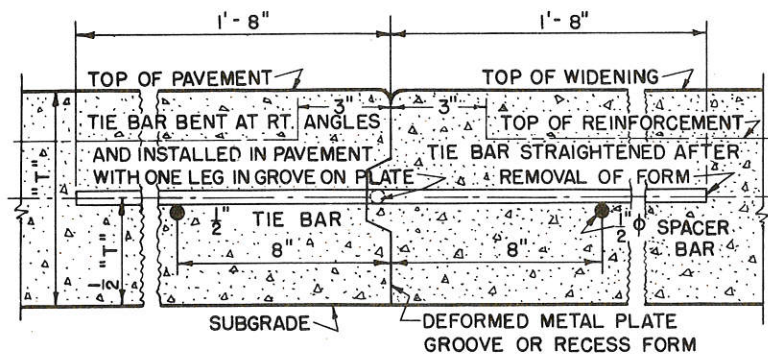


ALTERNATE TYPE "A" POURED JOINT



ALTERNATE TYPE "B" PREMOLDED JOINT

LONGITUDINAL WEAKENED PLANE JOINT



LONGITUDINAL CONSTRUCTION JOINT

SPACING OF TIE BARS IN LONGITUDINAL CONSTRUCTION JOINT:
 4'-0" CTR. TO CTR. THRU ABUTTING THICKENED EDGE PAVEMENT SLABS.
 3'-0" CTR. TO CTR. THRU ABUTTING UNTHICKENED EDGE PAVEMENT SLABS.

CONSTRUCTION NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DIMENSION "T" INDICATES PAVEMENT DEPTH ALONG LINE OF LONGITUDINAL JOINT.

JOINTS SHALL BE CLEAN CUT AT ALL INTERSECTIONS WITH OTHER JOINTS.

BITUMINOUS JOINT FILLER FOR SEALING JOINTS SHALL CONFORM TO THE REQUIREMENTS SPECIFIED FOR TRANSVERSE JOINTS.

FOLLOWING THE INSTALLATION OF THE JOINT OR TEMPORARY FILLER STRIPS FOR CREATING A WEAKENED PLANE, THE FINISHING MACHINE SHALL SCREED OVER THE SECTION AT LEAST ONE TIME.

WEAKENED PLANE TYPE JOINTS SHALL BE FORMED BY A MECHANICAL METHOD APPROVED BY THE ENGINEER

JOINTS SHALL NOT DEVIATE MORE THAN 5 DEGREES FROM THE VERTICAL NOR SHALL THE AXIS OF THE JOINT DEVIATE MORE THAN 1/2" EITHER WAY FROM A STRAIGHT LINE AT ANY POINT.

SPACER BARS ARE NOT REQUIRED AT LONGITUDINAL JOINTS ALONG THICKENED EDGES OF PAVEMENT SLAB.

LONGITUDINAL JOINT DETAILS FOR REINFORCED CONCRETE PAVEMENT

STATE HIGHWAY COMMISSION OF WISC.

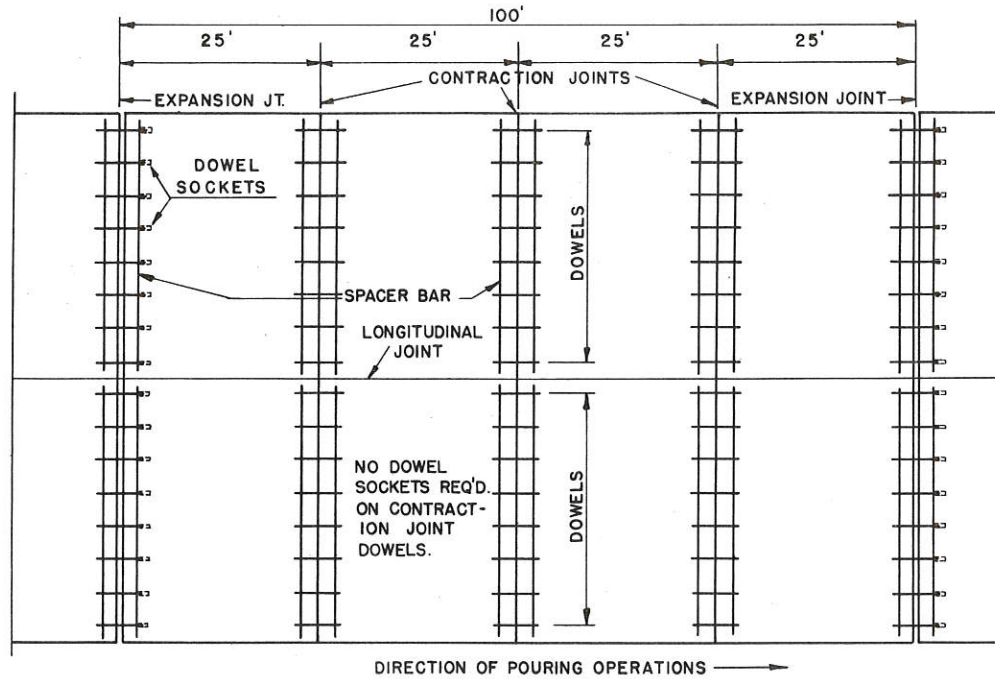
RECOMMENDED FOR APPROVAL:

Frank Crone
DESIGN ENGINEER

P. J. Bluk
CONSTRUCTION ENGINEER

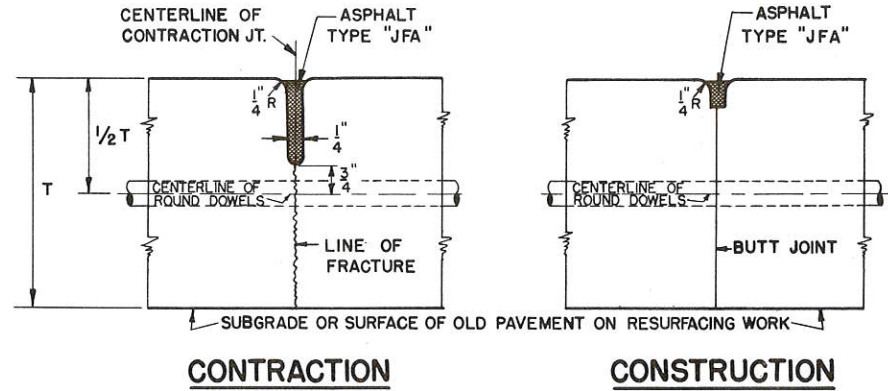
APPROVED

DRAWN W.P.W. 3/28/38 DATE STATE HIGHWAY ENGINEER
 CHECKED n.f.c.



PLAN SHOWING JOINT LAYOUT

FOR JOINT SPACING ON CONCRETE RESURFACE AND WIDENING SEE STD. SPECIFICATIONS



GENERAL NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.

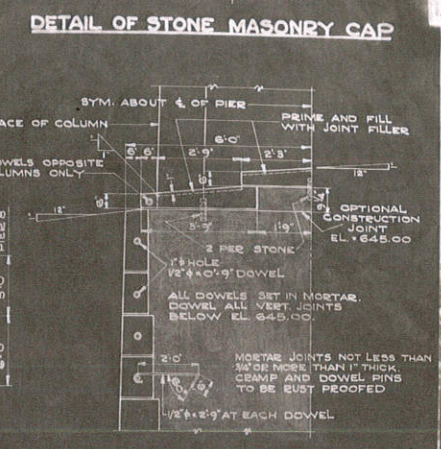
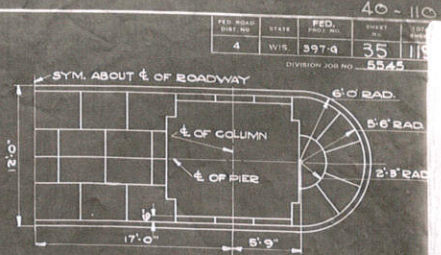
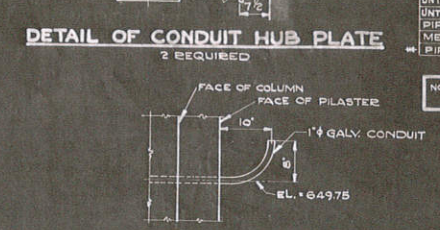
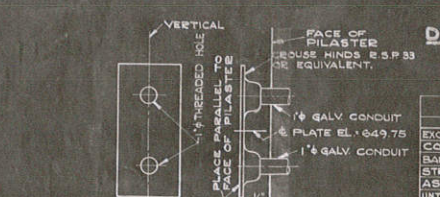
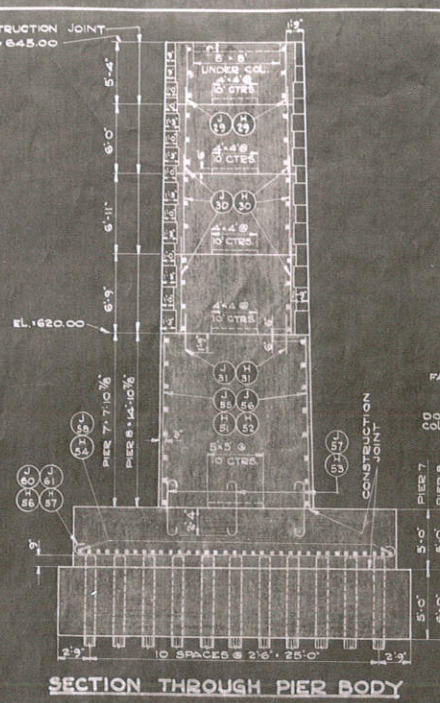
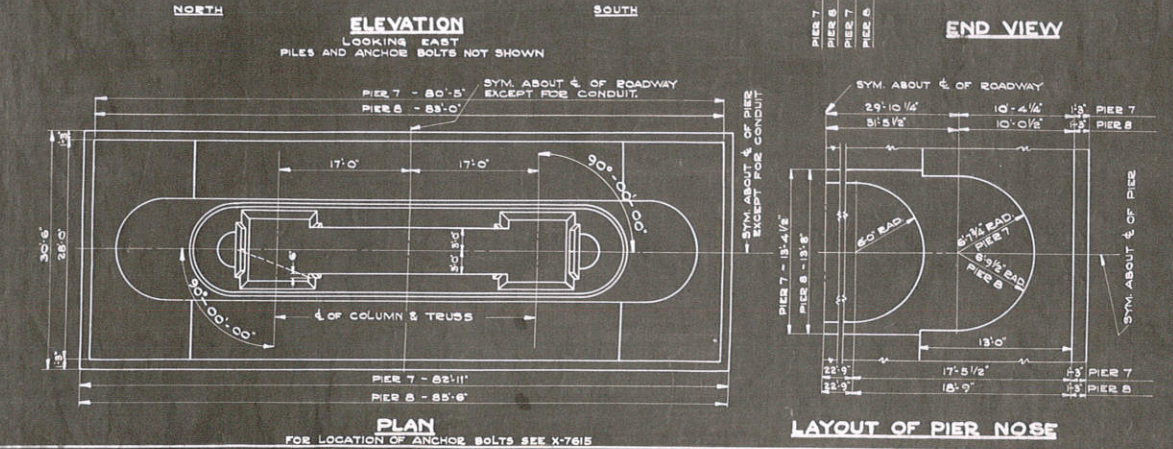
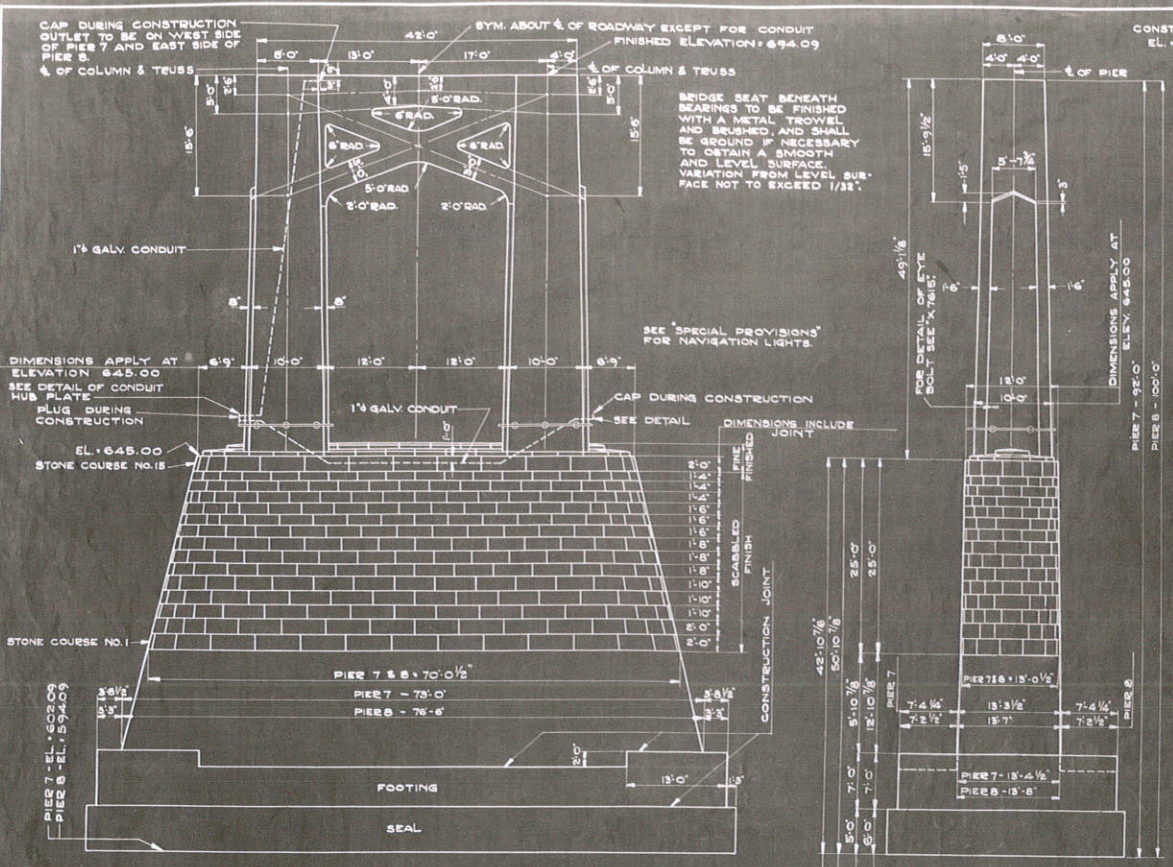
NO LONGITUDINAL BARS, REINFORCEMENT OR PARTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT.

ALL DOWELS SHALL BE TREATED TO BREAK BOND.

DOWELS FOR CONTRACTION JOINTS IN CONCRETE RESURFACING SHALL BE PLACED 2" BELOW TOP OF NEW SLAB AND DIRECTLY BENEATH PAVEMENT REINFORCEMENT.

TRANSVERSE JOINTS CONTRACTION - CONSTRUCTION	
STATE HIGHWAY COMMISSION OF WISC.	
RECOMMENDED FOR APPROVAL:	
<i>Frank Chase</i> DESIGN ENGINEER	4/7/38
<i>M. Bluh</i> CONSTRUCTION ENGINEER	
APPROVED:	
DRAWN W.W.-GL CHECKED N.F.C.	4/7/38 <i>E.G. Rostig</i> DATE STATE HIGHWAY ENGINEER

FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	PROJECT NO.
4	WIS.	397-A	35
DIVISION JOB NO.			5545



ESTIMATED QUANTITIES			
BID ITEMS	UNIT	PIER 7	PIER 8
EXCAVATION FOR STRUCTURES	C.Y.	1800	1750
CONCRETE MASONRY	C.Y.	2,804	2,238.4
BAR STEEL REINFORCEMENT	LB.	95,616	73,810
STRUCTURAL STEEL (CARBON)	LB.	350	75,660
ASHLAR MASONRY	C.Y.	145	145
UNTREATED TIMBER PILING DEL.	LF	13,640	14,080
UNTREATED TIMBER PILING DB.	LF	13,640	14,080
PIPE RAILING	LF	91	91
MEMBRANE WATERPROOFING	SF	650	655
PIPE CONDUIT	LF	102	102

NOTE: OMIT ALL ASHLAR MASONRY AND REPLACE WITH CONCRETE MASONRY. SEE X-7619 FOR SECTIONS THROUGH PIER BODY SHOWING REVISIONS FOR REINFORCING STEEL.

10 OF 80

STRUCTURE JOB NO. 5362

STATE HIGHWAY COMMISSION OF WISCONSIN

DETAILS OF PIERS 7 & 8

FOR

BRIDGE NO. 300

CITY OF LA CROSSE — LA CROSSE CO.

CORRECT: *G. N. Kist*

APPROVED: *G. N. Kist*

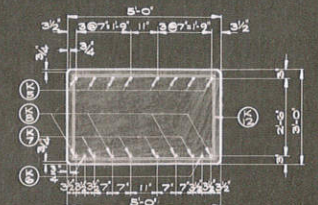
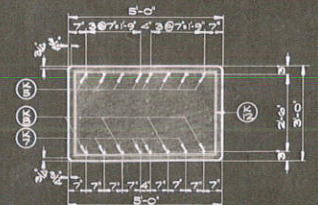
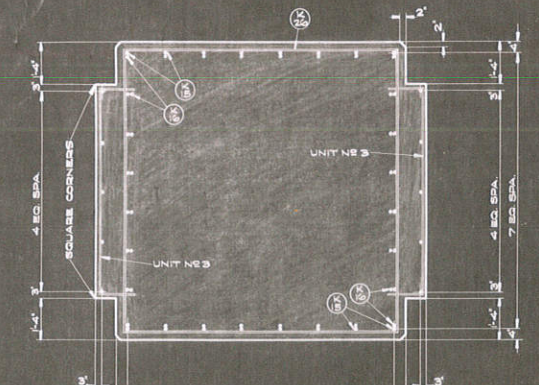
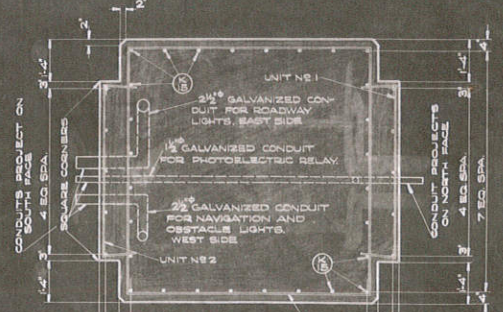
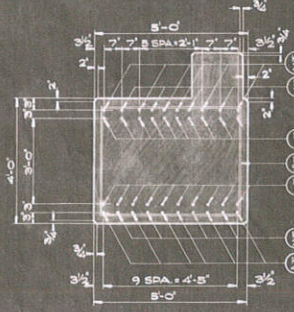
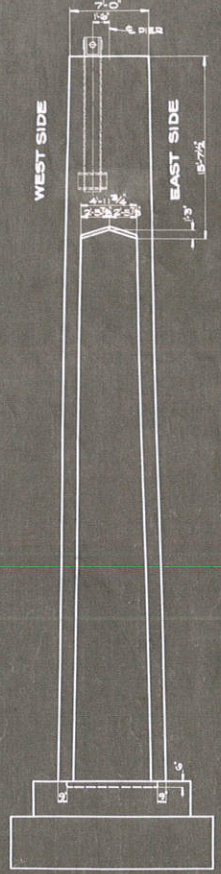
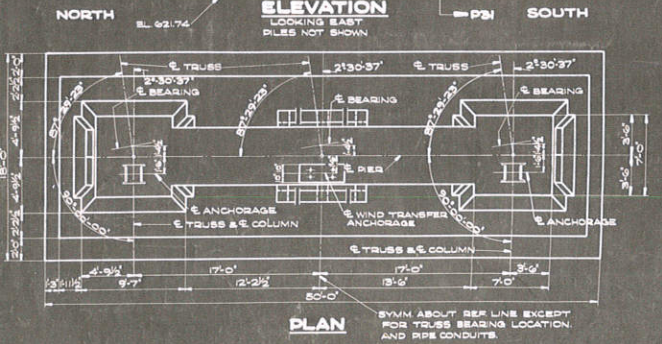
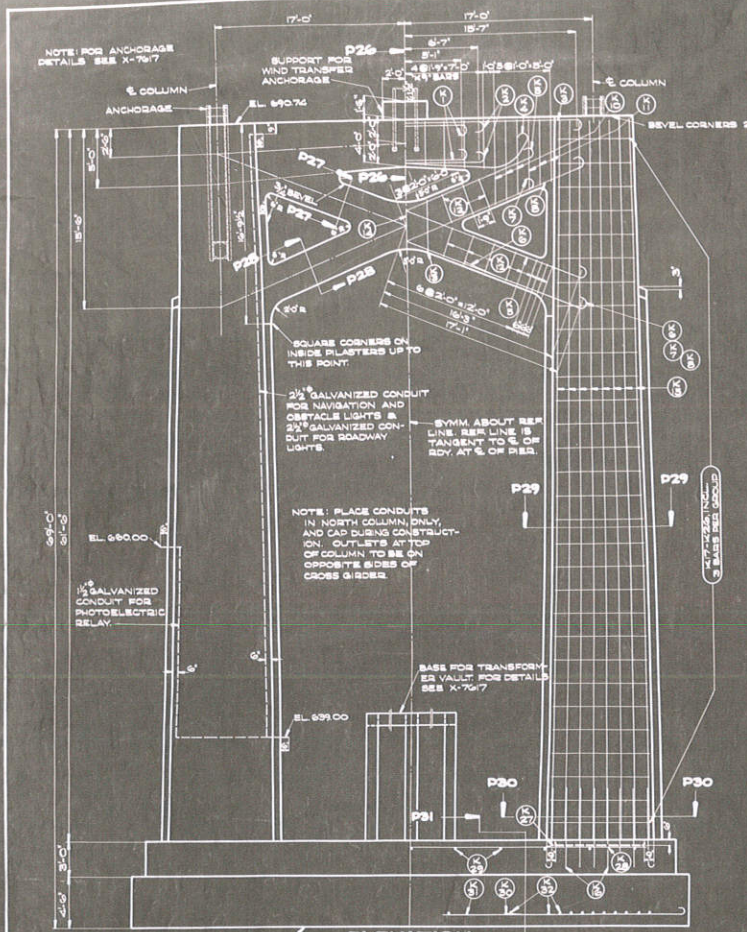
DATE: 11/21/36

DESIGNED BY: *G. N. Kist*

CHECKED BY: *G. N. Kist*

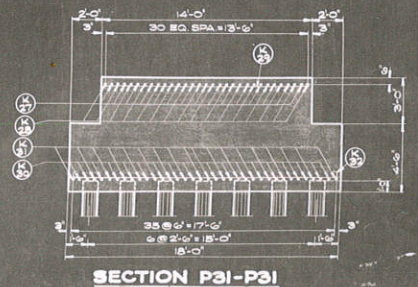
STATE ENGINEER

X7613



ESTIMATED QUANTITIES

BID ITEM	UNIT	QUANTITY
EXCAVATION FOR STRUCTURE	C.Y.	785
CONCRETE MASONRY	C.Y.	622.2
BAR STEEL REINFORCEMENT	LB.	45,940
STRUCTURAL STEEL [CARBON]	LB.	3,700
UNTREATED TIMBER PILING-DRIVEN	L.F.	4,410
MEMBRANE WATERPROOFING	S.F.	240
PIPE CONDUIT	L.F.	149



13 OF 80

REVISED 11-24-56

STATE HIGHWAY COMMISSION OF WISCONSIN

DETAILS OF PIER 9

FOR

BRIDGE NO. 300

CITY OF LA CROSSE — LA CROSSE CO.

STRUCTURE JOB NO. 5362

APPROVED: 11-24-56

DESIGNED BY: R. H. B. A.

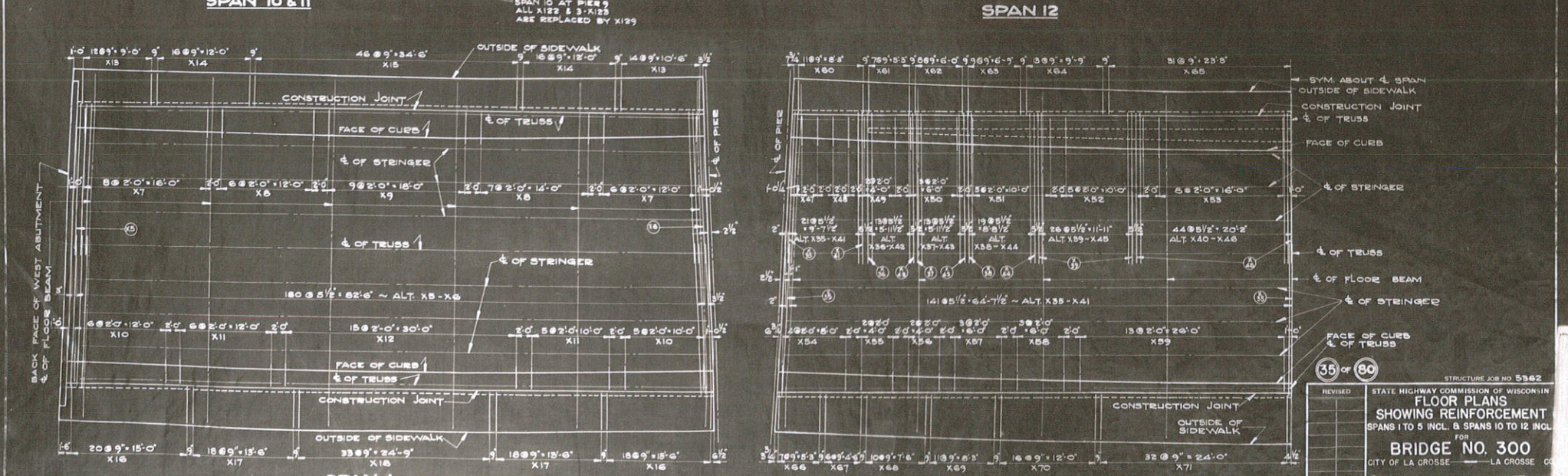
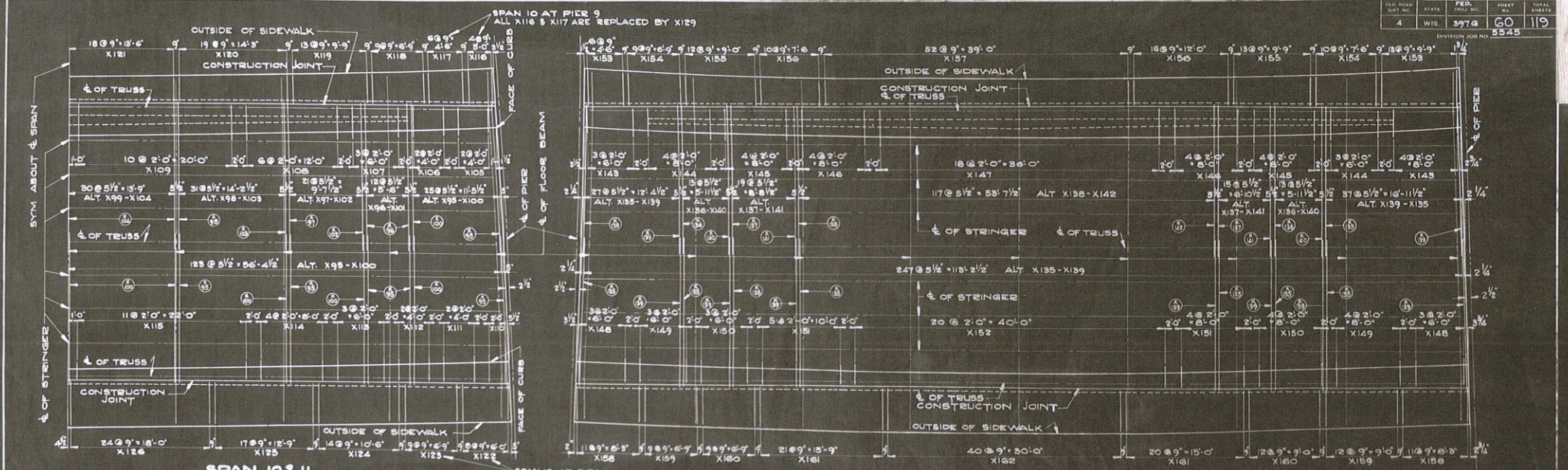
CHECKED BY: R. H. B. A.

DATE: 11-24-56

BY: R. H. B. A.

FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	397a	60	119

DIVISION JOB NO. 5345



STRUCTURE JOB NO. 5362

REVISED

STATE HIGHWAY COMMISSION OF WISCONSIN

FLOOR PLANS

SHOWING REINFORCEMENT

SPANS 1 TO 5 INCL. & SPANS 10 TO 12 INCL.

FOR

BRIDGE NO. 300

CITY OF LA CROSSE — LA CROSSE CO.

CORRECT: *G. J. ...*

APPROVED: *E. J. ...*

DATE: 11/23/37

DRAWN BY: *E. J. ...*

CHECKED BY: *E. J. ...*

NOTE: ALL VERTICAL TRUSS MEMBERS SHALL BE BUILT TRULY VERTICAL.

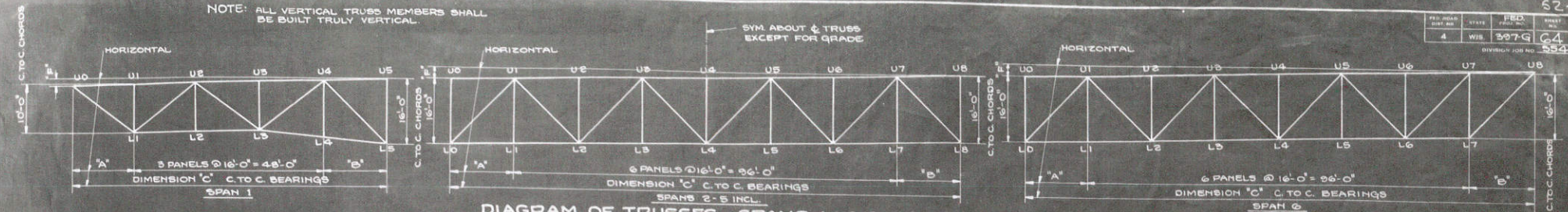


DIAGRAM OF TRUSSES - SPANS 1 TO 6 INCL. FOR DIMENSIONS 'A', 'B', 'C', 'D', 'E', 'F' SEE TABLE. STRESS AND MAKEUP OF TRUSSES

MEMBER	STRESS IN KIPS				TOTAL STRESS DIRECT BENDING IN KIPS	MAKEUP
	DEAD LOAD	ROADWAY LIVE LOAD	IMPACT	SIDEWALK LIVE LOAD		
U0 - U2	+116.0	-41.2	-10.1	-19.8	-185.1	2CS-15'-33.9" #1 PL 20"x 3/16"
U2 - U4	+171.0	-61.6	-15.0	-29.8	-277.4	50.7 2CS-15'-33.9" #1 PL 20"x 3/16"
U4 - U5	0	0	0	0	49.0	2CS-15'-33.9" #1 PL 20"x 3/16"
L1 - L3	+171.0	+61.6	+15.0	+29.8	+277.4	2CS-15'-33.9"
L3 - L5	+89.2	+32.2	+7.9	+15.6	+144.9	2CS-15'-33.9"
U0 - L1	+134.5	+57.3	+14.0	+23.4	+229.2	10'-54" B OR C
L1 - U2	-67.2	-28.8	-10.7	-14.1	-130.8	10'-41" B OR C
U2 - L3	0	+23.0	+6.9	+6.9	+36.8	10'-33" B OR C
L3 - U4	+28.2	+47.7	+19.2	+17.3	+112.4	10'-41" B OR C
U4 - L5	-124.0	-52.8	-12.9	-21.6	-211.3	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
U1 - L1	-35.6	-29.6	-8.0	-6.2	-76.4	10'-35" B OR C
U2 - L2	0	0	0	0	0	10'-35" B OR C
U3 - L3	-35.6	-29.6	-8.0	-6.2	-76.4	10'-35" B OR C
U4 - L4	0	0	0	0	0	10'-35" B OR C
U5 - L5	-17.8	-22.9	-6.9	-3.1	-50.7	10'-35" B OR C
U0 - U2	-121.0	-91.8	-22.3	-	-235.1	51.0 2CS-15'-33.9" #1 PL 20"x 3/16"
U2 - U4	-181.5	-138.0	-33.7	-	-353.2	51.0 2CS-15'-33.9" #1 PL 20"x 3/16"
U4 - U5	0	0	0	-	101.9	2CS-15'-33.9" #1 PL 20"x 3/16"
L1 - L3	+181.5	+138.0	+33.7	-	+353.2	2CS-15'-33.9"
L3 - L5	+94.7	+71.9	+17.5	-	+184.1	2CS-15'-33.9"
U0 - L1	+143.0	+128.0	+31.3	-	+302.3	10'-72" B OR C
L1 - U2	-71.4	-86.7	-23.9	-	-182.0	10'-49" B OR C
U2 - L3	0	+51.5	+15.5	-	+67.0	10'-35" B OR C
L3 - U4	+104.5	+107.0	+29.6	-	+241.1	10'-54" B OR C
U4 - L5	-131.5	-118.5	-28.9	-	-278.9	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
U1 - L1	-37.8	-59.7	-17.9	-	-115.4	10'-35" B OR C
U2 - L2	0	0	0	-	0	10'-35" B OR C
U3 - L3	-37.8	-59.7	-17.9	-	-115.4	10'-35" B OR C
U4 - L4	0	0	0	-	0	10'-35" B OR C
U5 - L5	-18.9	-31.3	-15.4	-	-65.6	10'-35" B OR C
U0 - U1	0	0	0	0	46.2	2CS-15'-33.9" #1 PL 20"x 3/16"
U1 - U3	-225.0	-65.1	-12.9	-30.5	-331.5	2CS-15'-33.9" #1 PL 20"x 3/16"
U3 - U4	-297.5	-86.6	-17.1	-40.6	-441.8	2CS-15'-33.9" #1 PL 20"x 3/16"
L0 - L2	+150.0	+88.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
L2 - L4	+279.0	+81.3	+16.1	+34.1	+414.5	2CS-15'-33.9"
L4 - L6	+184.0	+61.0	+12.1	+25.2	+282.3	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L6 - L7	+130.0	+38.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
L7 - U8	+130.0	+38.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
U0 - U1	0	0	0	0	0	10'-49" B OR C
U1 - L2	-194.0	-61.0	-18.1	-25.2	-288.3	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L2 - U3	+131.5	+48.5	+10.6	+18.9	+209.5	10'-49" B OR C
U3 - L4	-78.9	-36.9	-8.7	-15.5	-139.0	10'-49" B OR C
L4 - U5	+263.3	+71.1	+23.3	+39.6	+437.3	10'-49" B OR C
U5 - L6	-78.9	-36.9	-8.7	-15.5	-139.0	10'-49" B OR C
L6 - U7	+131.5	+48.5	+10.6	+18.9	+209.5	10'-49" B OR C
U7 - U8	-18.6	-22.9	-6.9	-3.1	-50.9	10'-35" B OR C
L0 - U0	0	0	0	0	0	10'-35" B OR C
L1 - U1	0	0	0	0	0	10'-35" B OR C
L2 - U2	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L3 - U3	0	0	0	0	0	10'-35" B OR C
L4 - U4	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L5 - U5	0	0	0	0	0	10'-35" B OR C
L6 - U6	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L7 - U7	-120.0	-48.1	-8.5	-17.8	-194.4	10'-49" B OR C
U0 - U1	0	0	0	0	102.0	2CS-15'-33.9" #1 PL 20"x 3/16"
U1 - U3	-236.5	-145.5	-28.8	-	-410.8	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U3 - U5	-315.0	-194.0	-38.4	-	-547.4	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U5 - U7	-336.5	-145.5	-28.8	-	-410.8	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U7 - U8	-158.0	-84.9	-16.8	-	-259.7	67.2 2CS-15'-33.9" #1 PL 20"x 3/16"
L0 - L2	+158.0	+84.9	+16.8	-	+259.7	2CS-15'-33.9"
L2 - L4	+295.5	+182.0	+36.0	-	+513.5	2CS-15'-33.9"
L4 - L6	+295.5	+182.0	+36.0	-	+513.5	2CS-15'-33.9"
L6 - L7	+158.0	+84.9	+16.8	-	+259.7	2CS-15'-33.9"
L7 - U8	+195.0	+136.5	+26.9	-	+358.4	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
U0 - U1	-195.0	-136.5	-26.9	-	-358.4	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L1 - L2	+139.5	+108.0	+23.6	-	+271.1	10'-60" B OR C
L2 - U3	-89.6	-82.6	-19.4	-	-185.6	10'-60" B OR C
U3 - L4	+27.9	+46.9	+15.3	-	+89.9	10'-49" B OR C
L4 - U5	+27.9	+46.9	+15.3	-	+89.9	10'-49" B OR C
U5 - L6	-89.6	-82.6	-19.4	-	-185.6	10'-54" B OR C
L6 - U7	+139.5	+108.0	+23.6	-	+271.1	10'-60" B OR C
L0 - U0	-19.7	-51.2	-15.4	-	-86.3	10'-35" B OR C
L1 - U1	0	0	0	0	0	10'-35" B OR C
L2 - U2	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L3 - U3	0	0	0	0	0	10'-35" B OR C
L4 - U4	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L5 - U5	0	0	0	0	0	10'-35" B OR C
L6 - U6	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L7 - U7	-138.0	-96.5	-19.1	-	-253.6	10'-60" B OR C

MEMBER	STRESS IN KIPS				TOTAL STRESS DIRECT BENDING IN FT KIPS	MAKEUP
	DEAD LOAD	ROADWAY LIVE LOAD	IMPACT	SIDEWALK LIVE LOAD		
U0 - U1	0	0	0	0	46.2	2CS-15'-33.9" #1 PL 20"x 3/16"
U1 - U3	-225.0	-65.1	-12.9	-30.5	-331.5	2CS-15'-33.9" #1 PL 20"x 3/16"
U3 - U5	-297.5	-86.6	-17.1	-40.6	-441.8	2CS-15'-33.9" #1 PL 20"x 3/16"
U5 - U7	-225.0	-65.1	-12.9	-30.5	-331.5	2CS-15'-33.9" #1 PL 20"x 3/16"
U7 - U8	-130.0	-38.0	-7.5	-17.8	-195.3	2CS-15'-33.9" #1 PL 20"x 3/16"
L0 - L2	+150.0	+88.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
L2 - L4	+279.0	+81.3	+16.1	+34.1	+414.5	2CS-15'-33.9"
L4 - L6	+184.0	+61.0	+12.1	+25.2	+282.3	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L6 - L7	+130.0	+38.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
L7 - U8	+130.0	+38.0	+7.5	+17.8	+195.3	2CS-15'-33.9"
U0 - U1	0	0	0	0	0	10'-49" B OR C
U1 - L2	-194.0	-61.0	-18.1	-25.2	-288.3	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L2 - U3	+131.5	+48.5	+10.6	+18.9	+209.5	10'-49" B OR C
U3 - L4	-78.9	-36.9	-8.7	-15.5	-139.0	10'-49" B OR C
L4 - U5	+263.3	+71.1	+23.3	+39.6	+437.3	10'-49" B OR C
U5 - L6	-78.9	-36.9	-8.7	-15.5	-139.0	10'-49" B OR C
L6 - U7	+131.5	+48.5	+10.6	+18.9	+209.5	10'-49" B OR C
U7 - U8	-18.6	-22.9	-6.9	-3.1	-50.9	10'-35" B OR C
L0 - U0	0	0	0	0	0	10'-35" B OR C
L1 - U1	0	0	0	0	0	10'-35" B OR C
L2 - U2	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L3 - U3	0	0	0	0	0	10'-35" B OR C
L4 - U4	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L5 - U5	0	0	0	0	0	10'-35" B OR C
L6 - U6	-37.2	-26.6	-8.0	-5.1	-76.9	10'-35" B OR C
L7 - U7	-120.0	-48.1	-8.5	-17.8	-194.4	10'-49" B OR C
U0 - U1	0	0	0	0	102.0	2CS-15'-33.9" #1 PL 20"x 3/16"
U1 - U3	-236.5	-145.5	-28.8	-	-410.8	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U3 - U5	-315.0	-194.0	-38.4	-	-547.4	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U5 - U7	-336.5	-145.5	-28.8	-	-410.8	50.3 2CS-15'-33.9" #1 PL 20"x 3/16"
U7 - U8	-158.0	-84.9	-16.8	-	-259.7	67.2 2CS-15'-33.9" #1 PL 20"x 3/16"
L0 - L2	+158.0	+84.9	+16.8	-	+259.7	2CS-15'-33.9"
L2 - L4	+295.5	+182.0	+36.0	-	+513.5	2CS-15'-33.9"
L4 - L6	+295.5	+182.0	+36.0	-	+513.5	2CS-15'-33.9"
L6 - L7	+158.0	+84.9	+16.8	-	+259.7	2CS-15'-33.9"
L7 - U8	+195.0	+136.5	+26.9	-	+358.4	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
U0 - U1	-195.0	-136.5	-26.9	-	-358.4	42.5 6"x3 1/2" #1 PL 10 1/2" x 10 1/2" B TO B
L1 - L2	+139.5	+108.0	+23.6	-	+271.1	10'-60" B OR C
L2 - U3	-89.6	-82.6	-19.4	-	-185.6	10'-60" B OR C
U3 - L4	+27.9	+46.9	+15.3	-	+89.9	10'-49" B OR C
L4 - U5	+27.9	+46.9	+15.3	-	+89.9	10'-49" B OR C
U5 - L6	-89.6	-82.6	-19.4	-	-185.6	10'-54" B OR C
L6 - U7	+139.5	+108.0	+23.6	-	+271.1	10'-60" B OR C
L0 - U0	-19.7	-51.2	-15.4	-	-86.3	10'-35" B OR C
L1 - U1	0	0	0	0	0	10'-35" B OR C
L2 - U2	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L3 - U3	0	0	0	0	0	10'-35" B OR C
L4 - U4	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L5 - U5	0	0	0	0	0	10'-35" B OR C
L6 - U6	-39.4	-59.7	-17.9	-	-117.0	10'-35" B OR C
L7 - U7	-138.0	-96.5	-19.1	-	-253.6	10'-60" B OR C

NOTE: THIS TABLE DOES NOT APPLY TO SPAN 4. FOR EXTERIOR TRUSSES IN THAT SPAN, SEE X7644.

LOCATION	ECCENTRICITY	BACK TO BACK (")
SPAN 1 - INTERIOR TRUSSES	2 1/4"	11 3/4"
SPAN 1 - EXTERIOR TRUSSES	1 3/16"	11 3/8"
SPANS 2-6 INCL - INTERIOR TRUSSES	2 1/16"	11 1/2"
SPANS 2,3,5 & 6 - EXTERIOR TRUSSES	2 3/16"	11 1/4"
SPAN 4 - EXTERIOR TRUSSES	2 3/16"	11 3/8"

END PANEL DIMENSIONS

LOCATION	DIMEN. 'A'	DIMEN. 'B'	DIMEN. 'C'	DIMENSION 'E'
SPAN 1				
NO. EXTERIOR TRUSS	15'-5"	15'-5 1/2"	78'-10 1/2"	4'-0"
INTERIOR TRUSS	16'-0"	16'-0"	80'-0"	4'-0"
SO. EXTERIOR TRUSS	16'-7"	16'-6 1/2"	81'-1 1/2"	4'-0"
SPANS 2,3,4 & 5				
NO. EXTERIOR TRUSS	15'-5"	15'-5 1/2"	126'-5 1/2"	6'-4 1/2"
INTERIOR TRUSS	16'-0"	16'-0"	128'-0"	6'-1 1/2"
SO. EXTERIOR TRUSS	16'-10 1/2"	16'-10 1/2"	129'-8 1/2"	6'-4 1/2"
SPAN 6				
NO. EXTERIOR TRUSS	15'-11 1/2"	16'-0"	127'-11 1/2"	4'-4 1/2"
INTERIOR TRUSS	16'-0"	16'-0"	128'-0"	5'-8 1/2"
SO. EXTERIOR TRUSS	16'-0"	16'-0"	128'-0"	5'-0"

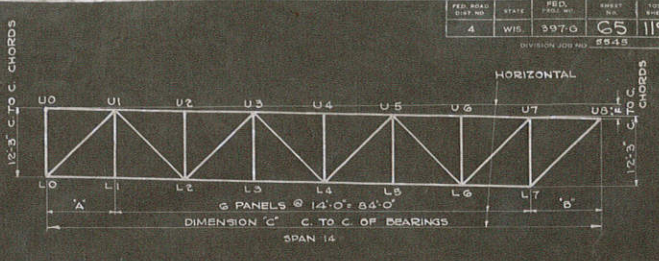
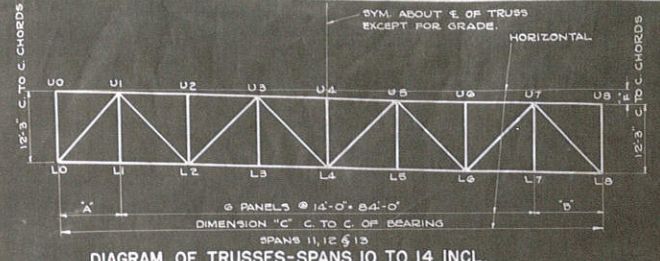
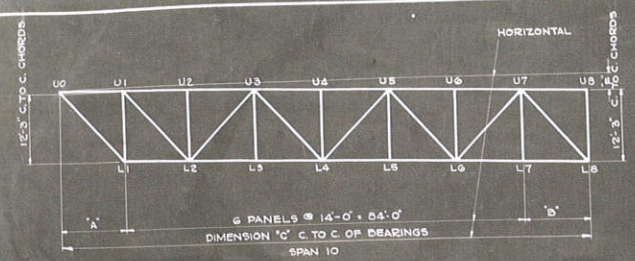
CAMBER IN TRUSSES

LOCATION	FABRICATED CAMBER	COMPUTED DEAD LOAD DEFLECTION	COMPUTED RESIDUAL CAMBER
SPAN 1			
EXTERIOR TRUSSES	1"	1/8"	1/2"
INTERIOR TRUSS	1"	7/16"	3/16"
SPANS 2-6 INCL.			
EXTERIOR TRUSSES	2"	1 1/4"	3/4"
INTERIOR TRUSSES	2"	1 1/8"	7/8"

DIMENSIONS "D" & "E"

SEE X7644

PANEL POINT	SPAN 1		SPAN 2 & 3		SPAN 4		SPAN 5		SPAN 6	
	DIM. 'D'	DIM. 'E'	DIM. 'D'	DIM. 'E'	DIM. 'D'	DIM. 'E'	DIM. 'D'	DIM. 'E'	DIM. 'D'	DIM. 'E'
U0	6 1/2"	9 3/4"	7 1/8"	10 1/4"	6					



NOTE: ALL VERTICAL TRUSS MEMBERS SHALL BE BUILT TRULY VERTICAL.

DIAGRAM OF TRUSSES-SPANS 10 TO 14 INCL. LOOKING NORTH

STRESS AND MAKEUP OF TRUSSES

MEMBER		STRESS IN KIPS				TOTAL STRESS		MAKEUP
SPAN 10	SPAN 14	DEAD LOAD	ROADWAY LIVE LOAD	IMPACT	SIDEWALK LIVE LOAD	DIRECT IN KIPS	BENDING IN FT. KIPS	
U0-U1	U6-U7	-128.1	-39.7	-8.4	-18.8	-194.9	31.5	2-15'-33.9" D-1 R 20'-0" A
U1-U2	U7-U8	-219.3	-60.1	-14.4	-32.2	-344.2	23.5	2-15'-33.9" D-1 R 20'-0" A
U2-U3	U3-U4	-232.5	-90.7	-19.1	-43.0	-445.3	23.5	2-15'-33.9" D-1 R 20'-0" A
U3-U4	U4-U5	-219.5	-68.1	-14.4	-32.2	-344.2	23.5	2-15'-33.9" D-1 R 20'-0" A
U4-U5	U5-U6	0	0	0	0	0	37.9	2-15'-33.9" D-1 R 20'-0" A
U5-U6	U6-U7	0	0	0	0	0	37.9	2-15'-33.9" D-1 R 20'-0" A
L1-L2	L7-L8	+128.0	+39.7	+8.4	+18.8	+194.9	---	2-15'-33.9" D
L2-L3	L8-L9	+274.5	+85.1	+18.0	+40.3	+417.9	---	2-15'-45" D
L3-L4	L9-L0	+274.5	+85.1	+18.0	+40.3	+417.9	---	2-15'-45" D
L4-L5	L0-L1	+128.0	+39.7	+8.4	+18.8	+194.9	---	2-15'-33.9" D
U0-L1	U6-L7	+170.0	+60.6	+12.6	+25.0	+268.4	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U1-L2	U7-L8	-112.0	-39.9	-8.5	-16.4	-176.8	---	10'-45" B OR C
L1-U2	L7-U8	+121.5	+48.2	+11.2	+18.7	+199.6	---	10'-45" B OR C
L2-U3	L8-U9	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
L3-U4	L9-U0	-72.9	-57.0	-9.2	-13.4	-132.5	---	10'-41" B OR C
U3-L3	U4-L4	0	0	0	0	0	---	10'-33" B OR C
U4-L4	U5-L5	+24.3	+19.1	+7.2	+13.9	+65.3	---	10'-33" B OR C
U5-L5	U6-L6	+24.3	+19.1	+7.2	+13.9	+65.3	---	10'-33" B OR C
L4-U4	L5-U5	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
U5-L5	U6-L6	0	0	0	0	0	---	10'-33" B OR C
U6-L6	U7-L7	-72.9	-57.0	-9.2	-13.4	-132.5	---	10'-41" B OR C
L6-U6	L7-U7	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
L7-U7	L8-U8	+121.5	+48.2	+11.2	+18.7	+199.6	---	10'-45" B OR C
U7-L7	U8-L8	0	0	0	0	0	---	10'-33" B OR C
U8-L8	U9-L9	+170.0	+60.6	+12.6	+25.0	+268.4	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U9-L9	U0-L0	-112.0	-39.9	-8.5	-16.4	-176.8	---	10'-45" B OR C
U0-U1	U6-U7	+138.0	-39.6	-18.7	-24.3	56.3	---	2-15'-33.9" D-1 R 20'-0" A
U1-U2	U7-U8	+233.5	-152.0	-32.1	-41.7	42.2	---	2-15'-33.9" D-1 R 20'-0" A
U2-U3	U3-U4	+311.0	-202.5	-42.7	-55.6	42.2	---	2-15'-50" D-1 R 20'-0" A
U3-U4	U4-U5	+233.5	-152.0	-32.1	-41.7	42.2	---	2-15'-33.9" D-1 R 20'-0" A
U4-U5	U5-U6	0	0	0	0	88.2	---	2-15'-33.9" D-1 R 20'-0" A
U5-U6	U6-U7	0	0	0	0	88.2	---	2-15'-33.9" D-1 R 20'-0" A
L1-L2	L7-L8	+138.0	+39.6	+18.7	+24.3	+243.3	---	2-15'-33.9" D
L2-L3	L8-L9	+291.5	+190.0	+40.1	+52.6	---	---	2-15'-50" D
L3-L4	L9-L0	+291.5	+190.0	+40.1	+52.6	---	---	2-15'-50" D
L4-L5	L0-L1	+138.0	+39.6	+18.7	+24.3	+243.3	---	2-15'-33.9" D
U0-L1	U6-L7	+180.5	+135.5	+28.6	+34.6	---	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U1-L2	U7-L8	-119.0	-39.1	-18.8	-22.9	---	---	10'-60" B OR C
U2-L3	U3-L4	+129.0	+107.5	+24.9	+26.4	---	---	10'-60" B OR C
U3-L4	U4-L5	-34.0	-27.6	-17.3	-18.9	---	---	10'-33" B OR C
U4-L5	U5-L6	-77.4	-62.6	-20.4	-18.0	---	---	10'-49" B OR C
U5-L6	U6-L7	0	0	0	0	---	---	10'-33" B OR C
U6-L7	U7-L8	+25.8	+20.5	+11.3	+12.2	---	---	10'-33" B OR C
U7-L8	U8-L9	+25.8	+20.5	+11.3	+12.2	---	---	10'-33" B OR C
L4-U4	L5-U5	-34.0	-27.6	-17.3	-18.9	---	---	10'-33" B OR C
U5-L5	U6-L6	0	0	0	0	---	---	10'-33" B OR C
U6-L6	U7-L7	-77.4	-62.6	-20.4	-18.0	---	---	10'-49" B OR C
L6-U6	L7-U7	-34.0	-27.6	-17.3	-18.9	---	---	10'-33" B OR C
L7-U7	L8-U8	+129.0	+107.5	+24.9	+26.4	---	---	10'-60" B OR C
U7-L7	U8-L8	0	0	0	0	---	---	10'-33" B OR C
U8-L8	U9-L9	+180.5	+135.5	+28.6	+34.6	---	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U9-L9	U0-L0	-119.0	-39.1	-18.8	-22.9	---	---	10'-60" B OR C

MEMBER		STRESS IN KIPS				TOTAL STRESS		MAKEUP
SPAN 11, 12 & 13	SPAN 14	DEAD LOAD	ROADWAY LIVE LOAD	IMPACT	SIDEWALK LIVE LOAD	DIRECT IN KIPS	BENDING IN FT. KIPS	
U0-U1	U6-U7	0	0	0	0	0	37.9	2-15'-33.9" D-1 R 20'-0" A
U1-U2	U7-U8	-219.3	-60.1	-14.4	-32.2	-344.2	23.5	2-15'-33.9" D-1 R 20'-0" A
U2-U3	U3-U4	-232.5	-90.7	-19.1	-43.0	-445.3	23.5	2-15'-33.9" D-1 R 20'-0" A
L0-L1	L6-L7	+128.0	+39.7	+8.4	+18.8	+194.9	---	2-15'-33.9" D
L1-L2	L7-L8	+274.5	+85.1	+18.0	+40.3	+417.9	---	2-15'-45" D
L2-L3	L8-L9	+274.5	+85.1	+18.0	+40.3	+417.9	---	2-15'-45" D
L3-L4	L9-L0	+128.0	+39.7	+8.4	+18.8	+194.9	---	2-15'-33.9" D
U0-L1	U6-L7	+170.0	+60.6	+12.6	+25.0	+268.4	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U1-L2	U7-L8	-112.0	-39.9	-8.5	-16.4	-176.8	---	10'-45" B OR C
L1-U2	L7-U8	+121.5	+48.2	+11.2	+18.7	+199.6	---	10'-45" B OR C
L2-U3	L8-U9	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
L3-U4	L9-U0	-72.9	-57.0	-9.2	-13.4	-132.5	---	10'-41" B OR C
U3-L3	U4-L4	0	0	0	0	0	---	10'-33" B OR C
U4-L4	U5-L5	+24.3	+19.1	+7.2	+13.9	+65.3	---	10'-33" B OR C
U5-L5	U6-L6	+24.3	+19.1	+7.2	+13.9	+65.3	---	10'-33" B OR C
L4-U4	L5-U5	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
U5-L5	U6-L6	0	0	0	0	0	---	10'-33" B OR C
U6-L6	U7-L7	-72.9	-57.0	-9.2	-13.4	-132.5	---	10'-41" B OR C
L6-U6	L7-U7	-32.0	-25.6	-7.7	-4.7	-70.2	---	10'-33" B OR C
L7-U7	L8-U8	+121.5	+48.2	+11.2	+18.7	+199.6	---	10'-45" B OR C
U7-L7	U8-L8	0	0	0	0	0	---	10'-33" B OR C
U8-L8	U9-L9	+170.0	+60.6	+12.6	+25.0	+268.4	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U9-L9	U0-L0	-112.0	-39.9	-8.5	-16.4	-176.8	---	10'-45" B OR C
U0-U1	U6-U7	-233.5	-152.0	-32.1	-41.7	-417.6	4.2	2-15'-33.9" D-1 R 20'-0" A
U1-U2	U7-U8	-311.0	-202.5	-42.7	-55.6	-556.2	4.2	2-15'-50" D-1 R 20'-0" A
L0-L1	L6-L7	+138.0	+39.6	+18.7	+24.3	+243.3	---	2-15'-33.9" D
L1-L2	L7-L8	+291.5	+190.0	+40.1	+52.6	---	---	2-15'-50" D
L2-L3	L8-L9	+291.5	+190.0	+40.1	+52.6	---	---	2-15'-50" D
L3-L4	L9-L0	+138.0	+39.6	+18.7	+24.3	+243.3	---	2-15'-33.9" D
U0-L1	U6-L7	+180.5	+135.5	+28.6	+34.6	---	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U1-L2	U7-L8	-129.0	-107.5	-24.9	-26.4	-261.4	---	10'-49" B OR C
L1-U2	L7-U8	+129.0	+107.5	+24.9	+26.4	+261.4	---	10'-49" B OR C
L2-U3	L8-U9	-77.4	-62.6	-20.4	-18.0	-180.4	---	10'-33" B OR C
U3-L3	U4-L4	0	0	0	0	0	---	10'-33" B OR C
U4-L4	U5-L5	+25.8	+20.5	+11.3	+12.2	+123.4	---	10'-33" B OR C
U5-L5	U6-L6	+25.8	+20.5	+11.3	+12.2	+123.4	---	10'-33" B OR C
L4-U4	L5-U5	-34.0	-27.6	-17.3	-18.9	-108.9	---	10'-33" B OR C
U5-L5	U6-L6	0	0	0	0	0	---	10'-33" B OR C
U6-L6	U7-L7	-77.4	-62.6	-20.4	-18.0	-180.4	---	10'-33" B OR C
L6-U6	L7-U7	-34.0	-27.6	-17.3	-18.9	-108.9	---	10'-33" B OR C
L7-U7	L8-U8	+129.0	+107.5	+24.9	+26.4	+261.4	---	10'-60" B OR C
U7-L7	U8-L8	0	0	0	0	0	---	10'-33" B OR C
U8-L8	U9-L9	+180.5	+135.5	+28.6	+34.6	---	---	1 R 10 1/2" x 4 1/2" @ 3 1/2" x 10 1/2" D TO D
U9-L9	U0-L0	-119.0	-39.1	-18.8	-22.9	-188.9	---	10'-60" B OR C

END PANEL DIMENSIONS

LOCATION	DIMENSION 'A'	DIMENSION 'B'	DIMENSION 'C'	DIMENSION 'D'
SPANS 10 & 11				
NO. EXTERIOR TRUSS	13'-2 1/2"	13'-2 1/2"	110'-5 3/4"	2'-2 1/2"
INTERIOR TRUSS	14'-0"	14'-0"	112'-0"	2'-7 1/2"
SO. EXTERIOR TRUSS	14'-0 1/2"	14'-0 1/2"	113'-6 1/4"	2'-2 1/2"
SPAN 12				
NO. EXTERIOR TRUSS	13'-3 3/4"	13'-6 1/4"	110'-9 1/4"	3'-1 1/2"
INTERIOR TRUSS	14'-0"	14'-0"	112'-0"	3'-1 1/2"
SO. EXTERIOR TRUSS	14'-0 1/2"	14'-0 1/2"	113'-2 1/4"	3'-1 1/2"
SPANS 13 & 14				
NO. EXTERIOR TRUSS	14'-0"	14'-0"	112'-0"	3'-7 1/2"
INTERIOR TRUSS	14'-0"	14'-0"	112'-0"	4'-2 1/2"
SO. EXTERIOR TRUSS	14'-0"	14'-0"	112'-0"	3'-7 1/2"

DIMENSIONS D & E

PANEL POINT	SPAN 10, 11 & 12		SPAN 13		SPAN 14	
	DIM. D	DIM. E	DIM. D	DIM. E	DIM. D	DIM. E
U0	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U1	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U2	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U3	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U4	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U5	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U6	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U7	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U8	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"
U9	6 1/2"	9 1/2"	6 1/2"	9 1/2"	6 1/2"	9 1/2"

CAMBER IN TRUSSES

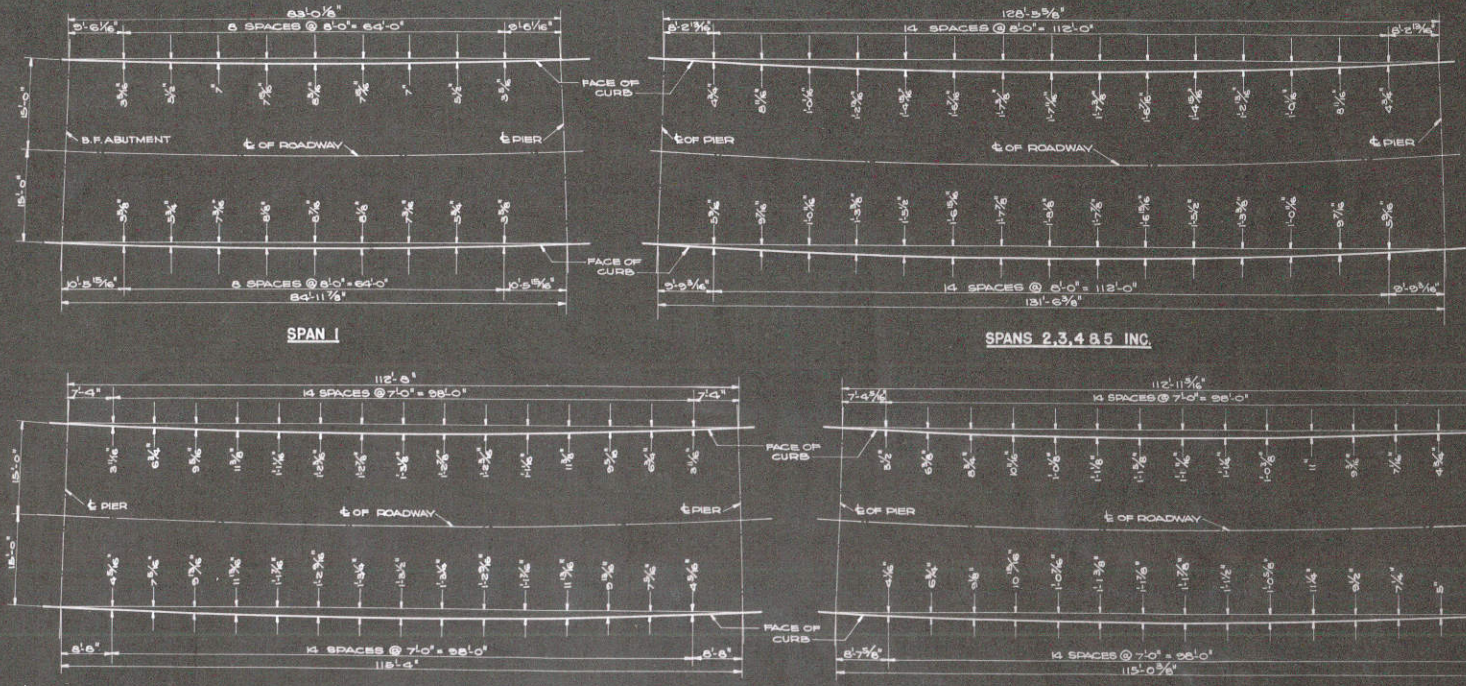
LOCATION	FABRICATED CAMBER	COMPUTED DEAD LOAD DEFLECTION	COMPUTED RESIDUAL CAMBER
EXTERIOR TRUSSES	1 1/2"	1 1/2"	0"
INTERIOR TRUSSES	1 1/2"	1 1/2"	0"

SPANS 10 TO 14 INCL.

TRUSS	ECCENTRICITY	BACK TO BACK OF CD
EXTERIOR	2 1/2"	11 1/2"
INTERIOR	2 1/2"	11 1/2"



STRUCTURE JOB NO. 8262
 STATE HIGHWAY COMMISSION OF WISCONSIN
TRUSS MAKEUP
 SPANS 10 TO 14 INCL.
 FOR
BRIDGE NO. 300
 CITY OF LA CROSSE - LA CROSSE CO.
 CORRECT: G. N. Fisher
 APPROVED: G. N. Fisher
 DRAWN: G. N. Fisher
 CHECKED: G. N. Fisher



STRESSES AND MAKE-UP FOR EXTERIOR TRUSS - SPAN-4

MEMBER	STRESS IN KIPS						TOTAL STRESS		MAKE-UP
	DEAD LOAD	ROWY. LIVE LOAD	IMPACT	SWK. LIVE LOAD	STAIRWAY DEAD LOAD	STAIRWAY LIVE LOAD	DIRECT IN KIPS	BENDING IN FT. KIPS	
U ₆ -U ₁	0	0	0	0	0	0	46.2	2[² -18 ³ -33.9 ³ -12-20 ³ × ³ / ₁₆]	
U ₁ -U ₂	-225.0	-65.1	-12.9	-20.5	-11.2	-7.6	-382.3	2[² -18 ³ -33.9 ³ -12-20 ³ × ³ / ₁₆]	
U ₂ -U ₃	-300.0	-86.6	-17.1	-40.6	-22.4	-15.2	-481.9	2[² -18 ³ -40 ³ -12-20 ³ × ³ / ₁₆]	
U ₃ -U ₇	-225.0	-65.1	-12.9	-30.5	-25.4	-19.8	-378.7	2[² -18 ³ -33.9 ³ -12-20 ³ × ³ / ₁₆]	
U ₇ -U ₈	0	0	0	0	0	0	46.2	2[² -18 ³ -33.9 ³ -12-20 ³ × ³ / ₁₆]	
L ₂ -L ₂	+131.5	+38.0	+7.5	+17.8	+5.6	+3.8	+204.2	2[² -18 ³ -33.9 ³]	
L ₂ -L ₄	+281.5	+81.3	+16.1	+38.1	+16.0	+11.4	+445.2	2[² -18 ³ -50 ³]	
L ₄ -L ₆	+131.5	+38.0	+7.5	+17.8	+12.7	+9.9	+217.4	2[² -18 ³ -33.9 ³]	
L ₆ -U ₁	+185.5	-61.0	-12.1	-25.2	-7.9	-5.4	-297.1	4L ² -6 ⁴ × ³ / ₁₆ × ³ / ₁₆ -12-10 ³ × ³ / ₈ -10 ³ × ³ / ₈ B. TO B.	
U ₁ -L ₂	+132.5	+48.3	+10.6	+18.9	+7.9	+5.4	+233.6	10 ³ -49 ³ B. OR C.	
L ₂ -U ₃	-79.6	-36.9	-8.7	-13.5	-7.9	-5.4	-152.0	10 ³ -49 ³ B. OR C.	
U ₃ -L ₄	+26.5	+18.1	+5.0	+5.4	+7.9	+5.4	+82.4	10 ³ -49 ³ B. OR C.	
L ₄ -U ₅	+26.5	+18.1	+5.0	+5.4	+7.9	+5.4	+82.4	10 ³ -49 ³ B. OR C.	
U ₅ -L ₆	-79.6	-36.9	-8.7	-13.5	-7.9	-5.4	-152.0	10 ³ -49 ³ B. OR C.	
L ₆ -U ₇	+132.5	+48.3	+10.6	+18.9	+18.0	+14.0	+242.3	10 ³ -54 ³ B. OR C.	
U ₇ -L ₈	-185.5	-61.0	-12.1	-25.2	-18.0	-14.0	-315.6	4L ² -6 ⁴ × ³ / ₁₆ × ³ / ₁₆ -12-10 ³ × ³ / ₈ -10 ³ × ³ / ₈ B. TO B.	
L ₈ -U ₈	-10.9	-2.9	-0.9	-2.5	0	0	-51.1	10 ³ -33 ³ B. OR C.	

MEMBER	STRESS IN KIPS						TOTAL STRESS		MAKE-UP
	DEAD LOAD	ROWY. LIVE LOAD	IMPACT	SWK. LIVE LOAD	STAIRWAY DEAD LOAD	STAIRWAY LIVE LOAD	DIRECT IN KIPS	BENDING IN FT. KIPS	
L ₁ -U ₁	0	0	0	0	0	0	0	0	10 ³ -33 ³ B. OR C.
L ₁ -U ₂	-375	-26.6	-8.0	-5.1	0	0	-77.2	0	10 ³ -33 ³ B. OR C.
L ₂ -U ₃	0	0	0	0	0	0	-77.2	0	10 ³ -33 ³ B. OR C.
L ₂ -U ₄	-375	-26.6	-8.0	-5.1	0	0	-77.2	0	10 ³ -33 ³ B. OR C.
L ₃ -U ₅	0	0	0	0	0	0	-97.8	0	10 ³ -33 ³ B. OR C.
L ₃ -U ₆	-375	-26.6	-8.0	-5.1	-10.1	-10.5	-97.8	0	10 ³ -33 ³ B. OR C.
L ₇ -U ₇	0	0	0	0	0	0	0	0	10 ³ -33 ³ B. OR C.
L ₇ -U ₈	-15.8	-22.9	-6.9	-2.5	0	0	-51.1	0	10 ³ -33 ³ B. OR C.

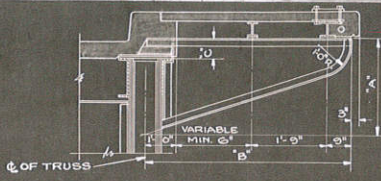
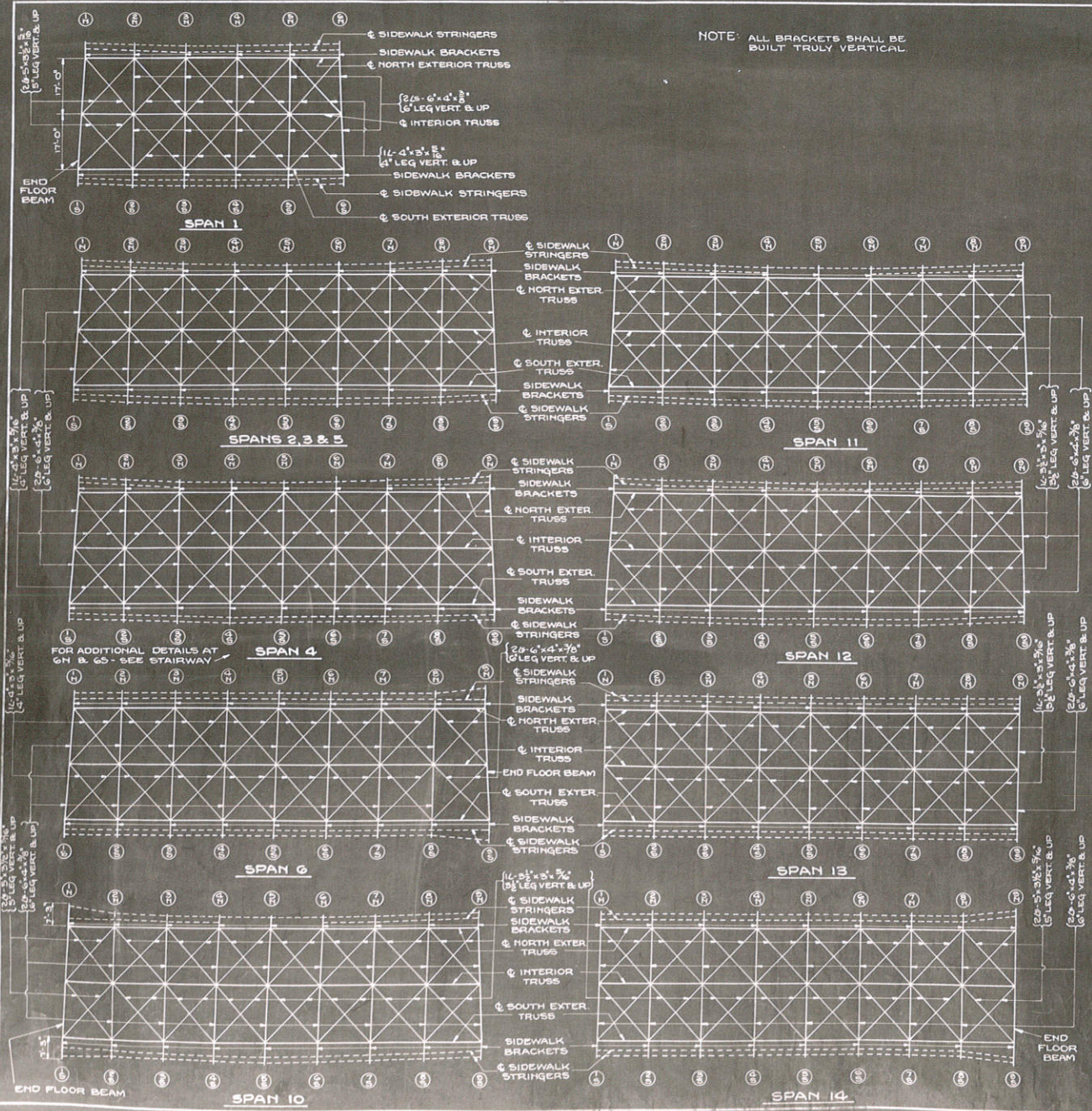
NOTE: THE LINE DIAGRAM FOR THIS TRUSS IS SHOWN ON X-7642.

41 OF 80

STRUCTURE JOB NO. 5545
 STATE HIGHWAY COMMISSION OF WISCONSIN
TRUSS MAKEUP-SPAN 4 AND CURB OFFSETS
 FOR
BRIDGE NO. 300
 CITY OF LA CROSSE - LA CROSSE CO.
 CORRECT: *E. J. Koster*
 APPROVED: *E. J. Koster*
 CHIEF ENGINEER
 DIVISION OF HIGHWAYS
 WISCONSIN DEPARTMENT OF TRANSPORTATION

X7644

NOTE: ALL BRACKETS SHALL BE BUILT TRULY VERTICAL.



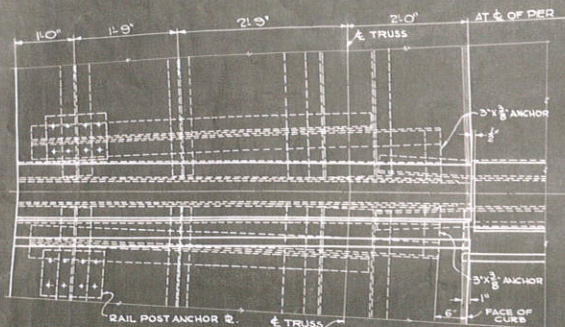
* DIMENSION 'A' FOR 1N & 1S = 2'-2 1/2" FOR SPAN 10
"B" = 5'-3 1/2" "C" = 7'-3"

SPAN NO.	BRACKET NO.	DIMENSION			
		"A"	"B"	"C"	
1	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	2 N	2'-6"	4'-10 1/8"	8 1/8"	
	2 S	2'-6"	4'-10 1/8"	8 1/8"	
	3 N	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	3 S	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	4 N	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	4 S	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	5 N	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	5 S	2'-5 3/8"	4'-7 3/8"	8 1/8"	
	6 N	2'-7 3/8"	5'-2 3/8"	8 3/8"	
	6 S	2'-7 3/8"	5'-2 3/8"	8 3/8"	
	2 & 3	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
		1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
2 N		2'-4 3/4"	4'-0 1/4"	8 1/4"	
2 S		2'-4 3/4"	4'-0 1/4"	8 1/4"	
3 N		2'-2 3/4"	4'-0"	8 1/4"	
3 S		2'-2 3/4"	4'-0"	8 1/4"	
4 N		2'-1 1/4"	3'-7 1/8"	8 1/8"	
4 S		2'-1 1/4"	3'-7 1/8"	8 1/8"	
5 N		2'-1 1/4"	3'-7 1/8"	8 1/8"	
5 S		2'-1 1/4"	3'-7 1/8"	8 1/8"	
6 N		2'-1 1/4"	3'-7 1/8"	8 1/8"	
6 S		2'-1 1/4"	3'-7 1/8"	8 1/8"	
7 N		2'-1 1/4"	3'-7 1/8"	8 1/8"	
7 S		2'-1 1/4"	3'-7 1/8"	8 1/8"	
4	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	2 N	2'-4 3/4"	4'-0 1/4"	8 1/4"	
	2 S	2'-4 3/4"	4'-0 1/4"	8 1/4"	
	3 N	2'-2 3/4"	4'-0"	8 1/4"	
	3 S	2'-2 3/4"	4'-0"	8 1/4"	
	4 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	4 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	5 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	5 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	6 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	6 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	7 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	7 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
6	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"	
	2 N	2'-4 3/4"	4'-0 1/4"	8 1/4"	
	2 S	2'-4 3/4"	4'-0 1/4"	8 1/4"	
	3 N	2'-2 3/4"	4'-0"	8 1/4"	
	3 S	2'-2 3/4"	4'-0"	8 1/4"	
	4 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	4 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	5 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	5 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	6 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	6 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	7 N	2'-1 1/4"	3'-7 1/8"	8 1/8"	
	7 S	2'-1 1/4"	3'-7 1/8"	8 1/8"	

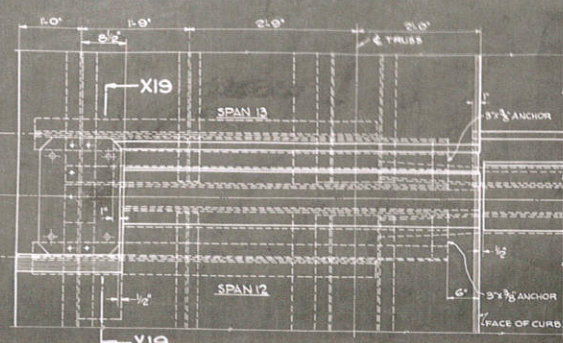
SPAN NO.	BRACKET NO.	DIMENSION		
		"A"	"B"	"C"
10 & 11	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	2 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
	3 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	3 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
	4 N	2'-3"	4'-0 3/8"	8 1/8"
	4 S	2'-3"	4'-0 3/8"	8 1/8"
	5 N	2'-0"	4'-0 3/8"	8 1/8"
	5 S	2'-0"	4'-0 3/8"	8 1/8"
	6 N	2'-3"	4'-0 3/8"	8 1/8"
	6 S	2'-3"	4'-0 3/8"	8 1/8"
	7 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	7 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
12	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	2 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
	3 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	3 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
	4 N	2'-3"	4'-0 3/8"	8 1/8"
	4 S	2'-3"	4'-0 3/8"	8 1/8"
	5 N	2'-0"	4'-0 3/8"	8 1/8"
	5 S	2'-0"	4'-0 3/8"	8 1/8"
	6 N	2'-3"	4'-0 3/8"	8 1/8"
	6 S	2'-3"	4'-0 3/8"	8 1/8"
	7 N	2'-5 3/8"	4'-7 3/8"	8 1/8"
	7 S	2'-5 3/8"	4'-7 3/8"	8 1/8"
13	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	3 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	3 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	4 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	4 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	5 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	5 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	6 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	6 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	7 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	7 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
14	1 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	1 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	2 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	3 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	3 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	4 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	4 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	5 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	5 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	6 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	6 S	2'-7 1/2"	5'-2 3/8"	8 3/8"
	7 N	2'-7 1/2"	5'-2 3/8"	8 3/8"
	7 S	2'-7 1/2"	5'-2 3/8"	8 3/8"

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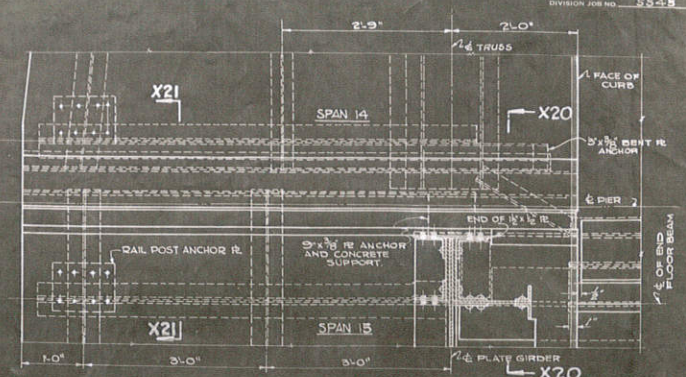
STRUCTURE JOB NO. 5532
 STATE HIGHWAY COMMISSION OF WISCONSIN
BRACKETS & BRACING
 SPANS 1 TO 6 INCL. & SPANS 10 TO 14 INCL.
 FOR
BRIDGE NO. 300
 CITY OF LA CROSSE — LA CROSSE CO.
 CONTRACTOR: *G. J. Knap*
 ENGINEER: *E. H. Reilly*



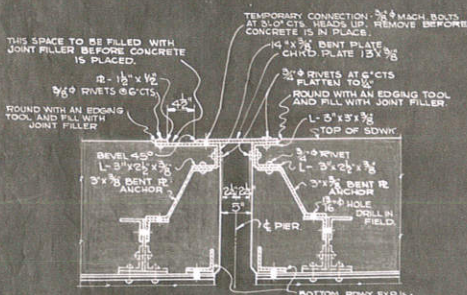
**PART PLAN OF SDWK AT PIERS 1 TO 5 INCL.
 PIERS 10, 11 & 13 SIMILAR.**



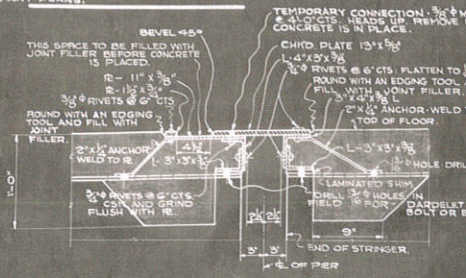
PART PLAN OF SDWK AT PIER 12.



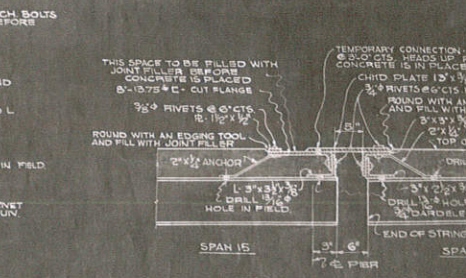
PART PLAN OF SDWK AT PIER 14.



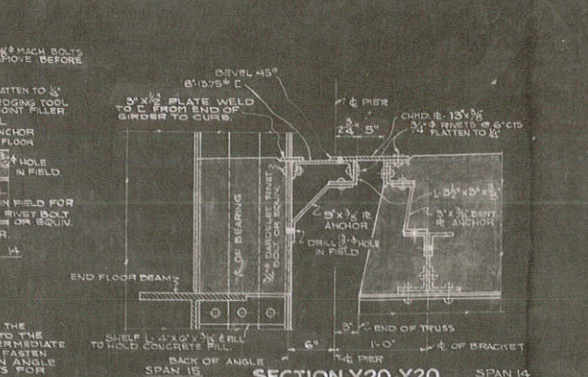
**SECTION THRU SDWK EXP. JOINT
 AT PIERS 1 TO 5 INCL. & 10 TO 13 INCL.
 SECTION TAKEN NEAR CURB.**



**SECTION THRU RDWY. EXP. JOINT
 AT PIERS 1 TO 5 INCL. & 10 TO 13 INCL.**

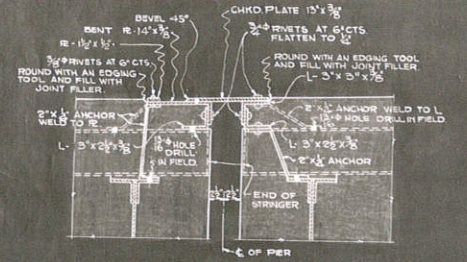


SECTION X21-X21.

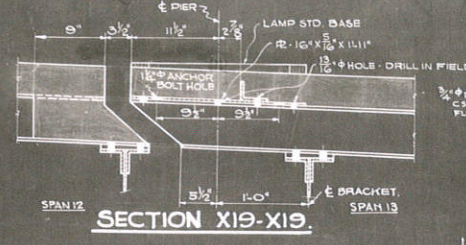


SECTION X20-X20.

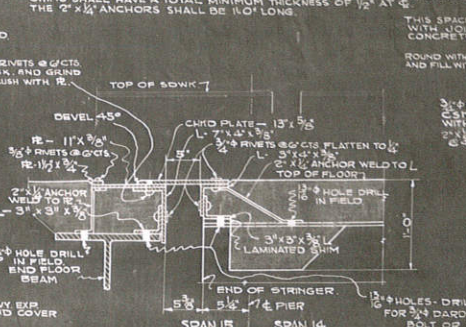
NOTES FOR EXPANSION JOINTS
 SIDEWALK EXPANSION JOINTS SHOULD UNLESS OTHERWISE INDICATED BE FASTENED TO THE SUPPORTS BY 2" x 1/2" ANCHOR BARS WELDED TO THE JOINT AND RIVETED TO THE SUPPORTS. IF SPACING OF THE SUPPORTS IS OVER 3'-0" PROVIDE INTERMEDIATE ANCHORS AT NOT OVER 10'-0" CTS. ANCHORS WHICH DO NOT FASTEN TO THE SUPPORTS SHALL HAVE ONE END BENT SIMILAR TO PROTECTION ANGLE. THE CONNECTION OF EXPANSION JOINTS SHALL BE DRILLED IN THE FIELD. SHIMS SHALL HAVE A TOTAL MINIMUM THICKNESS OF 1/2" AT 6". THE 2" x 1/2" ANCHORS SHALL BE 11'-0" LONG.



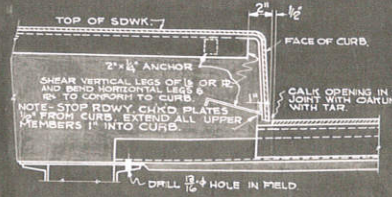
**SECTION THRU SDWK EXP. JOINT
 AT PIERS 1 TO 5 INCL. & 10 TO 13 INCL.
 SECTION TAKEN BETWEEN STRINGER & TRUSS.**



SECTION X19-X19.



**SECTION THRU RDWY. EXP. JOINT NEAR & RDWY.
 AT PIER 14. (43) OF (84)**



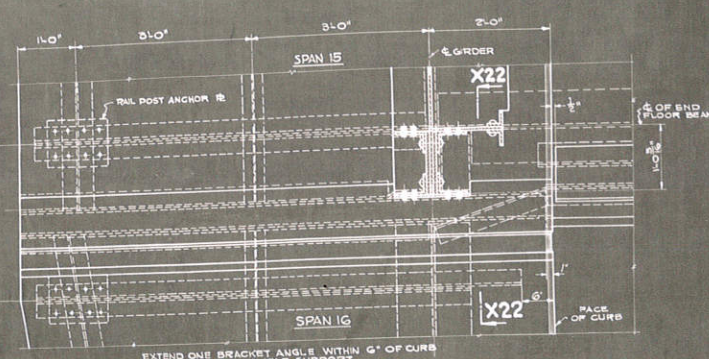
**SECTION THRU RDWY. EXP. JOINT
 AT PIER 14.**

TYPICAL EXP. DETAIL AT FACE OF CURB.

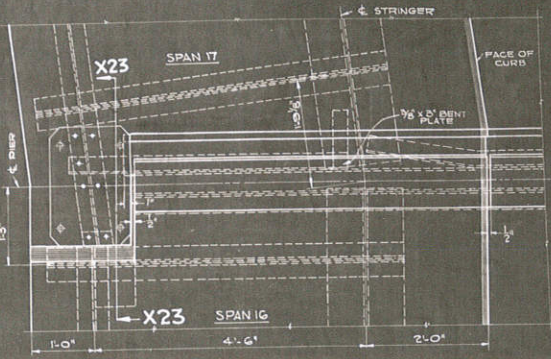
REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN
E.C.B. P.C.G.	EXPANSION DETAILS
B.L.S.	PIERS 1 TO 5 INCL. & PIERS 10 TO 14 INCL.
	FOR
	BRIDGE NO. 300
	CITY OF LA CROSSE — LA CROSSE, CO.
CHECKED	
APPROVED	
DESIGNED	
DRAWN	
CHECKED	

FED. ROAD DIST. NO.	STATE	FIELD PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	307-4	69	119

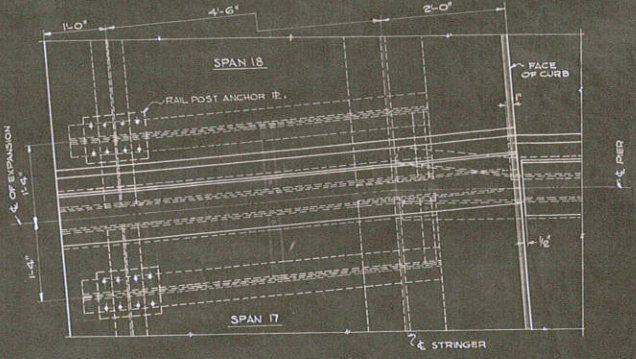
DIVISION JOB NO. 5545



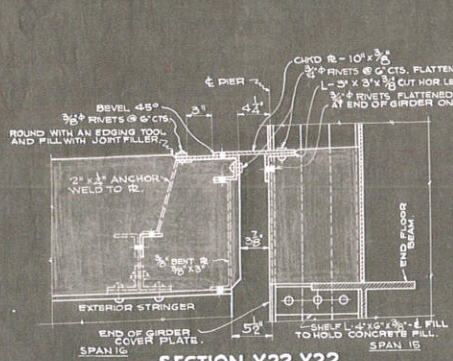
PART PLAN OF SDWK. AT PIER 15.



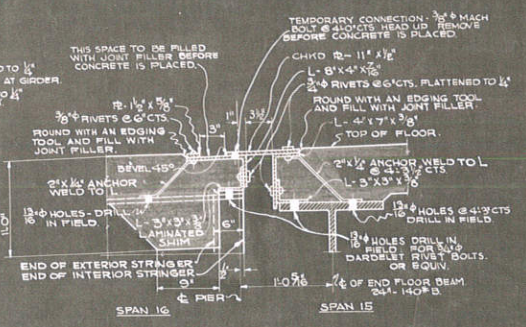
PART PLAN OF SDWK. AT PIER 16.



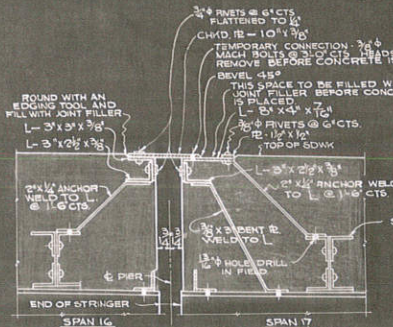
PART PLAN OF SDWK. AT PIER 17.



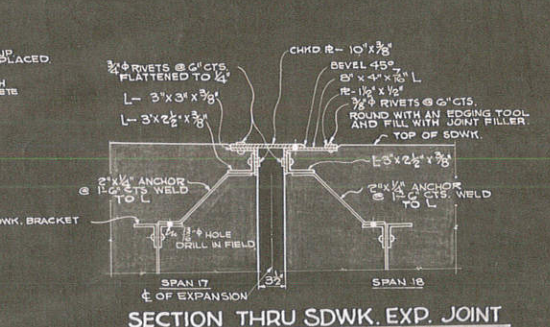
SECTION X22-X22.



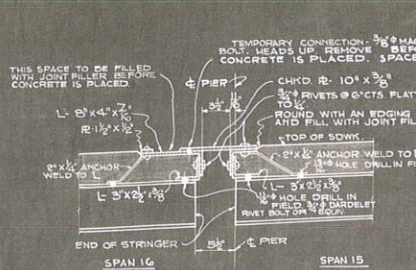
SECTION THRU RDWY. EXP. JOINT AT PIER 15.



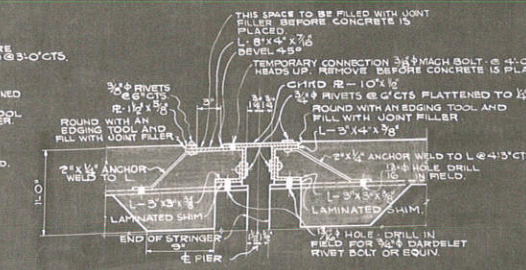
SECTION THRU SDWK. EXP. JOINT NEAR FACE OF CURB AT PIER 16.



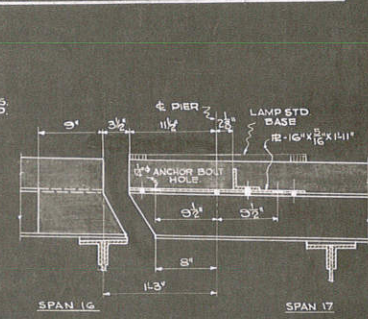
SECTION THRU SDWK. EXP. JOINT NEAR FACE OF CURB AT PIER 17.



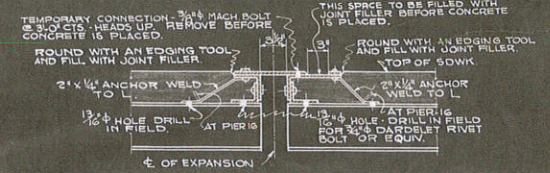
SECTION THRU SDWK. EXP. JOINT AT PIER 15.
SECTION TAKEN BETWEEN STRINGERS.



SECTION THRU RDWY. EXP. JOINT AT PIERS 16 & 17.



SECTION X23-X23.



SECTION THRU SDWK. EXP. JOINT AT PIER 17.

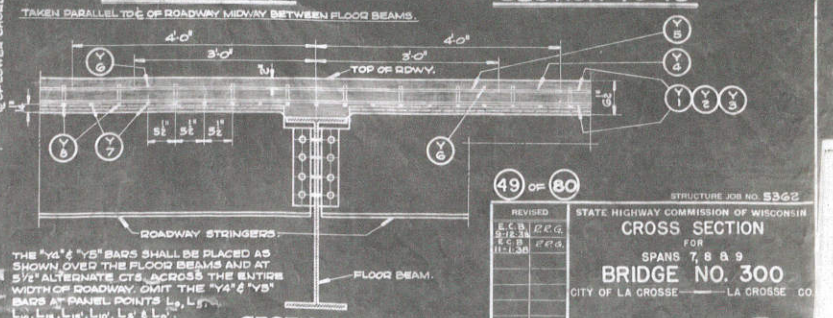
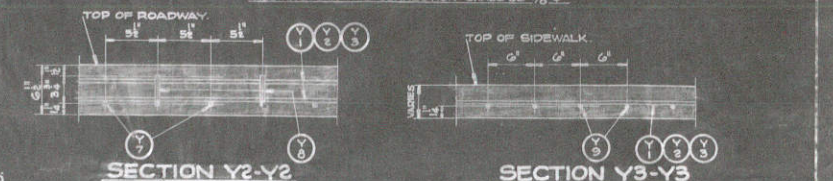
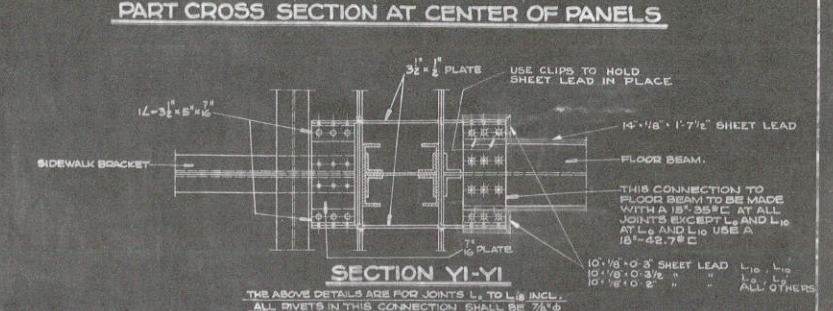
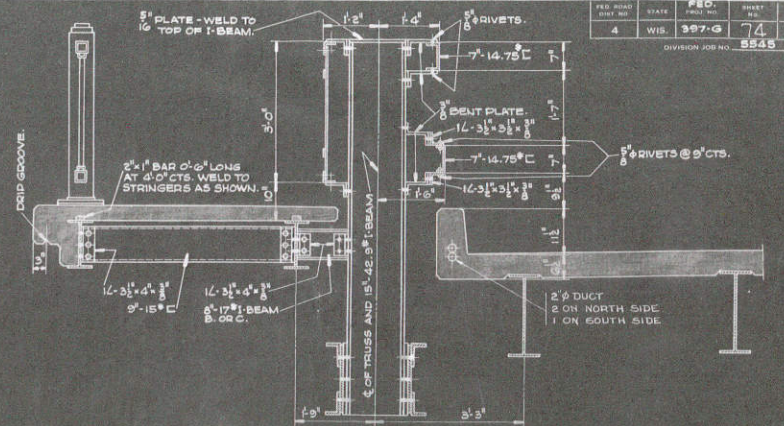
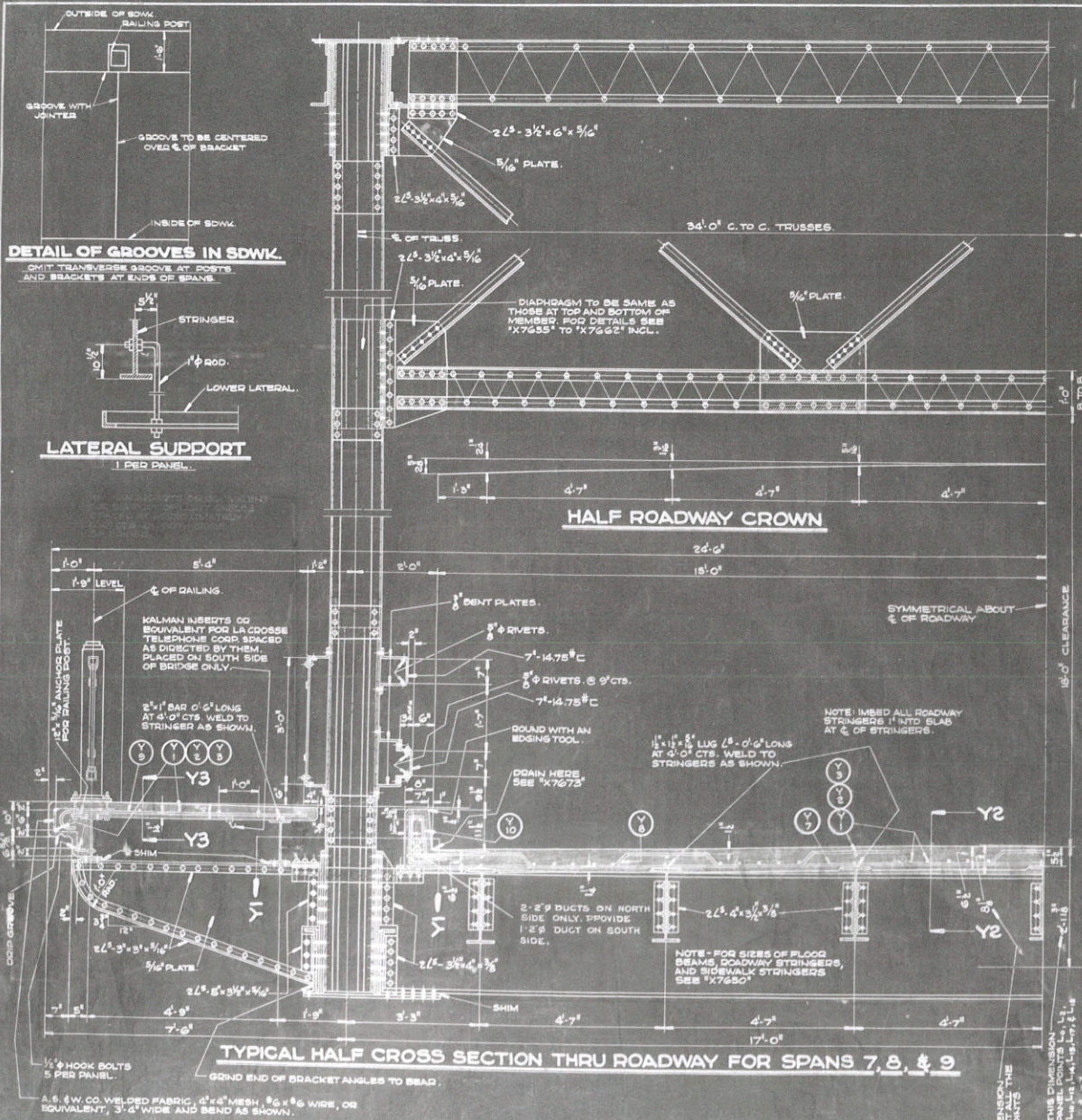
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REVISED	BY	DATE

STRUCTURE JOB NO. 5362
STATE HIGHWAY COMMISSION OF WISCONSIN
EXPANSION DETAILS
AT PIERS 15, 16 & 17
FOR
BRIDGE NO. 300
CITY OF LA CROSSE — LA CROSSE CO.

APPROVED: *G. J. ...*
DESIGNED BY: *G. J. ...*
CHECKED BY: *G. J. ...*

REV. NO.	DATE	BY	CHKD.	NO. OF SHEETS
4	WIS. 397-G	74	119	



49 OF 80

STRUCTURE JOB NO. 5362

STATE HIGHWAY COMMISSION OF WISCONSIN

CROSS SECTION

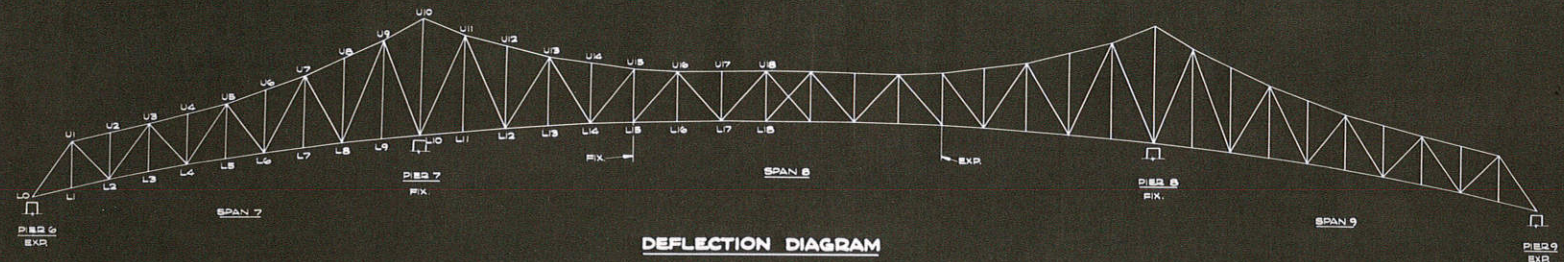
FOR

BRIDGE NO. 300

CITY OF LA CROSSE — LA CROSSE, WIS.

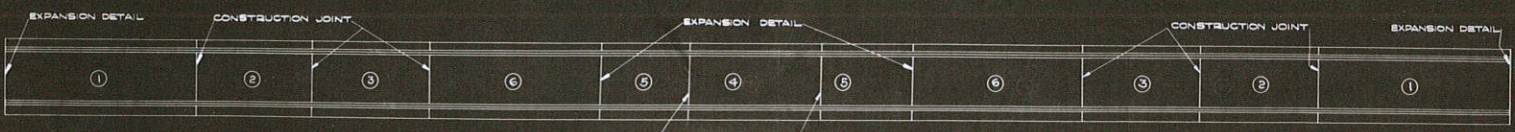
DESIGNED BY: *G. W. F. Smith*

APPROVED BY: *E. A. Rostrom*



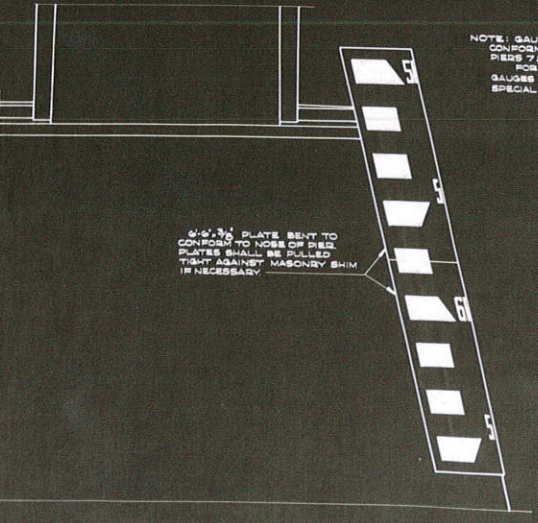
DEFLECTION TABLE

JOINT	DEAD LOAD DEFLECTION AS FOR A LEVEL SURFACE		HORIZONTAL DEFLECTION	
	VERTICAL	HORIZONTAL	VERTICAL	HORIZONTAL
L0	0	0	+0.02	+1/16
L1	+1.35	+1/8	+0.83	+1/16
L2	+0.24	0	+7.02	+1/16
L3	-0.60	-1/16	+2.79	+1/16
L4	-3.29	-3/16	+2.57	+1/16
L5	-6.09	-7/16	+3.76	+3/16
L6	-5.43	-9/16	+2.93	+1/2
L7	-4.71	-1/2	+3.79	+3/8
L8	-4.41	-7/16	+2.64	+1/2
L9	-2.07	-3/16	+1.35	+1/8
L10	0	0	0	0
L11	+1.87	+1/16	-1.48	-1/8
L12	+2.34	+2/16	-2.97	-1/8
L13	+3.29	+3/16	-2.48	-1/8
L14	+5.09	+5/16	-5.93	-3/16
L15	+6.99	+6/16	-5.93	-3/16
L16	+7.87	+7/16	+1.20	+1/8
L17	+8.33	+8/16	+2.41	+1/4
L18	+8.84	+8/16	+3.97	+3/8
U1	+0.64	+1/16	+7.00	+1/16
U2	+0.24	0	+6.93	+1/16
U3	-0.31	-1/16	+2.82	+1/16
U4	-3.10	-7/16	+7.26	+3/4
U5	-4.95	-1/2	+7.64	+3/4
U6	-5.45	-9/16	+8.62	+7/8
U7	-6.00	-5/8	+9.67	+15/16
U8	-4.41	-7/16	+1.15	+1/16
U9	-4.00	-7/16	+1.32	+1/16
U10	+3.29	+3/16	+1.82	+1/8
U11	+1.90	+1/16	+1.92	+1/8
U12	+2.34	+2/16	+1.82	+1/8
U13	+2.86	+3/16	+1.35	+1/8
U14	+5.08	+5/16	+1.02	+1/16
U15	+6.80	+6/16	+0.91	+7/16
U16	+7.87	+7/16	+0.80	+3/16
U17	+8.33	+8/16	+0.77	+1/16
U18	+8.78	+8/16	+0.48	+3/16



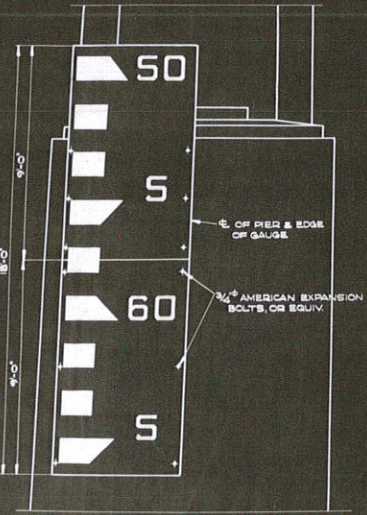
POURING DIAGRAM
 NUMBERS INDICATE ORDER OF POURING

NOTE: ALL DEFLECTIONS ARE GIVEN IN INCHES. DOWNWARD DEFLECTION IS SHOWN POSITIVE. HORIZONTAL DEFLECTION TO THE RIGHT IS SHOWN POSITIVE FOR LEFT HALF. HORIZONTAL DEFLECTIONS IN SUSPENDED SPAN ARE REFERRED TO L15. ALL TRUSS MEMBERS OF SPANS 7, 8 & 9 SHALL BE SHORTENED OR LENGTHENED BY THE AMOUNT NECESSARY TO MAKE THE LENGTH OF THE MEMBERS UNDER FULL DEAD LOAD STRESS EQUAL TO THE GEOMETRICAL LENGTH COMPUTED FROM THE TRUSS DIMENSIONS AND GRADE ELEVATIONS.

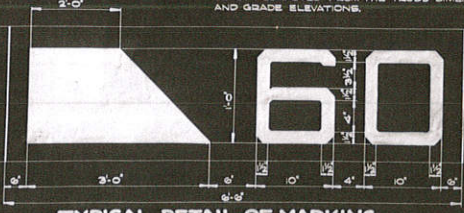


SIDE VIEW OF VERTICAL CLEARANCE GAUGE

NOTE: GAUGES TO BE BUILT TO CONFORM TO THE NOSES OF PIER 7 & 8. SEE 1.705 FOR VERTICAL POSITION OF GAUGES AND FOR PAINTING SEE SPECIAL PROVISIONS.

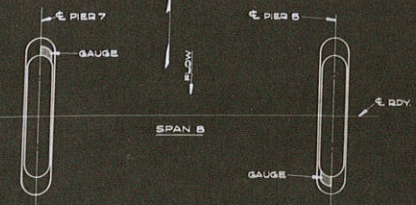


END VIEW OF VERTICAL CLEARANCE GAUGE



TYPICAL DETAIL OF MARKING

THE GRADUATIONS ON THE GAUGE SHALL BE IN TRUE VERTICAL PROJECTION. GRADUATIONS AND NUMERALS TO BE BLACK AND THE BALANCE OF THE EXPOSED GAUGE SURFACE TO BE WHITE.



LOCATION OF GAUGES

ESTIMATED QUANTITIES - SPANS 7, 8 & 9

BID ITEM	UNIT	QUANTITY
CONCRETE MASONRY	CY	850.1
BAR STEEL REINFORCEMENT	LB.	139,700
STEEL FABRIC REINFORCEMENT	SY	721
STRUCTURAL STEEL (CARBON)	LB.	1,663,000
STRUCTURAL STEEL (SILICON)	LB.	508,800
CAST STEEL	LB.	38,620
STEEL FORGINGS	LB.	38,620
SHIELD LEAD	LB.	9,070
CAST STEEL	LB.	886,218
ZINC PLATES	LB.	108,210
PIPE RAILING (WITH PIPE POSTS)	LF	138
ORNAMENTAL RAILING	LF	19,837.8
FLOOR DRAINS (COMPLETE)	NF	36
GRATING	SF	67

NON-BID ITEMS

EXPANSION JOINT FILLER SIZE 1'

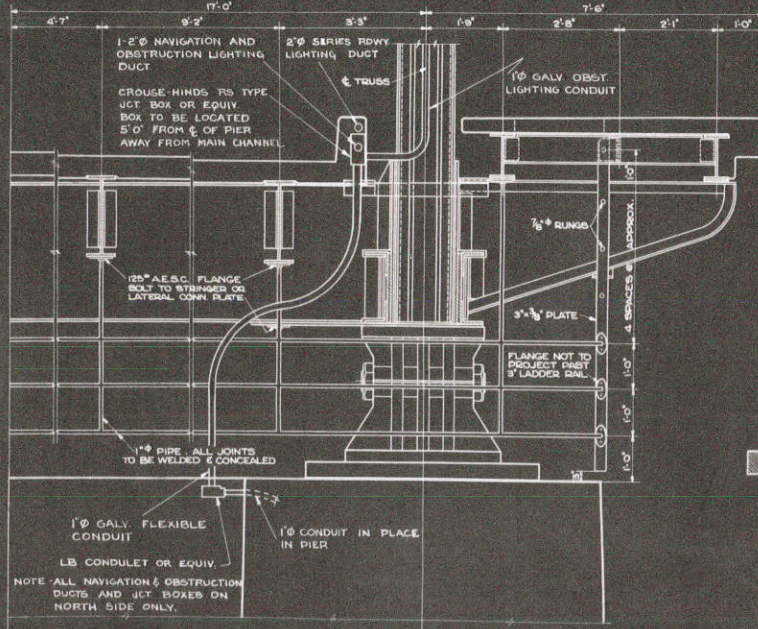
80# 80

REVISIONS: 1. 2.28.37
 2. 2.28.37
 3. 2.28.37

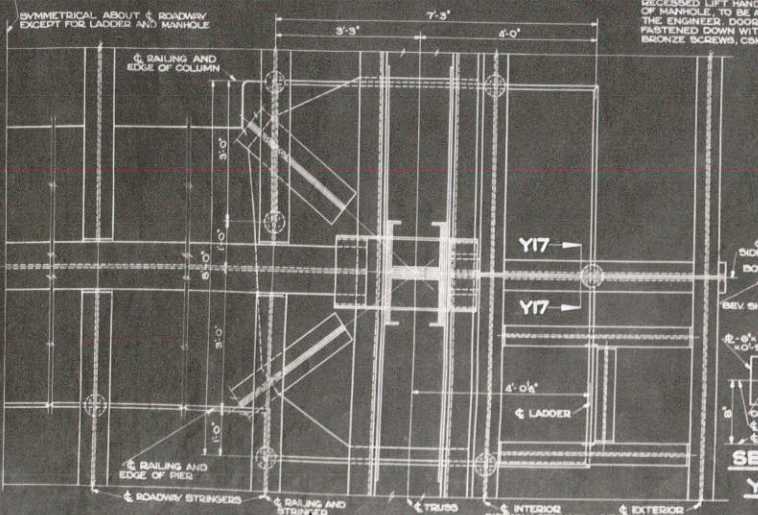
STATE HIGHWAY COMMISSION OF WISCONSIN
 FOR
DETAILS
BRIDGE NO. 300
 CITY OF LACROSSE - LA CROSSE, CO.

APPROVED: [Signature]
 CHECKED: [Signature]
 DRAWN: R.E.A.
 SCALE: C.F.E.

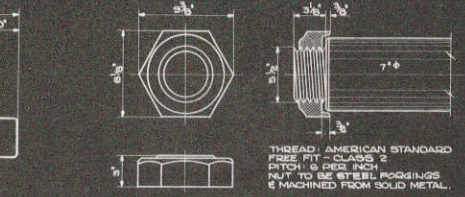
FED. PROJ. NO.	STATE	FED. DIST. NO.	SHEET NO.	TOTAL SHEETS
4	WIS.	397-G	86	119
DRAWING NO. 5545				



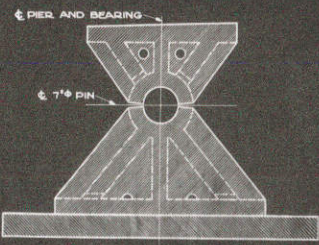
PART ELEVATION OF PIPE RAILING AT PIERS 7 AND 8



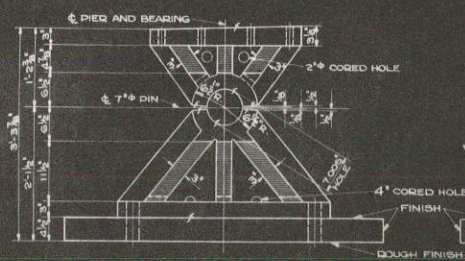
PART PLAN OF PIPE RAILING AT PIERS 7 AND 8
ROADWAY AND SIDEWALK SLAS NOT SHOWN



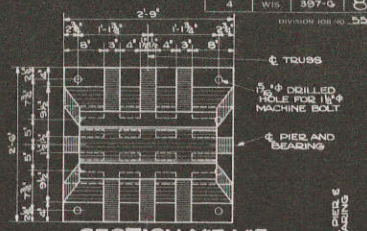
DETAIL OF PIN NUT
FOR BEARING B 28 - 6 REQUIRED



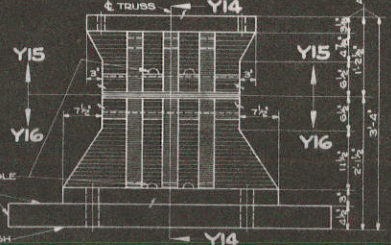
SECTION Y14-Y14



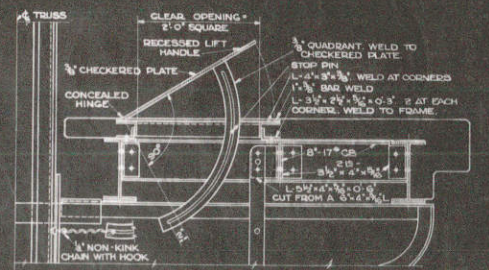
END VIEW



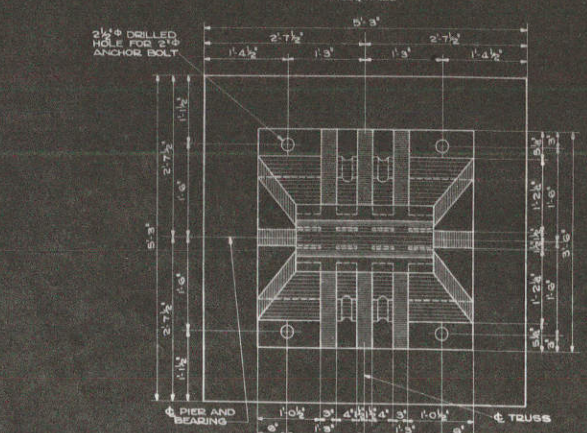
SECTION Y15-Y15



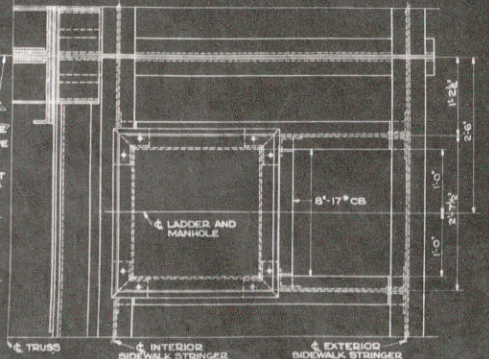
BEARING B28
4 REQUIRED



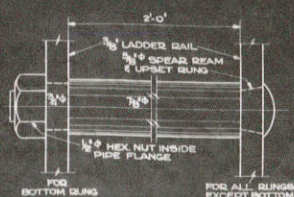
SECTION THRU MANHOLE



SECTION Y16-Y16



PLAN OF MANHOLE
CONCRETE, NOT SHOWN



DETAIL OF RUNG

61 OF 80

REVISED
E.C.B. 02.02
5-13-38

STRUCTURE JOB NO. 5362
STATE HIGHWAY COMMISSION OF WISCONSIN
BEARING AND LADDER DETAILS
AT PIERS 7 & 8
FOR
BRIDGE NO. 300
CITY OF LA CROSSE — LA CROSSE, CO

CONNECTED
APPROVED
E.C.B. 02.02
5-13-38

X7664