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- SHEET NO. 3K-3N MISCELLANEOUS QUANTITIES
- SHEET NO. 4 RIGHT OF WAY PLAT
- SHEET NO. 5-1B PLAN AND PROFILE STA. 13+30 TO STA. 67+83.5
- SHEET NO. 19-105 DRAINAGE STRUCTURES AND STANDARD DETAILS
- SHEET NO. 100-119 CROSS SECTIONS

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WISC	397-G	1	119

DIVISION JOB NO. 5545

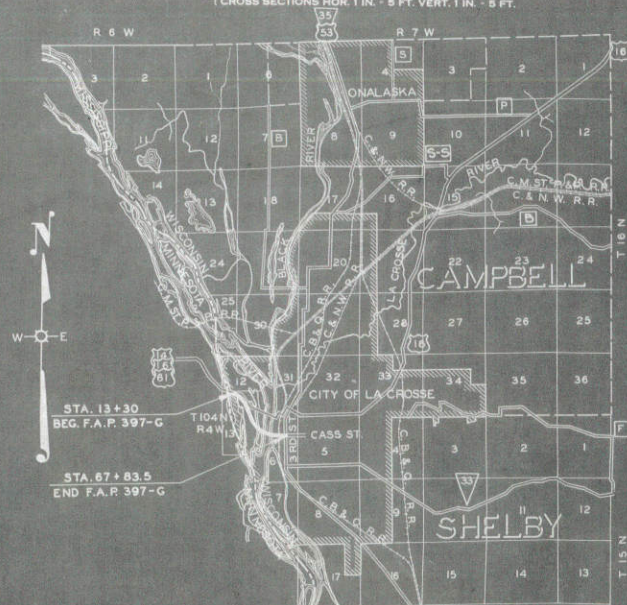
STATE OF WISCONSIN  
**WISCONSIN HIGHWAY COMMISSION**

PLAN AND PROFILE OF PROPOSED  
**SPARTA-LA CROSSE ROAD**  
**MISSISSIPPI RIVER BRIDGE**  
**LA CROSSE COUNTY**

397 G FEDERAL AID PROJECT 397 G

BEGINNING AT A POINT NEAR THE CENTER OF LOT 6, SEC. 13, T104 N, R 4 W, 5 TH P.M.  
 AND EXTENDING SOUTHEASTERLY TO A POINT NEAR THE INTERSECTION OF THIRD  
 AND CASS STREETS IN THE CITY OF LA CROSSE

SCALES: PLAN 1 IN. = 100 FT.  
 PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.  
 CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



STA. 13+30  
 BEG. F.A.P. 397-G

STA. 67+83.5  
 END F.A.P. 397-G

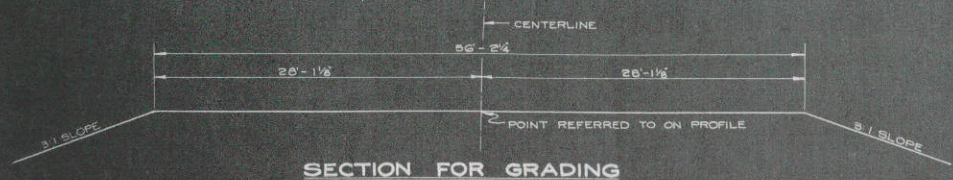
CONVENTIONAL SIGNS

- |                           |       |                               |            |
|---------------------------|-------|-------------------------------|------------|
| STATE LINE                | ----- | CULVERTS IN PLACE             | -----      |
| COUNTY LINE               | ----- | CULVERTS REQUIRED             | -----      |
| TOWNSHIP OR RANGE LINE    | ----- | DROP INLET                    | -----      |
| SECTION LINE              | ----- | POWER POLE                    | -----      |
| NEW RIGHT OF WAY LINE     | ----- | TELEPHONE OR TELEGRAPH POLE   | -----      |
| PRESENT RIGHT OF WAY LINE | ----- | RIGHT OF WAY MARKERS          | -----      |
| WIRE FENCE                | ----- | REFERENCE STAKE FOR HUBS ONLY | -----      |
| WOVEN                     | ----- | MARSH                         | -----      |
| BARBED                    | ----- | HEDGE                         | -----      |
| LOT LINE                  | ----- | TREES                         | -----      |
| CORPORATE OR CITY LIMITS  | ----- | GROUND ELEVATION              | DATUM LINE |
| PROPERTY LINE             | ----- | GRADE ELEVATION               | DATUM LINE |
| TRAVELED WAY OR P.E.      | ----- |                               |            |
| RAILROADS                 | ----- |                               |            |
| BASE OR SURVEY LINE       | ----- |                               |            |

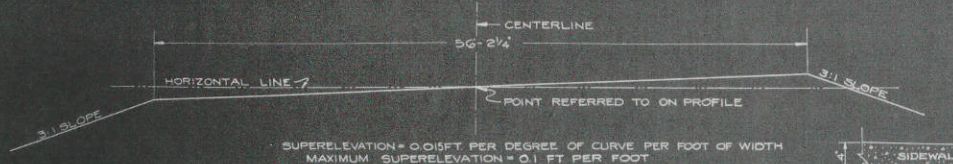
LAYOUT  
 SCALE 0 MI 1 MI 2 MI

TOTAL NET LENGTH OF CENTERLINE	1.069	MI.
WEST APPROACH	0.469	MI.
BRIDGE 500	0.479	MI.
EAST APPROACH	0.120	MI.

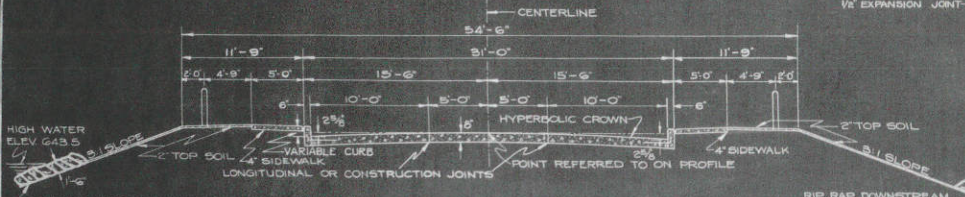
WISCONSIN HIGHWAY COMMISSION MADISON, WIS.		U.S. DEPARTMENT OF AGRICULTURE BUREAU OF PUBLIC ROADS	
Surveyor: J. R. A.	Note Book: 8050-8038	RECOMMENDED FOR APPROVAL:	
Dir. Computer: J. R. A.	M. O. Checker: W. F. M.	DISTRICT ENGINEER	
Div. Checker: R. G. H.	Correct: 6-1-31	CHIEF ENGINEER	
CORRECT:	<i>T. M. Reynolds</i>	APPROVED:	
	DIVISION ENGINEER	DIRECTOR	
RECOMMENDED FOR APPROVAL:	<i>Bookman</i>	DATE: 6/2/37	
	CONSULTING ENGINEER	DATE:	
APPROVED:	<i>E. L. Kuehn</i>	STATE HIGHWAY ENGINEER	
	DIVISION ENGINEER		
CHECKED IN THE FIELD BY: <i>W. C. Solank</i>			



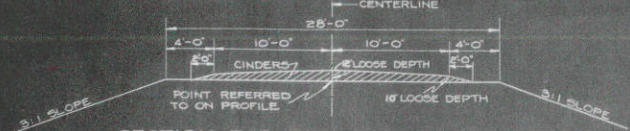
**SECTION FOR GRADING**



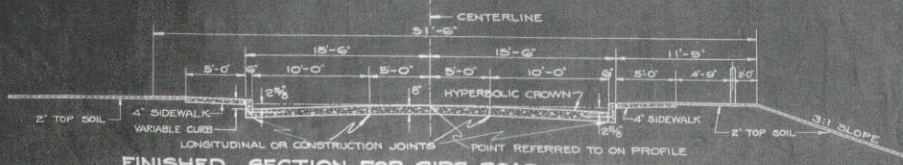
**TYPICAL SECTION ON CURVES**



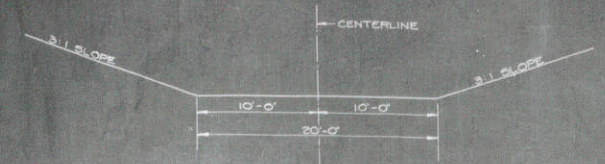
**TYPICAL FINISHED SECTION**



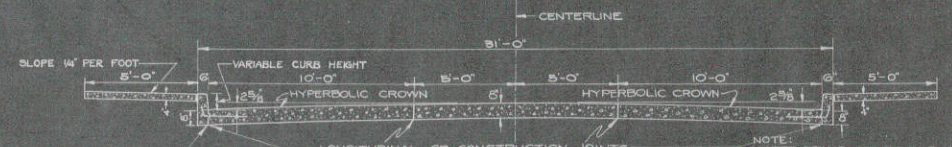
**SECTION FOR SIDE ROAD RT. STA. 25+50 WITH CINDER SURFACING**



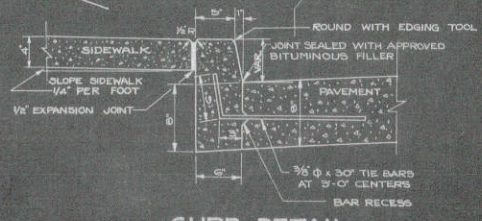
**FINISHED SECTION FOR SIDE ROAD LT. STA. 19+50**



**CHANNEL CHANGE SECTION (LAGOON SLOUGH) STA. 40+23.3**

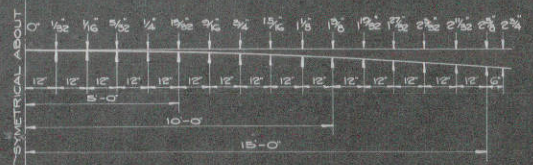


**TYPICAL SECTION FOR 8" CONCRETE PAVEMENT**

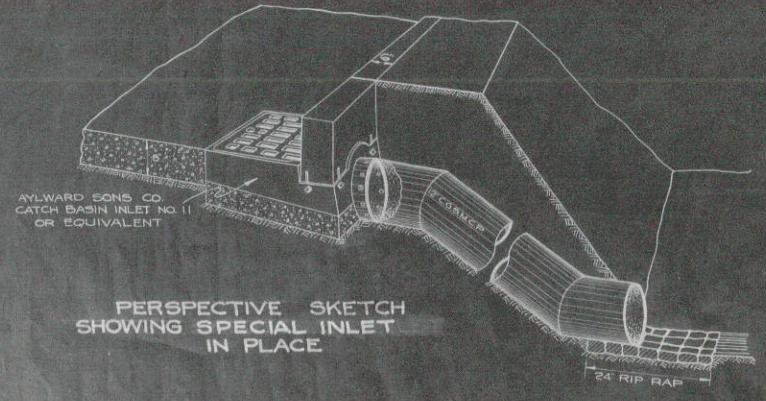


**CURB DETAIL**

5" CURB - 0.0085 CY CONC. PER LIN. FT.  
 3" CURB - 0.0251 CY CONC. PER LIN. FT.  
 1 1/2" CURB - 0.0285 CY CONC. PER LIN. FT.



**HYPERBOLIC CROWN FOR PAVEMENT SURFACE**



**PERSPECTIVE SKETCH SHOWING SPECIAL INLET IN PLACE**

**TYPICAL CROSS SECTION FOR 30' CONCRETE PAVEMENT 50' ROADBED**

- APPROVED APPLICABLE DETAIL DRAWINGS ARE:
- LONGITUDINAL JOINT DETAILS 5D-11
  - TRANSVERSE EXPANSION JOINTS 5D-23
  - PAVEMENT REINFORCEMENT 5D-25.1
  - CONCRETE HEADER BLOCK 5D-25.3
  - WIRE CABLE GUARD FENCE 5D-18
  - MARKER POOTS 5D-12.1
  - CONSTRUCTION BARRICADE 5D-16



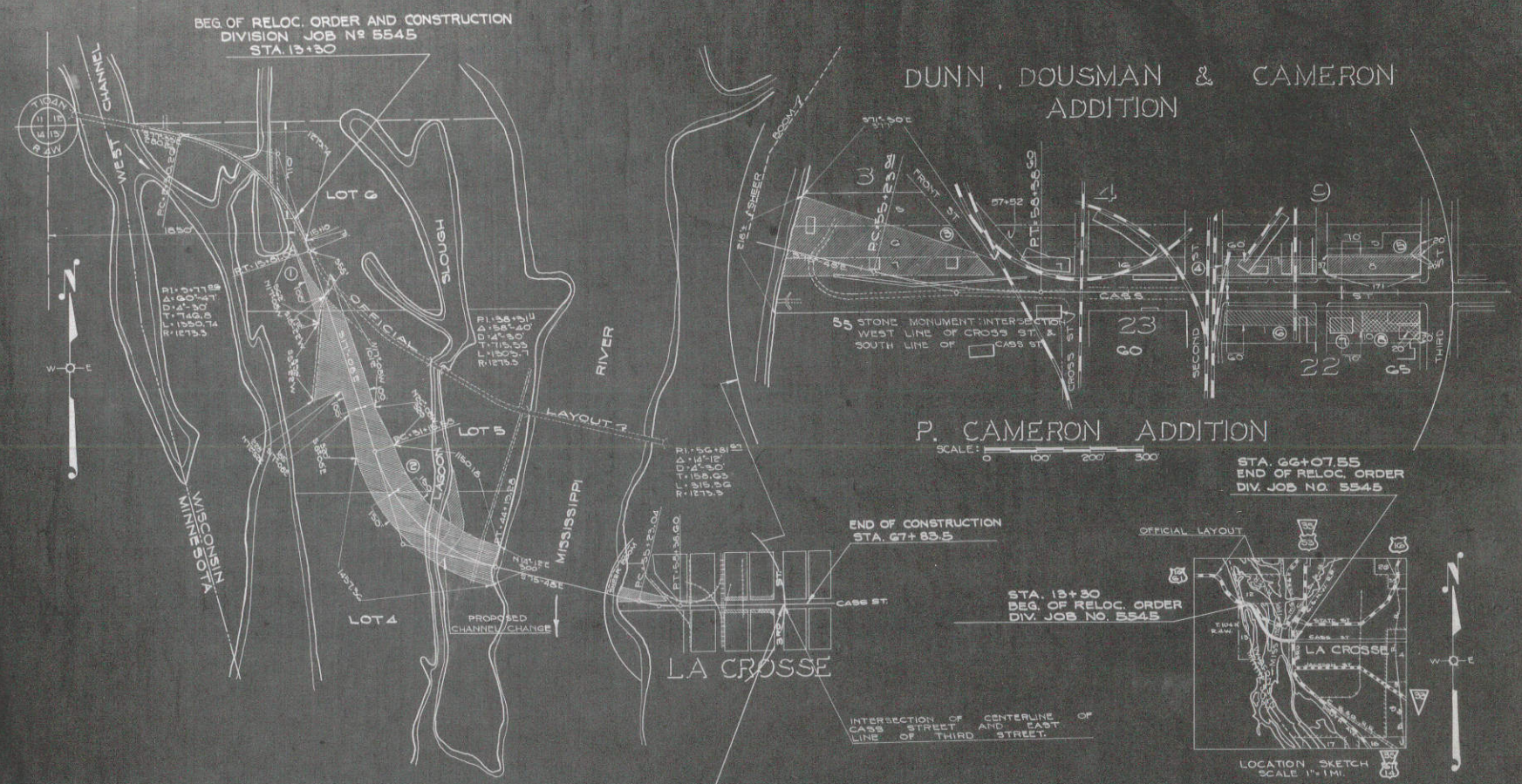
# DETAIL SUMMARY SHEET OF MISCELLANEOUS QUANTITIES

(GRADING - WEST APPROACH)

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	WV	397-G	3K	119

CLEARING			GRUBBING			RIP RAP			GUARD FENCE				CONCRETE BOX CULVERTS							PIPE CULVERTS															
MILE NO.	STATION TO STATION	ACRES	MILE NO.	STATION TO STATION	SIDE R. OR L.	ACRES	MILE NO.	STATION TO STATION	SIDE R. OR L.	CU. YDS.	MILE NO.	STATION TO STATION	SIDE R. OR L.	TYPE & LIN. FT.	ANCHOR AGES	MILE NO.	STATION	SPAN FT.	HEIGHT FT.	LENGTH FT.	CONC. CU. YDS.	STEEL LBS.	EXCAV. CU. YDS.	WATERPROOF. SQ. FT.	MARKER POSTS	MILE NO.	STATION	DIAMETER INCHES	LENGTH LIN. FT.	TYPE	6 SDY.	P.E. & S.H. L. & R.	RIP ROAD	MARKER	
1	13+30-16+00	5.83	1	13+30-16+00		1.24	1	20+15-38+03	LT	4096																1	25+35	24"	54	CGSM			5R		
								26+00-38+03	RT	3185																									
								19+50 (LAGOON BR. CONN.)		183																									
								38+09 (E. SIDE W. ABUT.)		520																									

FINISHING ROADWAY			CINDER SURFACING		
MILE NO.	STATION TO STATION	STA.	MILE NO.	STATION TO STATION	CU. YDS.
1	13+30-38+03	24.8	1	19+50 (NO. PETTIBONE CONN.)	44.0
				19+50 (SO. TEMPORARY CONN.)	100.0
1	25+50 (SO. PETTIBONE CONN.)	2.2	1	19+50-25+50	240.5
				25+50 (SO. PETTIBONE CONN.)	200.5

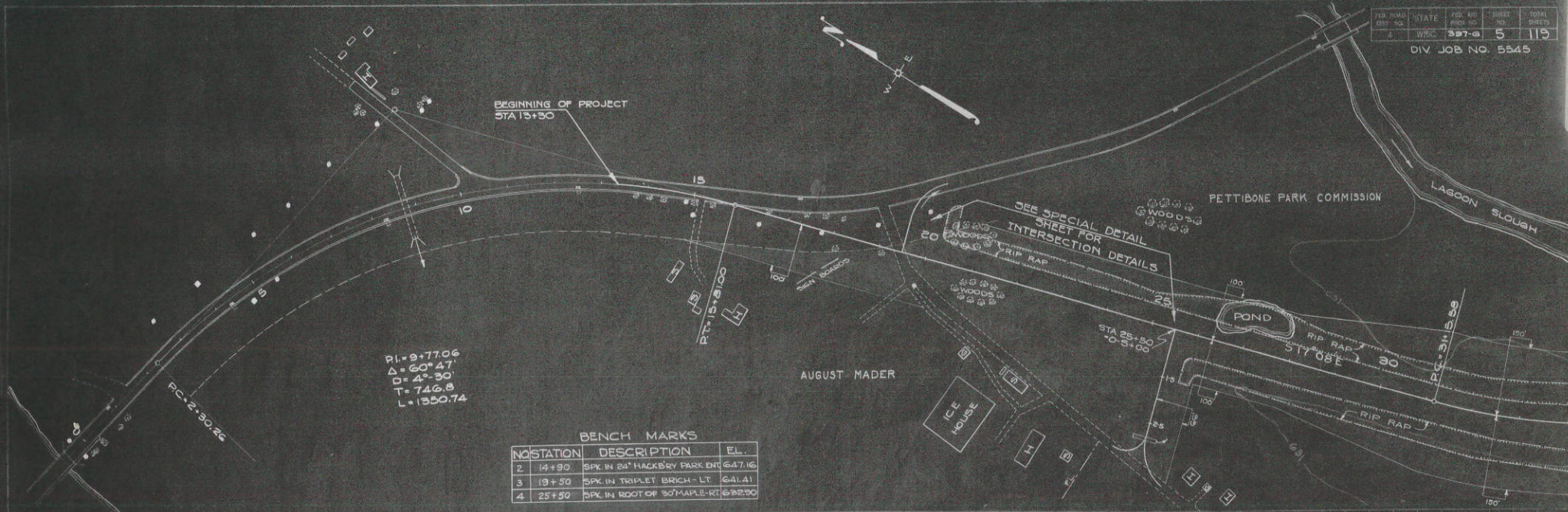


SCHEDULE OF R/W REQUIRED

PAR	OWNER	DESCRIPTION	ACRES	FENCINGS - R.O.W.
1	AUGUST MADER	LOTS 5, 6, SEC. 13, T104N, R4W	0.509	
2	PETTIBONE PARK COMM.	LOTS 4, 5, 6, SEC. 13, T104N, R4W	15.011	
3	LA CROSSE WOOL CO.	LOTS 5, 6, 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.773	
4	C.B. & G.R.R.	LOT 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.106	
5	EARL F. SCHULTZ	LOT 6, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.	0.149	
6	SEGELKE KOHLHAUS	LOT 1, BLK. 22, P. CAMERON ADD.	0.106	
7	KATIE HENGEL	LOT 10, BLK. 22, P. CAMERON ADD.	0.055	
8	LIZZIE KANARD	LOT 10, BLK. 22, P. CAMERON ADD.	0.050	
3A	ADE WILLIAMS (LEASEE)	FR. LOTS 5, 6; BLK. 3, DUNN, DOUSMAN & CAMERON ADD.		
4A	WHITEBREAST COAL CO. (LEASEE)	LOT 7, BLK. 3, DUNN, DOUSMAN & CAMERON ADD.		

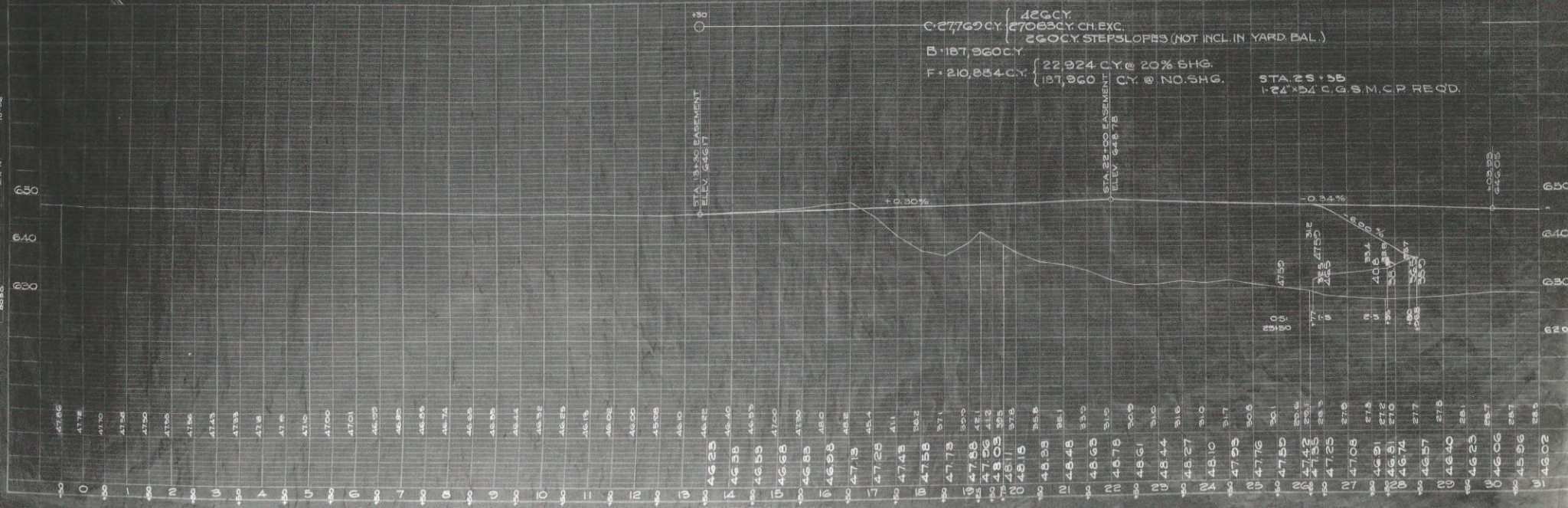
PLAT OF RIGHT OF WAY REQUIRED  
DIVISION JOB NO. 5545  
LA CRESCENT ~ LA CROSSE ROAD  
U.S.H.NO. 10 & 61 LA CROSSE COUNTY  
SCALE  
1" = 1000'

REVISED APR. 23, 1937  
REVISED: NOV. 10, 1936.



BENCH MARKS

NO	STATION	DESCRIPTION	ELEV.
2	14+90	SPK. IN 24" HACKBERRY PARK ENT.	647.16
3	19+50	SPK. IN TRIPLET BRICH - LT	641.41
4	25+50	SPK. IN ROOT OF 30" MAPLE - RT	632.30

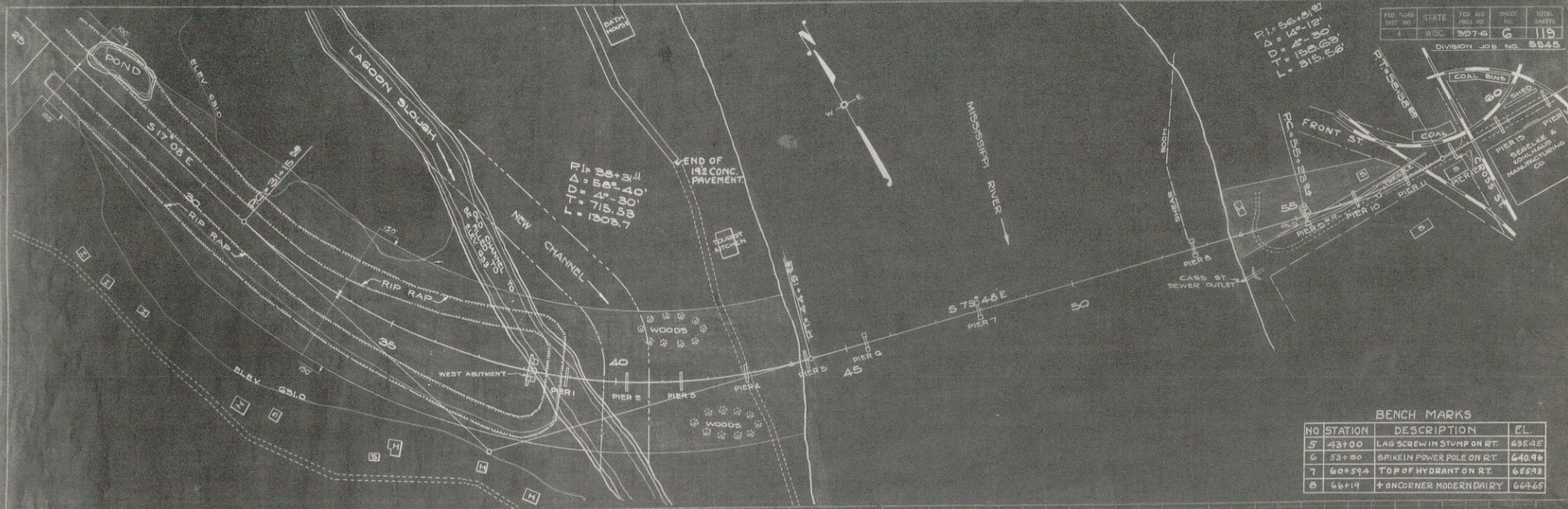


11-11-11  
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 11-11-11

DATE: 11/14/54  
 DRAWN BY: J. A. ...  
 CHECKED BY: ...  
 PROJECT NO.: ...  
 SHEET NO.: ...



FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	PROJECT NO.	TOTAL SHEETS
4	WISC.	307-G	6	119

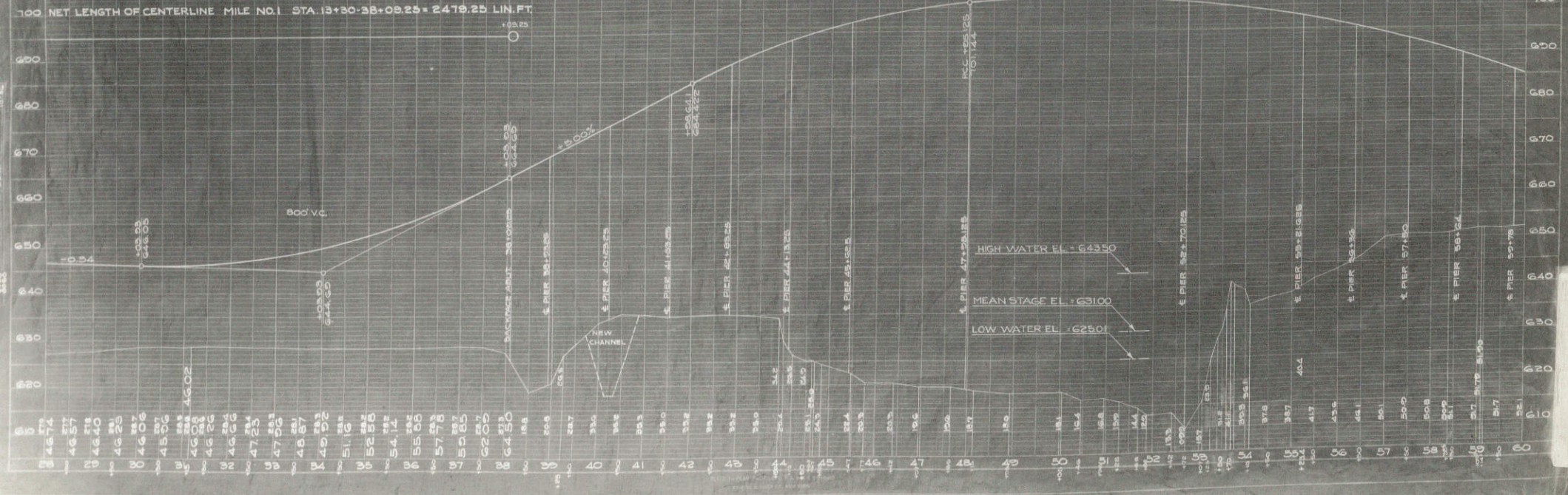
DIVISION JOB NO. 5545

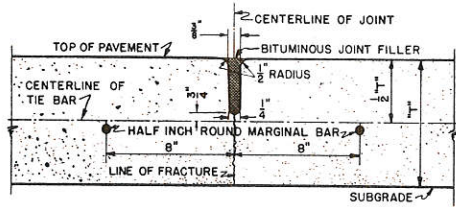
$P1 = 56+51.87$   
 $L1 = 46+12.87$   
 $L2 = 150+60$   
 $L3 = 150+56$

$P1 = 38+31.4$   
 $\Delta = 58^{\circ} 40'$   
 $D = 4^{\circ} 30'$   
 $T = 715.53$   
 $L = 1303.7$

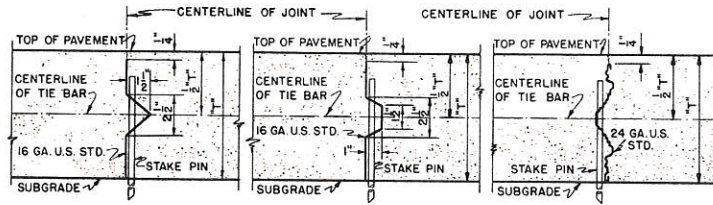
**BENCH MARKS**

NO.	STATION	DESCRIPTION	EL.
5	43+00	LAG SCREW IN STUMP ON RT.	635.95
6	53+80	SPIKE IN POWER POLE ON RT.	640.96
7	60+59.4	TOP OF HYDRANT ON RT.	655.83
8	66+19	+ ON CORNER MODERN DAIRY	667.65



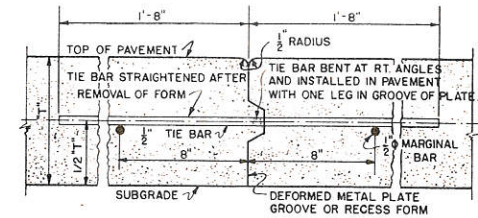


POURED JOINT

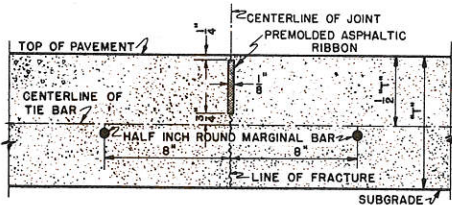


TIE AND MARGINAL BARS REQD.-NOT SHOWN

ALTERNATE-TYPE "A"



LONGITUDINAL CONSTRUCTION JOINT



PREMOLDED JOINT

ALTERNATE DUMMY JOINTS



TIE BAR LAYOUTS, OTHER THAN SHOWN, MUST BE SUBMITTED AND APPROVED BY ENGINEER PRIOR TO USE AND SHALL BE SUBJECT TO THE FOLLOWING:  
 MAXIMUM SPACING = 3'-0" CENTER TO CENTER FOR 1/2" RD. BAR.  
 MAXIMUM SPACING = 4'-0" CENTER TO CENTER FOR 1/2" SQ. BAR.  
 MINIMUM AREA STEEL (SQ. IN. PER LIN. FT. OF JOINT) = 0.063 FOR SQ. BAR  
 MINIMUM AREA STEEL (SQ. IN. PER LIN. FT. OF JOINT) = 0.065 FOR RD. BAR

TYPES OF TIE BARS

CONSTRUCTION NOTES

CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DIMENSION "T" INDICATES PAVEMENT DEPTH ALONG LINE OF LONGITUDINAL JOINT.

MARGINAL BARS SHALL BE DOUBLE CROSS WIRED TO TIE BARS, I.E. WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION. USE NO. 16 GA. LOOP WIRE TIE.

JOINT ASSEMBLY COMPLETE WITH TIE BARS AND MARGINAL BARS SHALL BE SECURELY PLACED AND HELD IN THE REQUIRED POSITION THROUGHOUT CONCRETE PLACING OPERATIONS.

JOINTS SHALL BE CLEAN CUT AT ALL INTERSECTIONS WITH OTHER JOINTS.

LONGITUDINAL MARGINAL BARS AND DEFORMED METAL PLATES SHALL NOT EXTEND THRU ANY TRANSVERSE JOINT.

SHIMS TO SUPPORT THE JOINT ASSEMBLY TO THE TRUE GRADE SHALL BE PROVIDED WHERE SUBGRADE CONDITIONS REQUIRE.

BITUMINOUS JOINT FILLER FOR SEALING JOINTS SHALL CONFORM TO THE REQUIREMENTS SPECIFIED FOR TRANSVERSE JOINTS.

FOLLOWING THE INSTALLATION OF THE JOINT OR TEMPORARY FILLER STRIPS FOR CREATING A WEAKENED PLANE, THE FINISHING MACHINE SHALL SCREED OVER THE SECTION AT LEAST ONE TIME.

WEAKENED PLANE TYPE JOINTS SHALL BE FORMED BY A MECHANICAL METHOD APPROVED BY THE ENGINEER.

JOINTS SHALL NOT DEVIATE MORE THAN 5 DEGREES FROM THE VERTICAL NOR SHALL THE AXIS OF THE JOINT DEVIATE MORE THAN 1/2" EITHER WAY FROM A STRAIGHT LINE AT ANY POINT.

SPACER OR MARGINAL BARS ARE NOT REQUIRED AT LONGITUDINAL JOINTS ALONG THICKENED EDGES OF PAVEMENT SLAB.

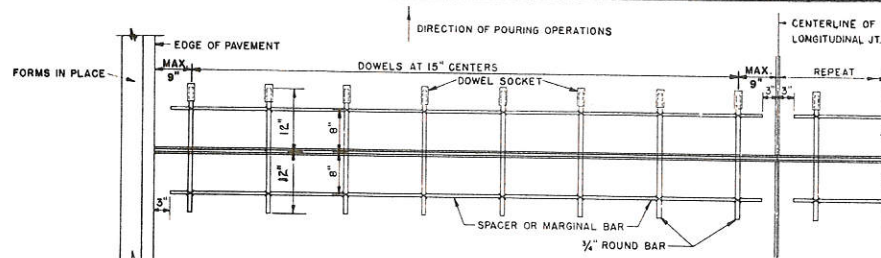
DETAILS OTHER THAN THOSE SHOWN TO EFFECT THE SEPARATION OF SLABS AND PROVIDE FOR TIE & LOAD TRANSFER ACROSS THE JOINT, COMPARABLE TO THESE DETAILS, MAY BE SUBMITTED TO THE COMMISSION FOR CONSIDERATION & APPROVAL PRIOR TO THEIR USE ON ANY PROJECT.

LONGITUDINAL JOINT DETAILS  
 FOR  
 CONCRETE PAVEMENT

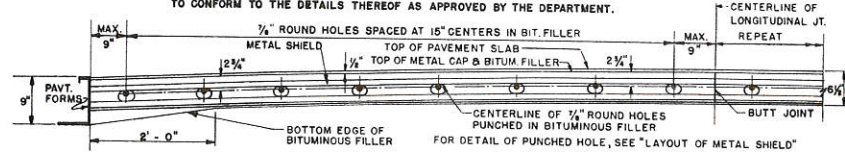
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:  
*Frank C. Crane* DESIGN ENGINEER  
 APPROVED: *A. M. Hub* CONSTRUCTION ENGINEER  
 DRAWN: NFE DATE: 3/10/57 CHECKED: STATE HIGHWAY ENGINEER

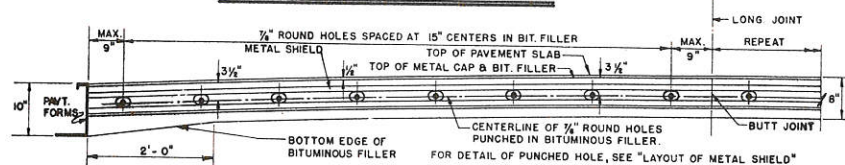




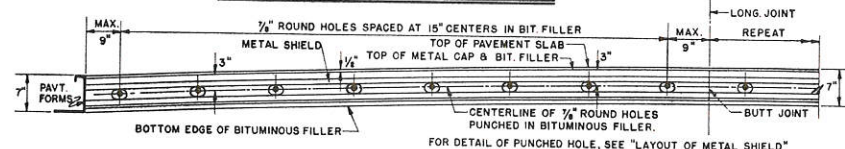
**PLAN OF TRANSVERSE JOINT**  
 NOTE: THE ABOVE DETAILS REFER TO STANDARD DOWEL DESIGN FOR LOAD TRANSFER. INSTALLATIONS OF OTHER TYPES OF LOAD TRANSFER DEVICES ARE TO CONFORM TO THE DETAILS THEREOF AS APPROVED BY THE DEPARTMENT.



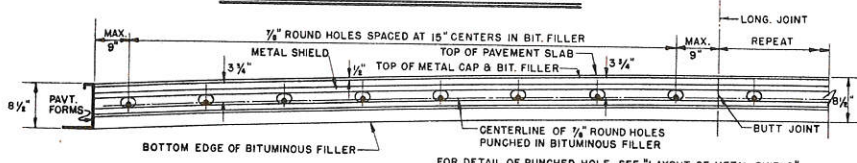
**JOINT LAYOUT FOR "9-6 1/2-9" PAVEMENT**



**JOINT LAYOUT FOR "10-8-10" PAVEMENT**



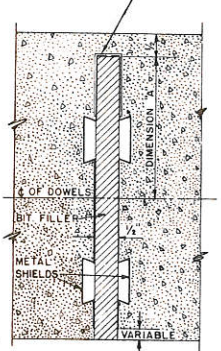
**JOINT LAYOUT FOR UNIFORM 7" PAVEMENT**



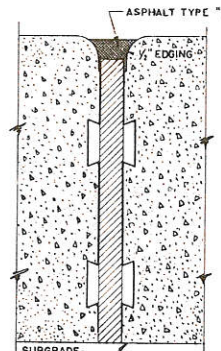
**JOINT LAYOUT FOR UNIFORM 8 1/2" PAVEMENT**

**ELEVATIONS OF TRANSVERSE JOINTS**

AUXILIARY CAP TO BE REMOVED DURING FINISHING OPERATIONS.



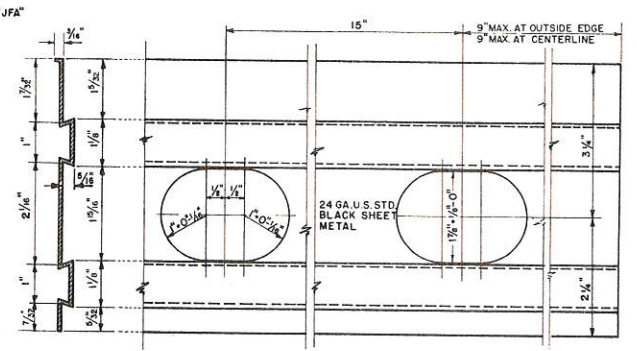
INSTALLATION PRACTICE FINISHING



INSTALLATION PRACTICE COMPLETED

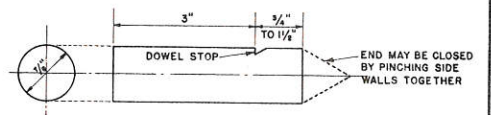
**INSTALLATION PRACTICE**

- DIMENSION "X"**  
 "9-6 1/2-9" PAVEMENT + 2 1/4"  
 "10-8-10" PAVEMENT + 3 1/4"  
 UNIF 7" PAVEMENT + 3"  
 UNIF 8 1/2" PAVEMENT + 3 3/4"

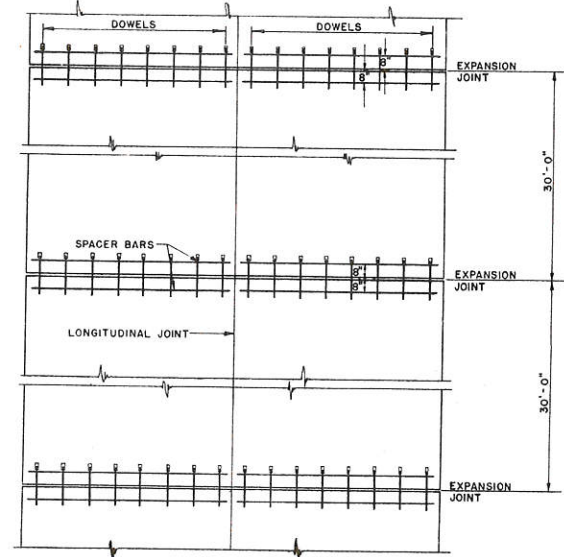


FABRICATE TO CONFORM WITH CROWN OF PAVEMENT

**LAYOUT OF METAL SHIELD**



**DOWEL SOCKET DETAIL**



LONGITUDINAL JOINT TIE BARS NOT SHOWN  
**PLAN SHOWING JOINT LAYOUT**

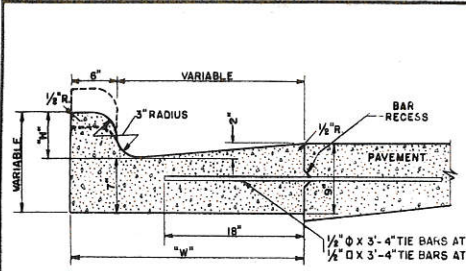
**GENERAL NOTES**

- CONSTRUCTION DETAILS NOT SHOWN HERE SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.
- MARGINAL OR SPACER BARS SHALL BE ONE-HALF INCH ROUND AND DOUBLE CROSSWIRED TO THE DOWELS, I.E. WIRED IN BOTH DIRECTIONS ACROSS THE INTERSECTION. USE NO. 16 GA. LOOP TIE WIRE.
- NO LONGITUDINAL BARS, REINFORCEMENT OR PARTING STRIP SHALL EXTEND THRU ANY TRANSVERSE JOINT.
- THE TERM "BITUMINOUS FILLER" USED HERE REFERS TO AN APPROVED PREMOLDED BITUMINOUS EXPANSION JT. FILLER, 1/2" THICK, SHAPED TO CONFORM TO PERTINENT CROSS SECTION OF PAVEMENT SLAB AND CUT 1/2" BELOW CROWN OF PAVEMENT.
- ALL DOWELS SHALL BE TREATED TO BREAK BOND. LENGTH OF SHIELDS SHALL CONFORM TO MINIMUM LENGTH OF PREMOLDED BITUMINOUS JOINT FILLER.
- ENDS OF SHIELDS TO BE CLOSED BY CRIMPING IN A MANNER SATISFACTORY TO THE ENGINEER.
- METHOD OF FASTENING BUTT JOINTS TOGETHER SHALL MEET WITH THE APPROVAL OF THE ENGINEER.
- MODIFICATIONS OF DESIGN WILL BE ACCEPTABLE SUBJECT TO THE APPROVAL OF THE ENGINEER.
- METAL SHIELDS ARE NOT TO BE HELD TOGETHER BY MEANS OF NAILS OR OTHER DEVICES WHICH WOULD TEND TO HOLD SHIELDS TOGETHER PERMANENTLY AFTER CONCRETE HAS SET.

**STANDARD DESIGN  
 TRANS. EXPANSION JOINT FOR CONC. PAVEMENT  
 PREMOLDED BITUMINOUS TYPE**

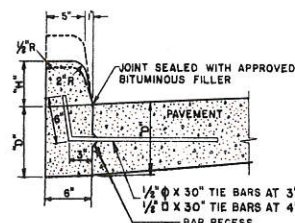
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL:  
*Frank Crane* DESIGN ENGINEER  
*M. J. ...* CONSTRUCTION ENGINEER  
 APPROVED  
 DRAWN BY: *P. C. ...*  
 CHECKER: *P. C. ...*  
 DATE: 3/10/37  
 STATE HIGHWAY ENGINEER



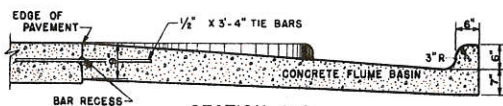
W"	H"	C. Y. CONC. PER LIN. FT.	W"	H"	C. Y. CONC. PER LIN. FT.	W"	H"	C. Y. CONC. PER LIN. FT.
24"	4"	0.0541	30"	4"	0.0664	36"	4"	0.0788
24"	6"	0.0571	30"	6"	0.0694	36"	6"	0.0818
24"	9"	0.0617	30"	9"	0.0740	36"	9"	0.0864

**STANDARD SECTION**

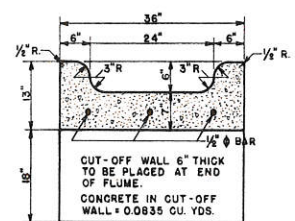


D"	H"	C. Y. CONC. PER LIN. FT.	D"	H"	C. Y. CONC. PER LIN. FT.
7"	4"	0.0161	9"	4"	0.0191
7"	6"	0.0192	9"	6"	0.0223
7"	9"	0.0229	9"	9"	0.0259
8"	4"	0.0176	10"	4"	0.0208
8"	6"	0.0208	10"	6"	0.0238
8"	9"	0.0332	10"	9"	0.0274

**STANDARD SECTION**

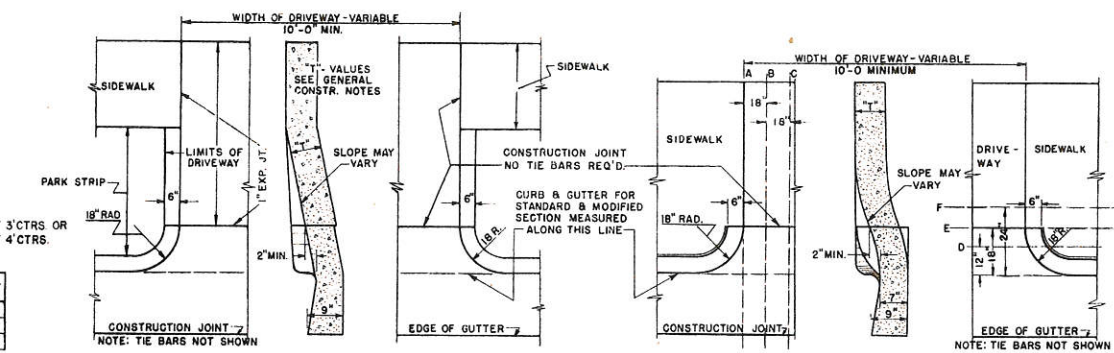


**SECTION A-A**

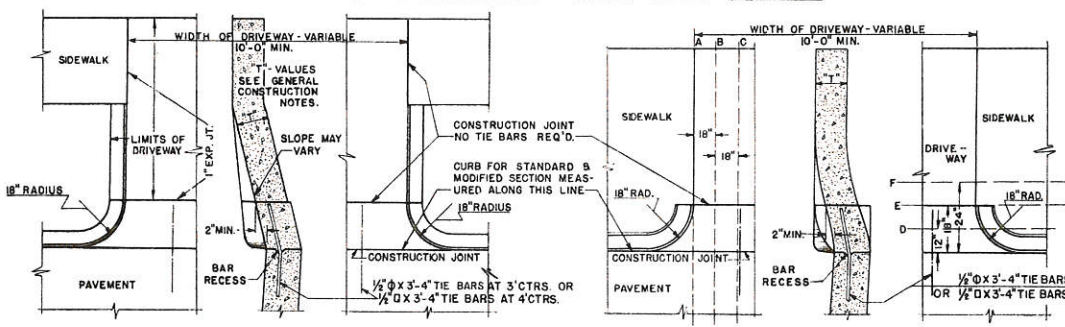


**SECTION B-B**

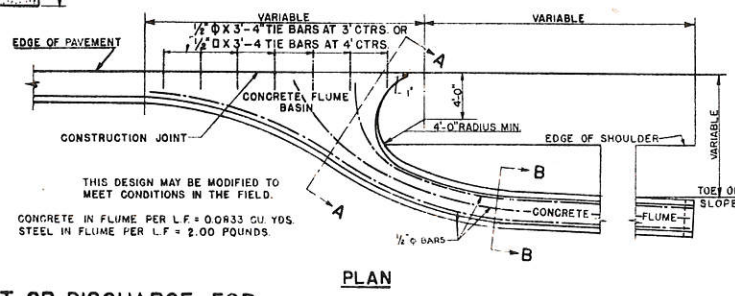
**INLET OR DISCHARGE FOR STANDARD CURB AND GUTTER SURFACE DRAIN**



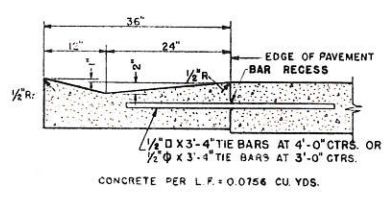
**CONCRETE COMBINATION CURB AND GUTTER**



**CONCRETE CURB**



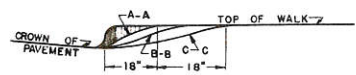
**PLAN**



**STANDARD V-TYPE CONCRETE GUTTER**



**PROFILES PARALLEL TO CENTERLINE OF ROADWAY**



**PROFILES PARALLEL TO CENTERLINE OF DRIVEWAY**

**GENERAL CONSTRUCTION NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN SHALL BE IN ACCORDANCE WITH THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE SPECIFIED, GUTTER AND COMBINATION CURB & GUTTER SHALL BE TIED TO THE PAVEMENT WITH 1/2 BARS AS SHOWN.

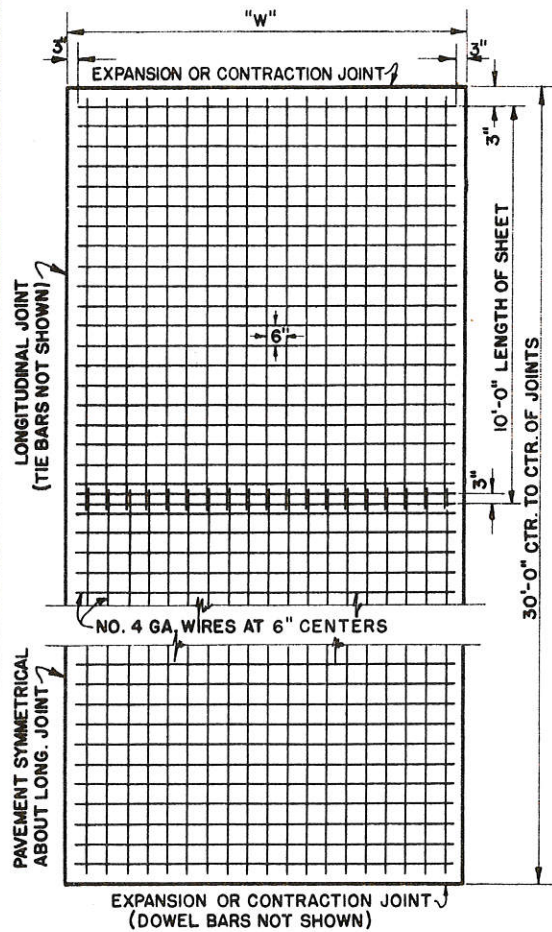
ALL CONCRETE SHALL CONFORM TO THE REQUIREMENTS SPECIFIED IN THE SPECIFICATIONS.

**STANDARD DESIGN CONCRETE CURB, GUTTER, COMBINATION CURB & GUTTER, SURFACE DRAIN & DRIVEWAYS.**

STATE HIGHWAY COMMISSION OF WISCONSIN

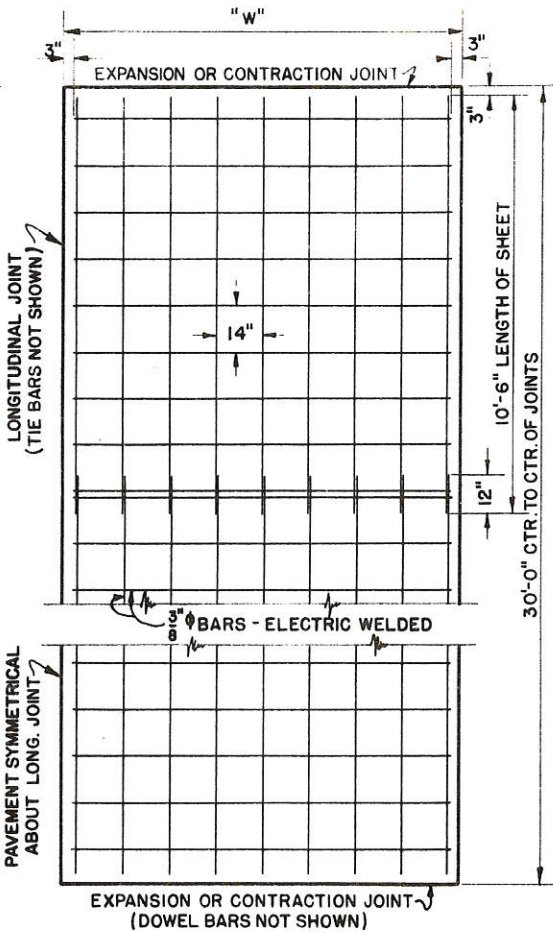
RECOMMENDED FOR APPROVAL:  
*Frank D. ...*  
 DESIGN ENGINEER  
 APPROVED:  
*...*  
 CONSTRUCTION ENGINEER

DRAWN N.F.C. CHECKED R.R.B. STATE HIGHWAY ENGINEER DATE 3/12/37



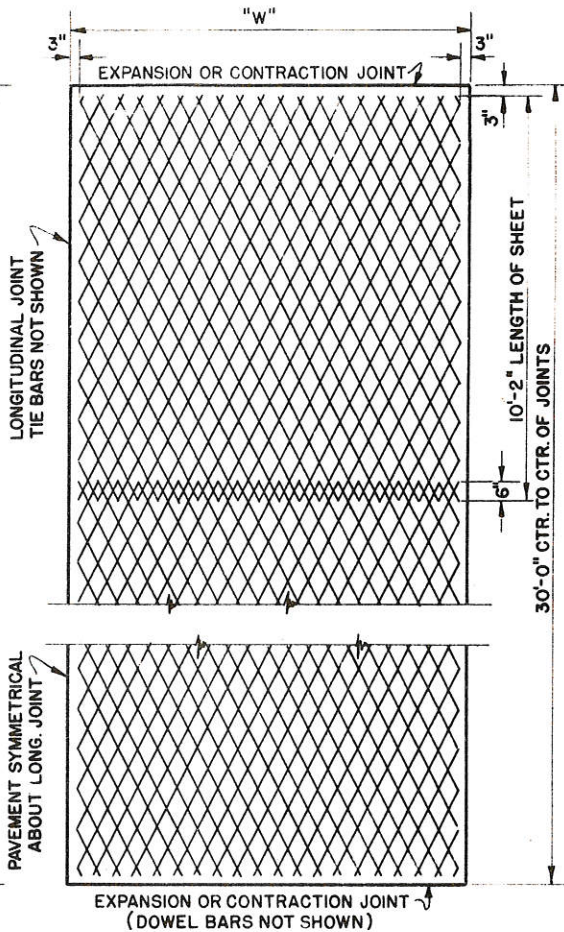
**STEEL FABRIC REINFORCEMENT  
ALTERNATE TYPE "A"**

NOTE:  
THE DIAMETER OF ALL MEMBERS SHALL BE NOT LESS THAN 0.2253 INCHES (NO. 4 GAUGE).  
LONGITUDINAL AND TRANSVERSE MEMBERS OF THE FABRIC SHALL BE SPACED SIX (6) INCHES CENTER TO CENTER AND BE RIGIDLY WELDED AT ALL INTERSECTIONS.



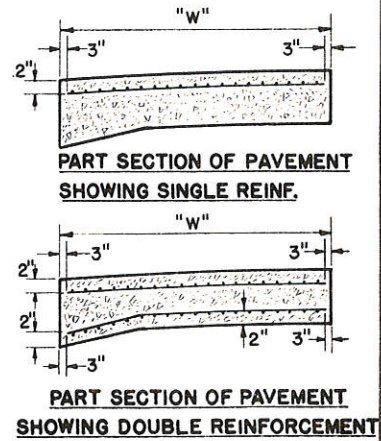
**BAR MAT REINFORCEMENT  
ALTERNATE TYPE "B"**

NOTE:  
LONGITUDINAL AND TRANSVERSE MEMBERS SHALL BE NOT LESS THAN THREE-EIGHTHS INCH ROUND, SMOOTH STEEL RODS SPACED FOURTEEN (14) INCHES CENTER TO CENTER AND RIGIDLY WELDED AT ALL INTERSECTIONS. ALTERNATE METHODS OF FASTENING AT JUNCTIONS OF BARS MAY BE CONSIDERED SUBJECT TO SPECIFIC APPROVAL BY THE DEPARTMENT.



**EXPANDED METAL MESH REINFORCEMENT  
ALTERNATE TYPE "C"**

NOTE:  
REINFORCEMENT SHALL CONSIST OF DIAMOND SHAPED STEEL MESH.  
THE SIZE OF DIAMOND SHALL BE NOT LESS THAN 5" BY 12" NOR MORE THAN 6 1/2" BY 12". UNLESS OTHERWISE SPECIFIED, THE EXPANDED METAL MESH SHALL WEIGH 56 LBS. PER 100 SQ. FT.



**GENERAL NOTES**

DETAILS NOT COVERED ON THE PLAN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND/OR APPLICABLE SPECIAL PROVISIONS.  
THE USE OF SLEDS TO SUPPORT THE REINFORCEMENT WILL NOT BE PERMITTED.  
ALL REINF. MUST BE SHIPPED FROM THE FACTORY AND DELIVERED ON THE SITE OF THE WORK IN FLAT SHEETS.  
SIDE LAPS FOR ADJACENT SHEETS SHALL BE THE SAME AS SHOWN ON THE PLAN FOR END LAPPING.  
DIMENSION "W" REPRESENTS WIDTH OF PAVEMENT SLAB BETWEEN LONGITUDINAL JOINTS OR BETWEEN LONGITUDINAL JOINT AND EDGE OF PAVEMENT.

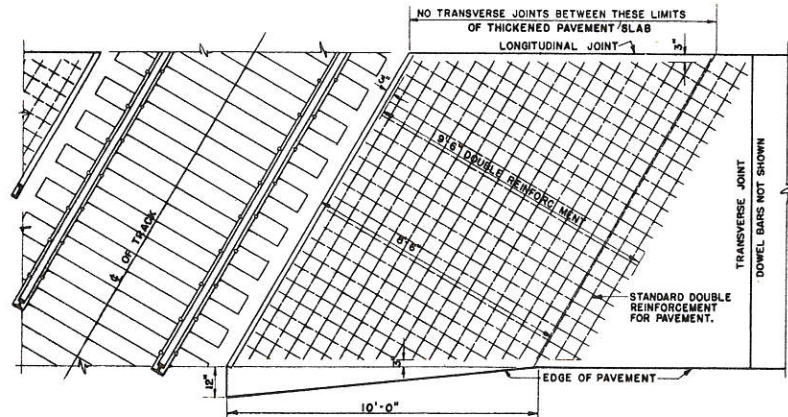
**PLANS OF ALTERNATE TYPES OF STEEL REINFORCEMENT FOR CONG. PAVEMENT**

STATE HIGHWAY COMMISSION OF WISCONSIN  
**STANDARD DESIGN**  
**PAVEMENT REINFORCEMENT**

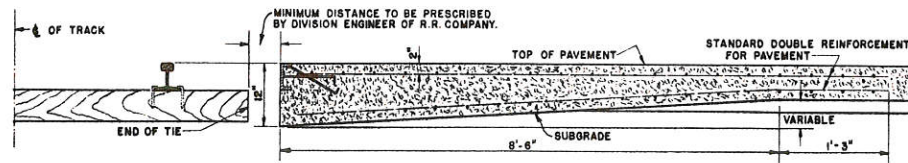
RECOMMENDED FOR APPROVAL:  
*Frank Crare*  
DESIGN ENGINEER  
*M. Bluk*  
CONSTRUCTION ENGINEER

APPROVED:  
*A.G. Rottler*  
STATE HIGHWAY ENGINEER

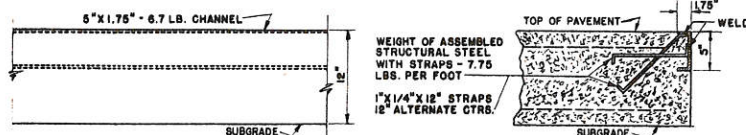
DRAWN RBS 3/16/37  
CHECKED ELR DATE



**PART PLAN OF RAILROAD APPROACH SLABS**



**SECTION AT RIGHT ANGLES TO TRACK SHOWING REINFORCEMENT**



**END ELEVATION**

**SECTION**

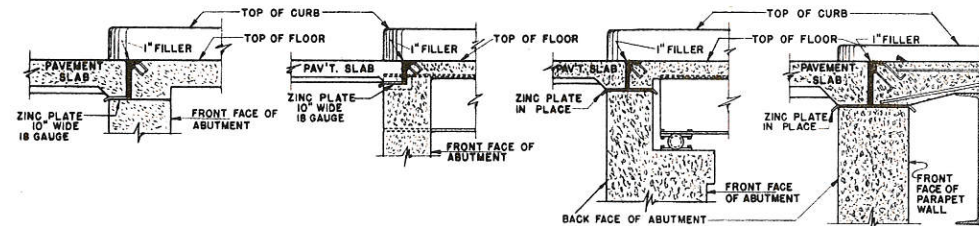
**STRUCTURAL STEEL AT RAILROAD CROSSINGS**

**CONSTRUCTION NOTES**

**STRUCTURAL STEEL & REINFORCEMENT AT R. R. CROSSINGS.**  
 PROTECT ENDS OF PAVEMENT AT ALL RAILROAD CROSSINGS AS SHOWN. PAVEMENT TO BE FLAT. CROWN TO BE WORKED OUT IN 25 FEET.  
 ADDITIONAL CONCRETE REQUIRED IN PAVEMENT TO BE CONVERTED INTO SQUARE YARDS OF STANDARD CONCRETE PAVEMENT AND PAID FOR AS SUCH.  
 STRUCTURAL STEEL IS TO BE MEASURED BY THE POUND AND PAID FOR AT THE CONTRACT UNIT PRICE BID IN THE PROPOSAL FOR "STRUCTURAL STEEL FOR PAVEMENT" WHICH PRICE SHALL BE FULL COMPENSATION FOR FURNISHING COMPLETE IN PLACE.

**SUBGRADE TREATMENT FOR R.R. APPROACH SLABS**

UNLESS THE SUBGRADE IS COMPOSED OF SAND OR OTHER FREELY SELF-DRAINING MATERIAL, POSITIVE DRAINAGE OF THE SUBGRADE SHALL BE OBTAINED BY MEANS OF TRENCHING FOR THE FULL WIDTH OF THE SLAB TO A DEPTH OF AT LEAST 18 INCHES AT THE RAILROAD END OF THE PAVEMENT AND RUNNING OUT TO ZERO AT THE FIRST TRANSVERSE JOINT AND BACKFILLING WITH SAND-GRAVEL BACKFILL. TRENCH SHALL BE DRAINED ADEQUATELY THRU THE SHOULDERS BY MEANS OF PIPE UNDERDRAINS OR STONE UNDERDRAINS AS SHOWN ON THE PLANS.



**SLAB SPANS 6 TO 24 FT. INCL.**

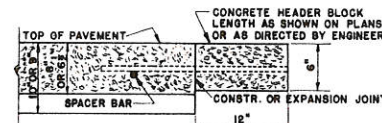
**I-BEAM SPANS 8 TO 45 FT. INCL. DECK GIRDER SPANS 20 TO 45 FT. INCL. ARE SIMILAR**

**I-BEAM SPANS 50 TO 60 FT. INCL. DECK GIRDER SPANS 50, 55 & 60 FT. INCL. ARE SIMILAR**

**STEEL TRUSS AND PLATE GIRDERS**

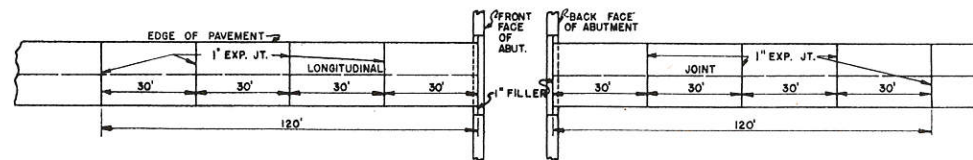
**METHODS OF CONNECTING CONCRETE PAVEMENT WITH BRIDGE STRUCTURES**

NOTE: 1" FILLER TO BE PREMOLDED BITUMINOUS JOINT OR EQUIVALENT.  
 EXPANSION JOINT FILLER ALSO TO BE PLACED BETWEEN CURB ENDS OR OTHER PROJECTING APPURTENANCES IN THE EVENT PAVEMENT IS PLACED IN CONTACT THEREWITH.



CONSTRUCTION JOINT WITH 1/2" & OR □ TIE BARS WHEN PLACED AT EDGE OF PAVEMENT OR STANDARD EXPANSION JOINT WITH STANDARD DOWELS WHEN PLACED AT END OF PAVEMENT.

**CONCRETE HEADER BLOCK**



**DETAIL OF ONE INCH EXPANSION JOINTS IN BRIDGE APPROACHES**

STANDARD DESIGN  
 R.R. APPROACH SLAB, CONC. HEADER BLOCK & PAVT. CONNECTIONS WITH BRIDGES

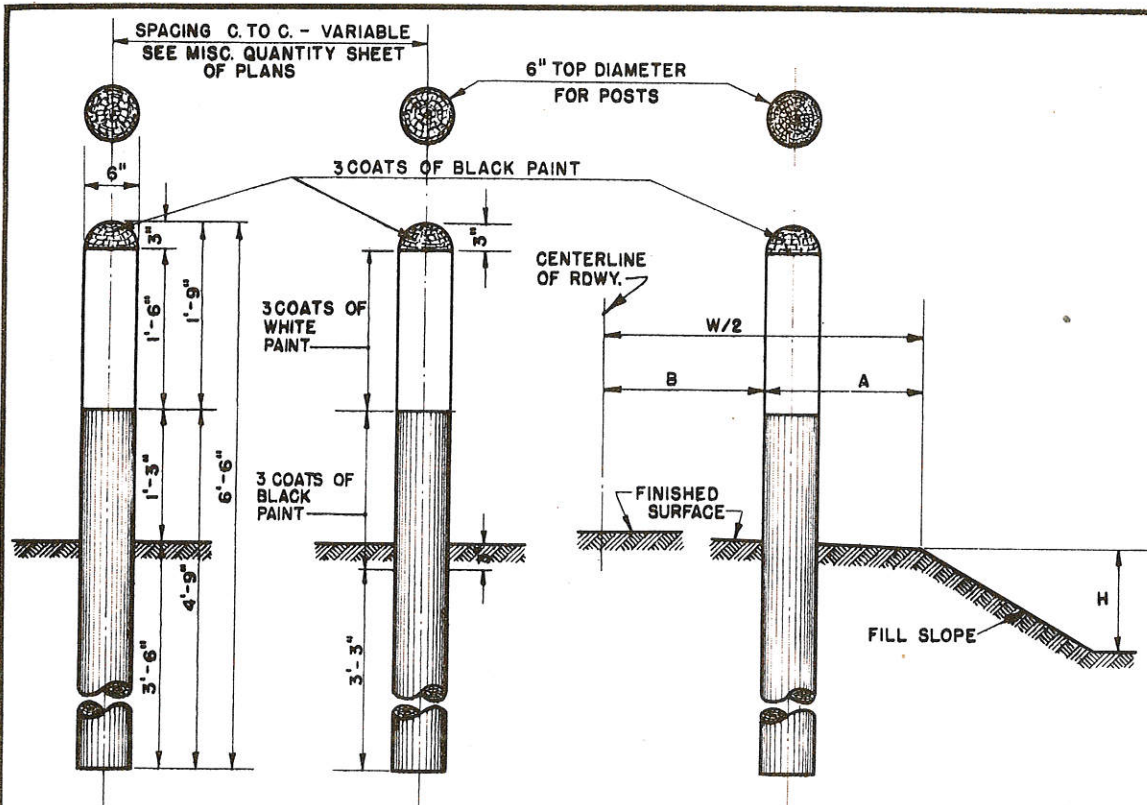
STATE HIGHWAY COMMISSION OF WISCONSIN

RECOMMENDED FOR APPROVAL  
 Frank Cassin DESIGN ENGINEER

APPROVED: E. L. Rostky CONSTRUCTION ENGINEER

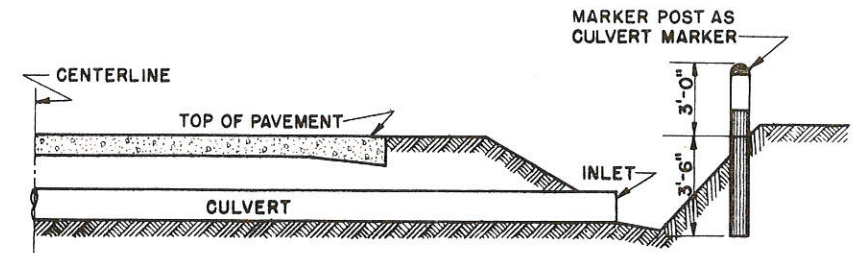
DATE: 3/10/27

DRAWN: RBS CHECKED: W.H.G.

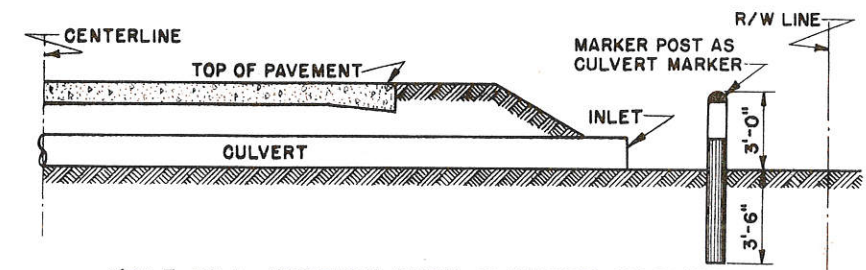


**ELEVATION SHOWING SPACING**

**ELEVATION SHOWING LOCATION  
ON ROAD SHOULDER**



**HALF CUT SECTION WITH CULVERT MARKER**



**HALF FILL SECTION WITH CULVERT MARKER**

**NOTE:**

MARKER POST AS CULVERT MARKER TO BE PLACED AT INLET END OF CULVERT.  
 IN CUT, PLACE POST CLEAR OF CHANNEL.  
 IN FILL, PLACE POST ON BACK SLOPE.  
 IN FILL, PLACE POST BEYOND END OF CULVERT SO IT MAY BE VISIBLE FROM THE ROADWAY.  
 MARKER POSTS FOR RIGHT OF WAY MARKERS SHALL BE PLACED WHERE DIRECTED BY THE ENGINEER.

**TABLE OF DIMENSIONS**

HEIGHT OF FILL	30' RDWY.			36' RDWY.			40' RDWY.		
	B	A	W/2	B	A	W/2	B	A	W/2
UNDER 5 FT.	15'	2'-6"	17'-6"	18'	2'-6"	20'-6"	20'	2'-6"	22'-6"
5 FT. TO 10'	15'	3'-6"	18'-6"	18'	3'-6"	21'-6"	20'	3'-6"	23'-6"
OVER 10 FT.	15'	4'-6"	19'-6"	18'	4'-6"	22'-6"	20'	4'-6"	24'-6"

**GENERAL NOTES**

TOP OF POST SHALL BE NEATLY ROUNDED.

POSTS TO BE CUT AND MANUFACTURED FROM LIVE, GREEN, GROWING NORTHERN WHITE CEDAR, YELLOW PINE, NORWAY PINE, OR WHITE PINE, STACKED & SEASONED IN AN APPROVED MANNER.

PAINT AND APPLICATION SHALL CONFORM TO PERTINENT SPECS.

PORTION OF POST TO BE PAINTED SHALL BE SHAVED TO THE WHITE BEFORE PAINTING.

**MARKER POSTS**

STATE HIGHWAY COMMISSION OF WISC.

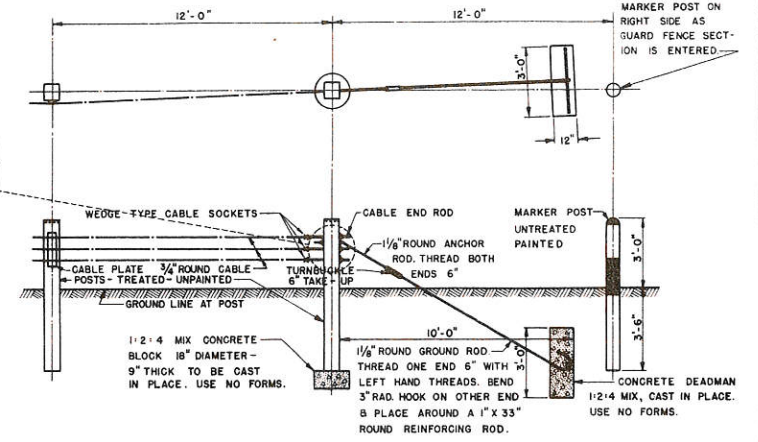
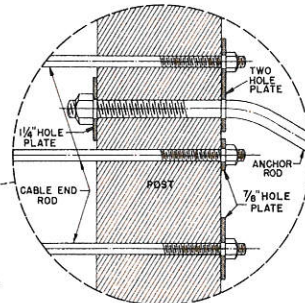
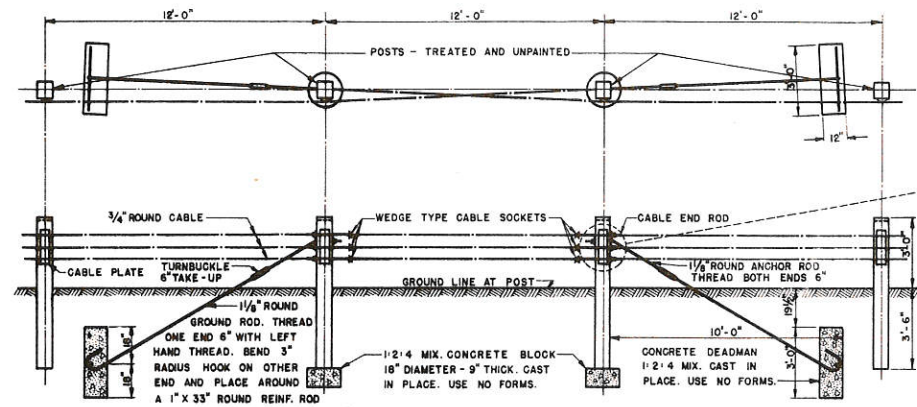
RECOMMENDED FOR APPROVAL:

*Frank Evans*  
DESIGN ENGINEER

*W. Blak*  
CONSTRUCTION ENGINEER

APPROVED:

DRAWN: *F.D.S.* 3/10/37  
CHECKED: *N.F.C.* DATE: *E.G. Rottler*  
STATE HIGHWAY ENGINEER

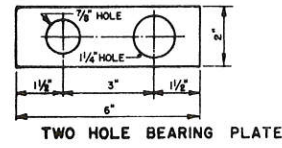


INTERMEDIATE ANCHOR

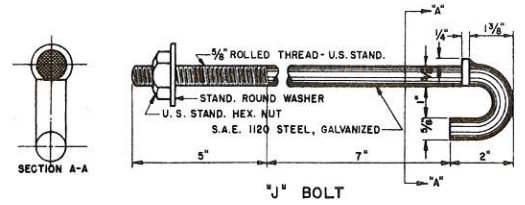
END ANCHOR

GENERAL CONSTRUCTION NOTES

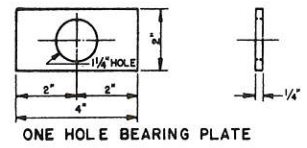
DETAILS OF CONSTRUCTION NOT COVERED ON THIS SHEET SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.  
 FOUR DETAILED DRAWINGS OF SOCKETS, TURNBUCKLES AND PLATES MUST BE SUBMITTED TO THE STATE HIGHWAY COMMISSION FOR APPROVAL BEFORE THEIR USE WILL BE PERMITTED.  
 ALL FITTINGS TO BE GALVANIZED, AND BREAKAGE OF GALVANIZING DURING ERECTION WILL BE CAUSE FOR REJECTION AND MUST BE REPLACED.  
 AFTER ERECTION, ALL CABLE END BOLTS AND "J" BOLTS PROJECTING MORE THAN ONE INCH FROM NUT SHALL BE CUT OFF ONE HALF INCH FROM NUT AND CHECKED.  
 VARIATIONS FROM THESE DESIGNS OF FITTINGS WILL BE CONSIDERED UPON SUBMISSION OF FOUR COPIES OF DETAILED PLANS FOR APPROVAL.  
 IN GENERAL, NO CONTINUOUS SECTION OF CABLE SHALL EXCEED 750 FEET WITHOUT INTERMEDIATE ANCHORAGE.  
 SEE TYPICAL SECTION SHEET FOR LOCATION OF RAIL IN REFERENCE TO SHOULDER.



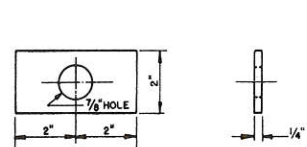
TWO HOLE BEARING PLATE



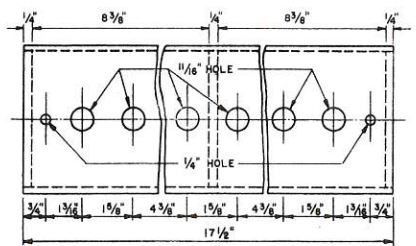
"J" BOLT



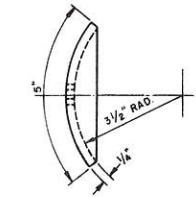
ONE HOLE BEARING PLATE



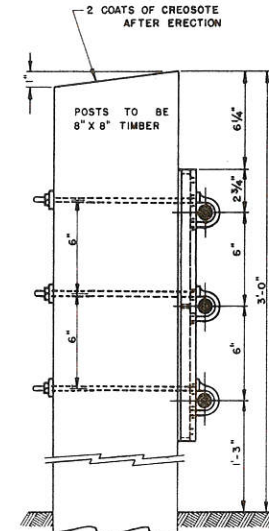
ONE HOLE BEARING PLATE



CABLE



PLATE

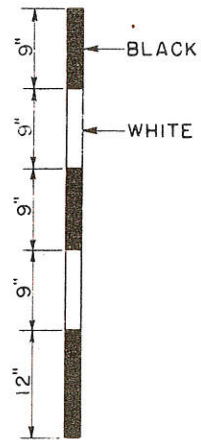
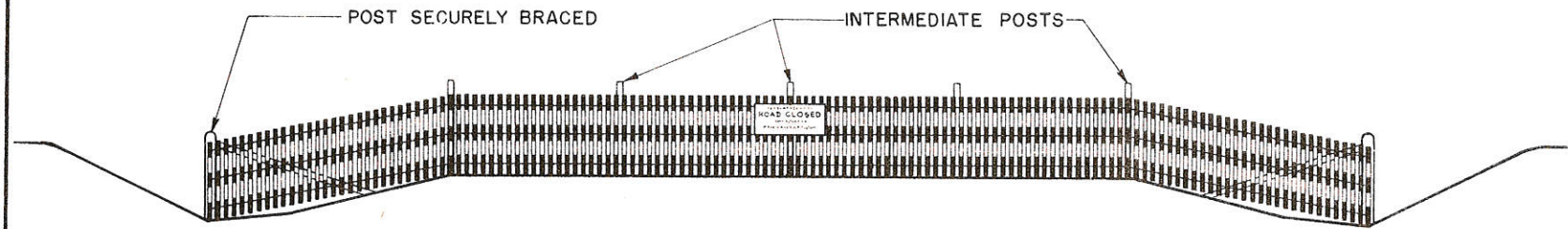


SIDE ELEVATION

STANDARD DESIGN FOR	
<b>3 WIRE CABLE GUARD FENCE</b>	
STATE HIGHWAY COMMISSION OF WISCONSIN	
RECOMMENDED FOR APPROVAL:	
<i>Frank Brown</i> DESIGN ENGINEER	<i>A. J. ...</i> ENGINEER
APPROVED	
DRAWN BY <i>...</i>	DATE <i>3/10</i> 1937.
CHECKED BY <i>...</i>	STATE HIGHWAY ENGINEER

SECTION OF SNOW FENCE TO BE PAINTED ALTERNATELY BLACK AND WHITE AS SHOWN BELOW.  
 LENGTH TO BE SUFFICIENT TO REACH FROM TOE OF THE BACKSLOPE TO TOE OF THE BACKSLOPE.  
 PROVIDE SUFFICIENT NUMBER OF INTERMEDIATE POSTS TO ADEQUATELY SUPPORT THE FENCE.

STANDARD "ROAD CLOSED" SIGN (W.H.C.- NO. R-6) TO BE ERECTED ON FENCE AT THE TOP ON  
 THE CENTERLINE OF ROAD.

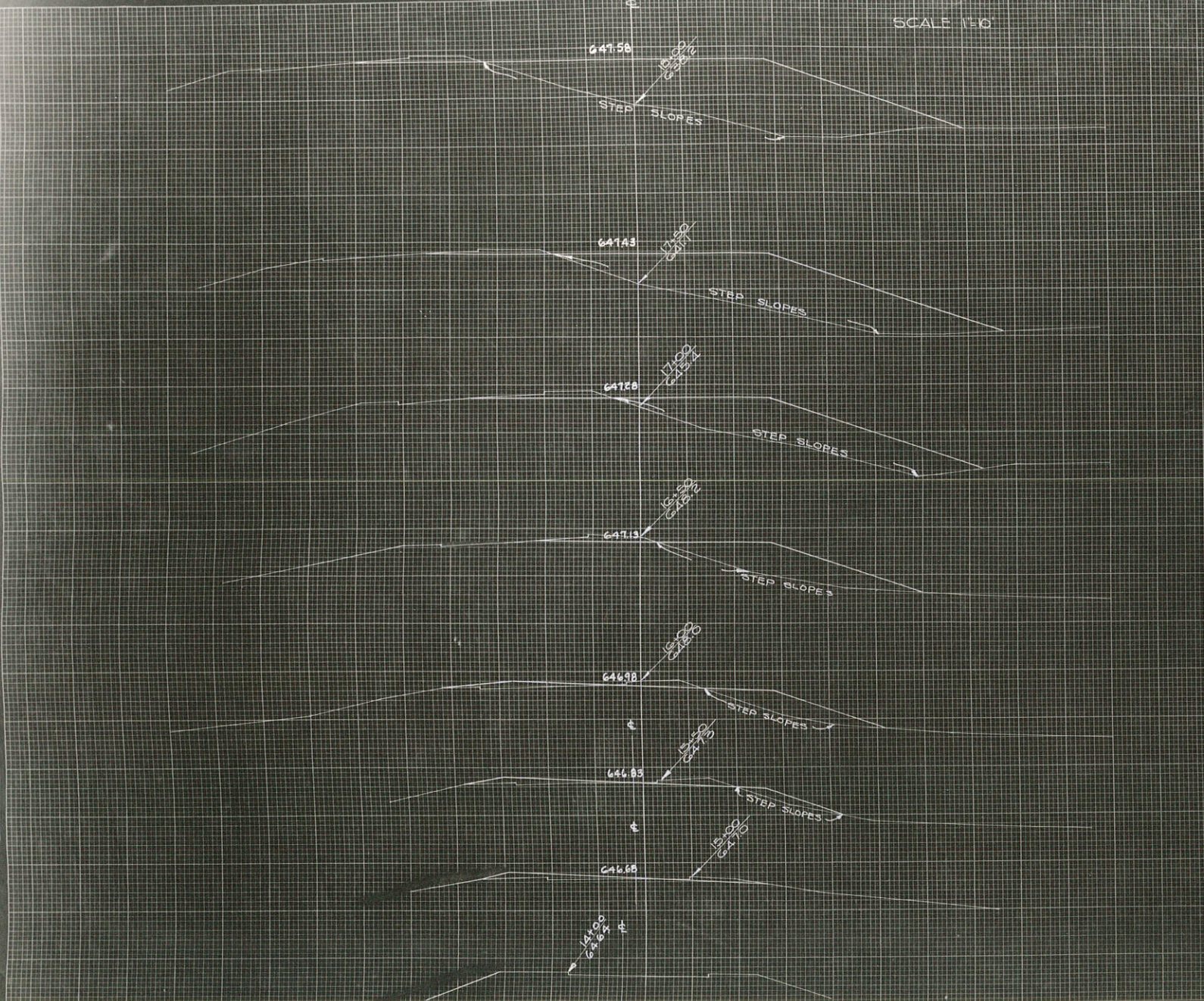


DETAIL SHOWING METHOD OF STRIPING  
 FOR 4' SNOW FENCE.

STATE HIGHWAY COMMISSION OF WISCONSIN	
<b>CONSTRUCTION BARRICADE</b>	
RECOMMENDED FOR APPROVAL:	
<i>Frank Brase</i> DESIGN ENGINEER	
<i>M. Bluh</i> 6/13/35 CONSTRUCTION ENGINEER	
APPROVED:	
DRAWN HM CHECKED WHG	DATE 6/15/35 <i>E. G. Rettig</i> STATE HIGHWAY ENGINEER

ENGINEER'S USE ONLY  
 SCALE 1"=10'

FILE NO. 4  
 STATE WISC.  
 PROJ. NO. 3276  
 SHEET NO. 106  
 DIVISION JOB NO. 3345



CY STEP SLOPES	STATION	END AREA		FILL	YARDAGE					
		EXCAVATION			EXCAVATION					
		COMMON	ROCK		COMMON	ROCK	TOTAL	TOTAL FILL		
	13	0		0						
	14	0		0						
	15	12		30	100	22			22	56
7	16	18		43	50	28			28	67
20	17	16		122	50	31			31	152
10	18	22		212	50	35			35	309
31	19	20		448	50	39			39	611
46	20	19		632	50	37			37	1056
44	21	26		815	50	41			41	1394
167										

SUB TOTAL - MILE \_\_\_\_\_  
 SHEET TOTAL 233 233 3645

DATE: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_

SCALE: 1"=10'  
 SHEET NO. 106  
 PROJECT NO. 3276  
 DIVISION JOB NO. 3345



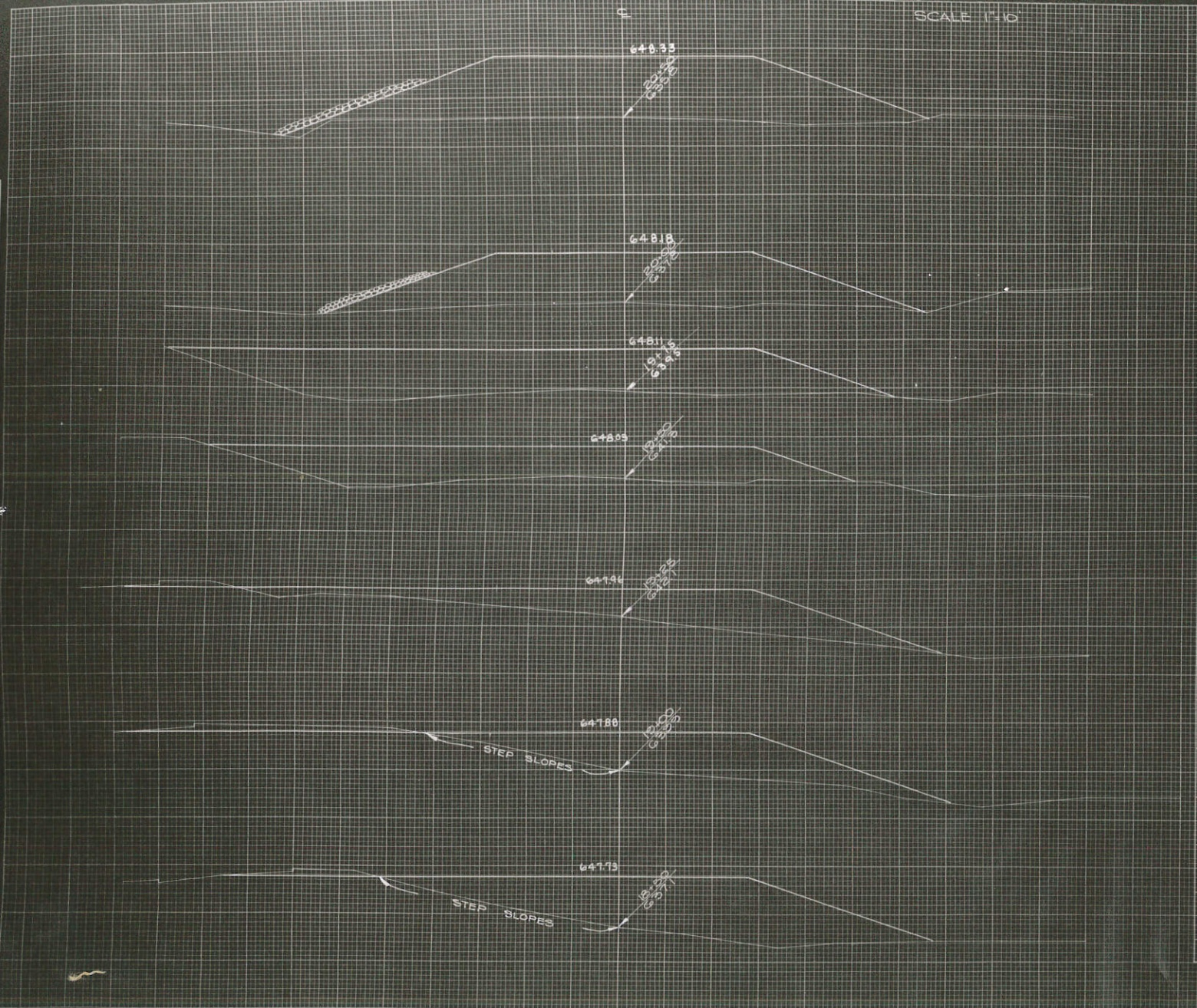
REPRODUCED FROM THE ORIGINAL DRAWING BY THE MISSOURI DEPARTMENT OF TRANSPORTATION

SCALE 1"=10'

FED. ROAD DIST. NO. 4 STATE MO. PROJ. NO. 3970 DRAWING NO. 107 TOTAL SHEETS 119 DIVISION JOB NO. 245

DATE: 11/10/54  
 DRAWN BY: J. A. J. J. J.  
 CHECKED BY: J. A. J. J. J.  
 APPROVED BY: J. A. J. J. J.

DATE: 11/10/54  
 DRAWN BY: J. A. J. J. J.  
 CHECKED BY: J. A. J. J. J.  
 APPROVED BY: J. A. J. J. J.



CY STEP SLOPES	STATION	END AREA			DISTANCE	YARDAGE				
		COMMON EXCAVATION	ROCK EXCAVATION	FILL		COMMON EXCAVATION	ROCK EXCAVATION	TOTAL EXCAVATION	TOTAL FILL	
	18	26		815						
	37	+50	29	912	50	50		50		600
	43	19	70	592	50	93		93		393
	13	+25	20	628	25	41		41		565
		+50	0	834	25	9		9		674
		+75	0	1200	25	0		0		943
	20	0		800	25	0		0		103
	+50	0		500	50	0		0		2065
93										

SUB TOTAL - MILE \_\_\_\_\_  
 SHEET TOTAL 193 193 825

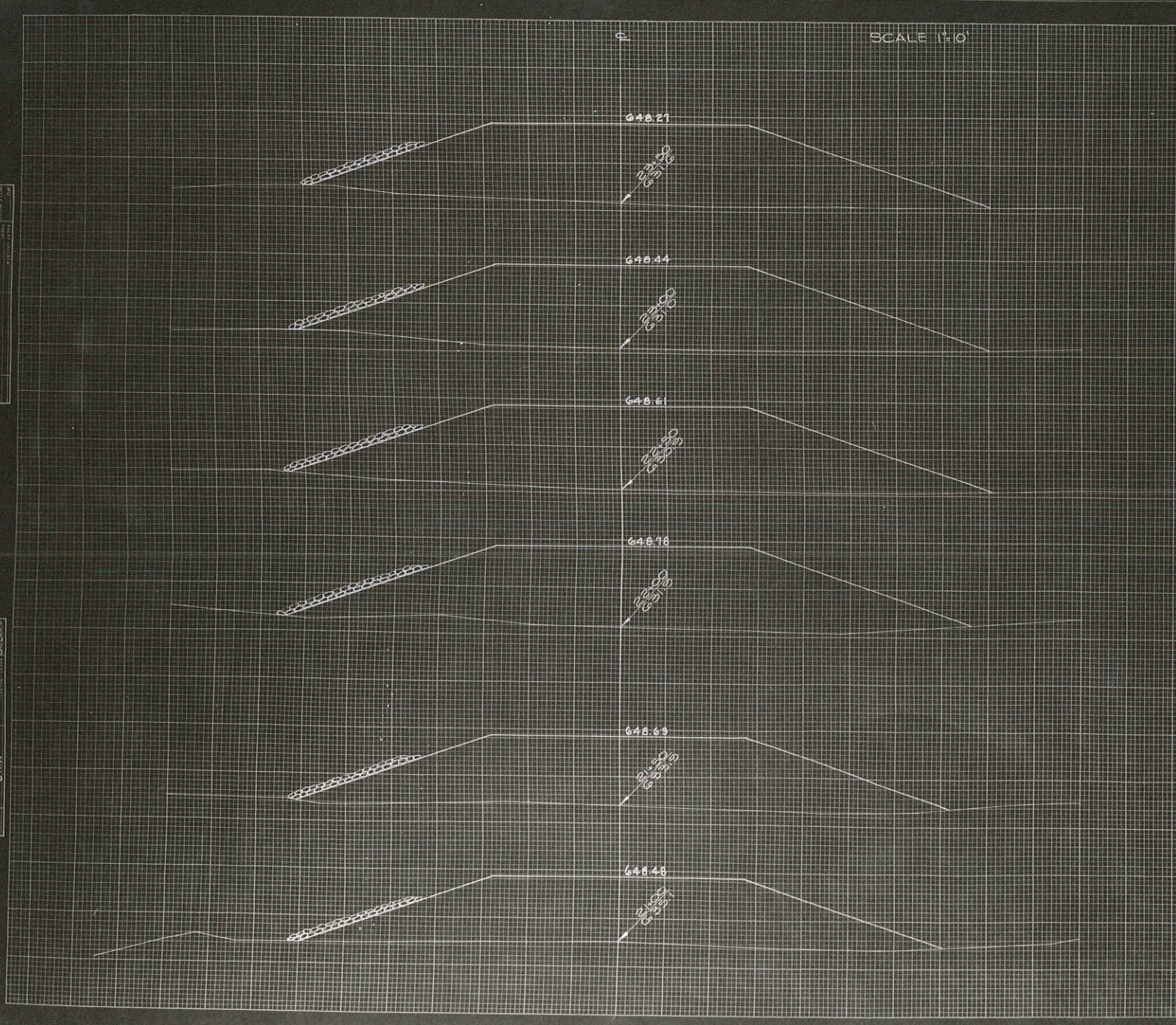
DESIGNED BY: M. R. V. MOYER, P.E. DRAWN BY: J. J. MOYER, P.E. CHECKED BY: J. J. MOYER, P.E. DATE: 10/15/05

SCALE 1"=10'

PER. NO. 4 STATE WISC. FAP. NO. 397-G BAK. NO. 108 TOTAL SHEETS 119  
DIVISION JOB NO. 5045

NO. 1001  
DATE 10/15/05  
BY J. J. MOYER  
CHECKED J. J. MOYER  
APPROVED J. J. MOYER

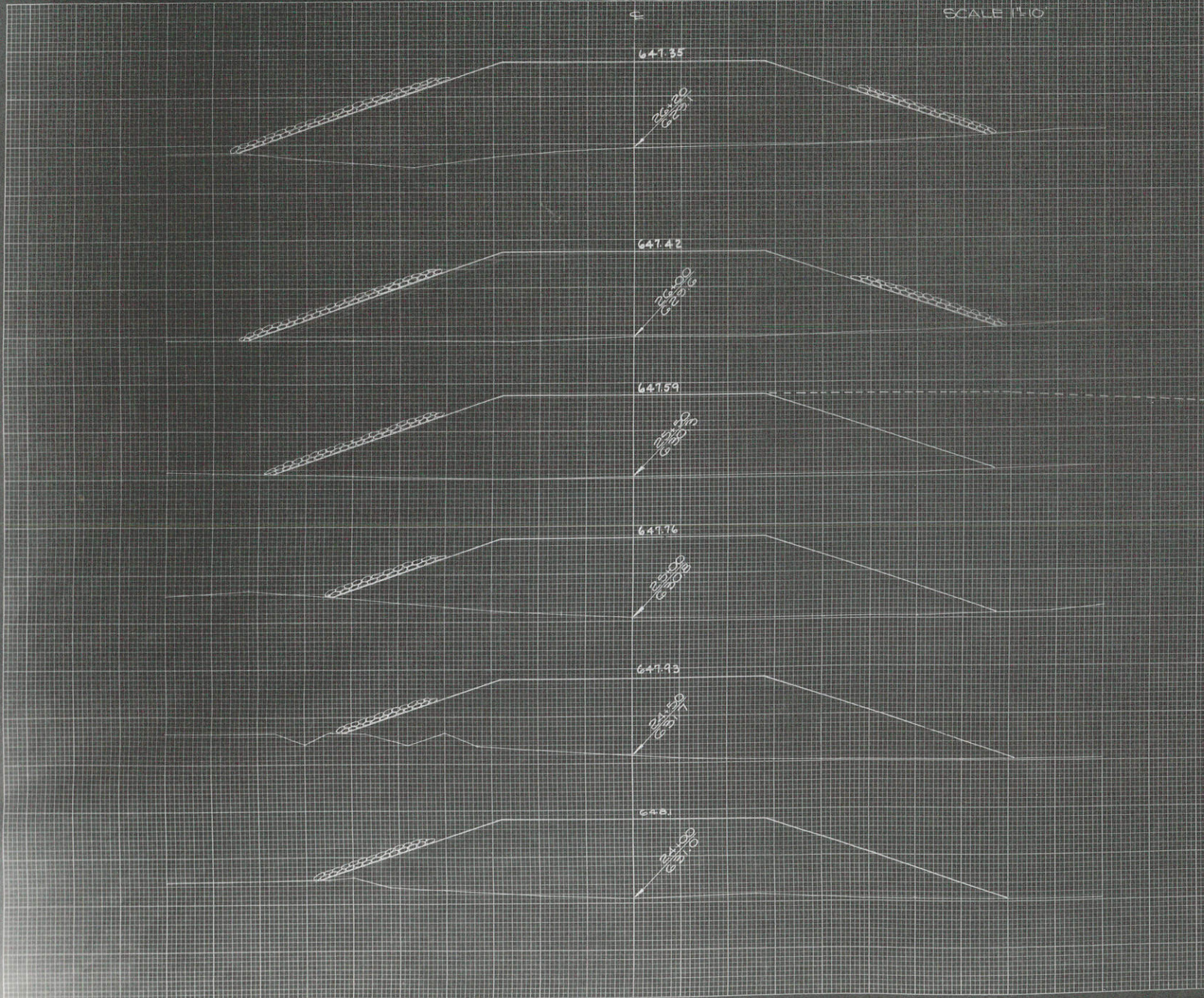
NO. 1002  
DATE 10/15/05  
BY J. J. MOYER  
CHECKED J. J. MOYER  
APPROVED J. J. MOYER



CIV. STEP SLOPES	STATION	END AREA			DISTANCE	YARDAGE			TOTAL FILL
		EXCAVATION		TOTAL		EXCAVATION		TOTAL	
		COMMON	ROCK			COMMON	ROCK		
	20	0		50	0	0	0	2400	
	+50	0		50	0	0	0	2400	
	21	0		50	0	0	0	2400	
	+50	0		50	0	0	0	2400	
	22	0		50	0	0	0	2400	
	+50	0		50	0	0	0	2400	
	23	0		50	0	0	0	2400	
	+50	0		50	0	0	0	2400	
SUB TOTAL - MILE									
SHEET TOTAL					0		0	1005B	

SCALE 1"=10'

PER. ROAD DIST. NO.	STATE	F.A.P. DIST. NO.	SHEET NO.	TOTAL SHEETS
A	WISC	347-6	109	119
DIVISION JOB NO. 5545				



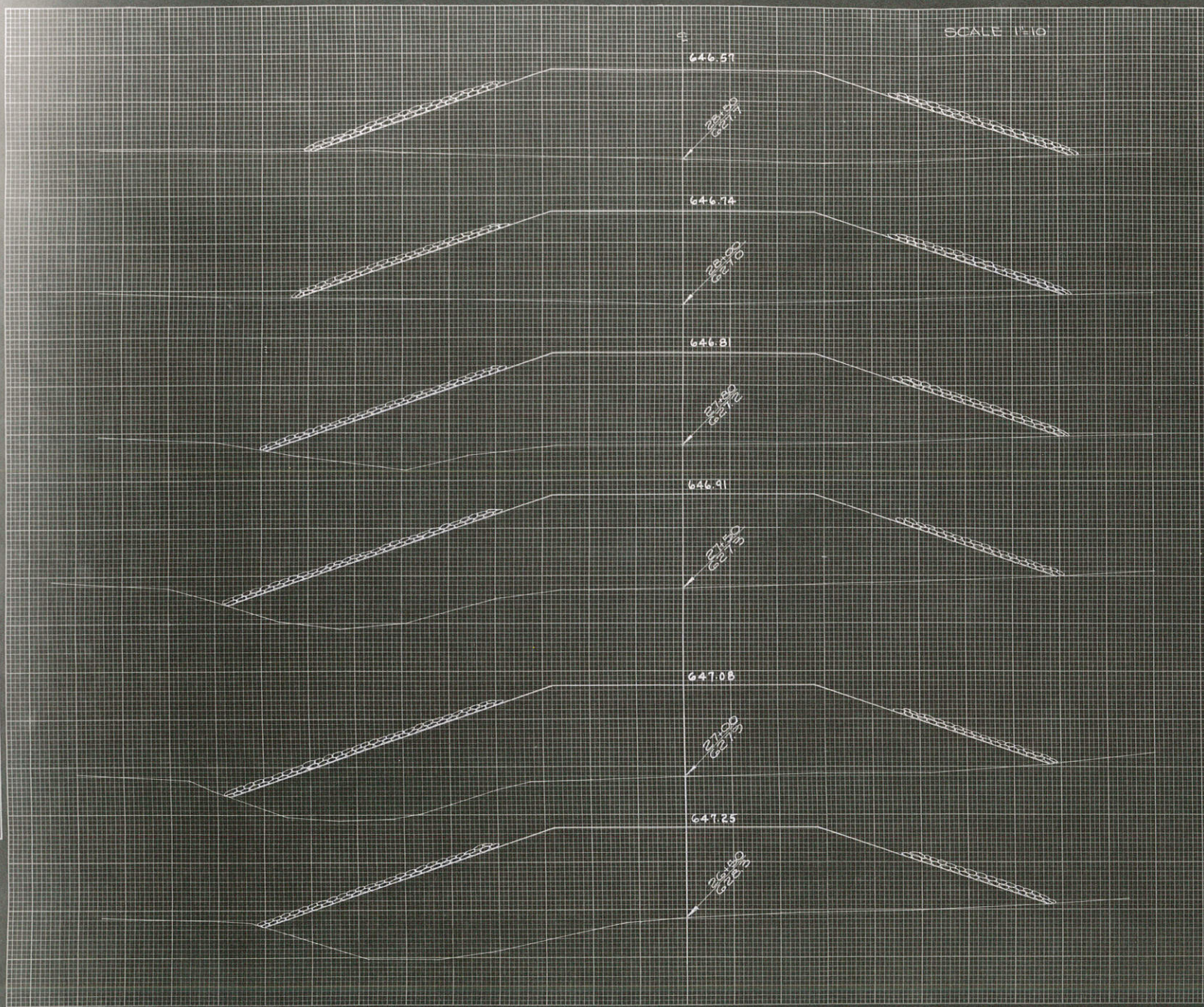
CY STEP SLOPES	STATION	END AREA			DISTANCE	YARDAGE				
		COMMON EXCAVATION	ROCK EXCAVATION	FILL		COMMON EXCAVATION	ROCK EXCAVATION	TOTAL EXCAVATION	TOTAL FILL	
	23	0								
	+50	0			50	0			0	34
	24	0			50	0			0	30
	+50	0			50	0			0	20
	25	0			50	0			0	30
	+50	0			50	0			0	30
	26	0			50	0			0	40
	+20	0			20	0			0	14
SUB TOTAL - MILE									0	0
SHEET TOTAL									0	134

DATE	BY	CHECKED	DATE

DATE	BY	CHECKED	DATE

SCALE 1"=10'

PLAN NO. 4  
 STATE WISCONSIN  
 DIVISION JOB NO. 3435  
 SHEET NO. 119



CY. STEP SLOPES	STATION	END AREA		DISTANCE	YARDAGE			
		COMMON	ROCK		COMMON	ROCK	TOTAL	TOTAL FILL
	26	0						
	+20	0		30	0			253
	+50	0		50	0			400
	27	0		50	0			400
	+50	0		30	0			267
	+80	0		20	0			160
	28	0		50	0			385
	+50	0						

DATE	NO. OF SHEETS
SCALE	DATE
BY	DATE
CHECKED	DATE

DATE	NO. OF SHEETS
SCALE	DATE
BY	DATE
CHECKED	DATE

SUB TOTAL - MILE  
 SHEET TOTAL

0 0 1989