

| STATE PROJECT | FEDERAL PROJECT |          |
|---------------|-----------------|----------|
|               | PROJECT         | CONTRACT |
| 5991-0-14     | M 4202 (001)    | 1        |
|               |                 |          |
|               |                 |          |

**Index of Sheets**

|                      |  |
|----------------------|--|
| Sheet No. 1          | Title                                    |
| Sheet No. 2-2.6      | Typical Cross Sections                   |
| Sheet No. 3          | Estimate of Quantities                   |
| Sheet No. 3A thru 3D | Miscellaneous Quantities                 |
| Sheet No. 4          | Right of Way Plat                        |
| Sheet No. 5-5.4      | Plan and Profile STA. 73 + 56 TO 85 + 16 |
| Sheet No. 6-6.10     | Standard Details                         |
| Sheet No. —          | Structure Plans                          |
| Sheet No. —          | Computer Earthwork Data                  |
| Sheet No. 8-8.4      | Cross Sections                           |

TOTAL SHEETS = 35



STATE OF WISCONSIN  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

PLAN AND PROFILE OF PROPOSED  
**LOSEY BOULEVARD, CITY OF LA CROSSE**

STATE ROAD INTERSECTION

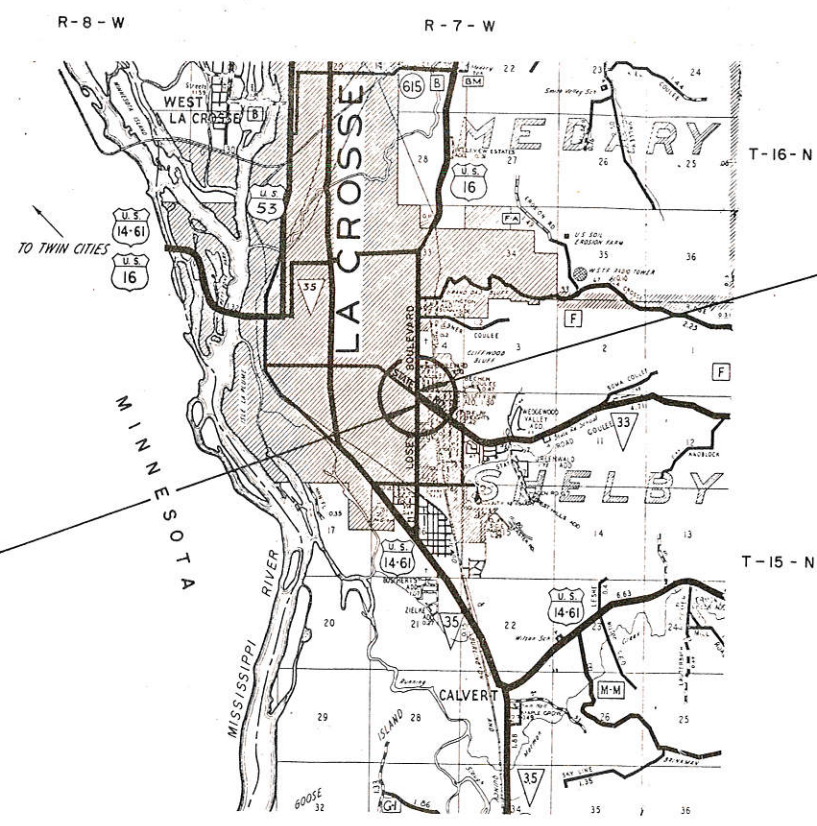
LA CROSSE COUNTY

STATE PROJECT NUMBER  
**5991-0-14**

Scales  
 Plan 1 in. = 20 ft.  
 Profile Hor. 1 in. = 20 ft. Vert. 1 in. = 1 ft.  
 Cross Sections Hor. 1 in. = 5' Vert. 1 in. = 2'

**Design Designation**

|             | LOSEY BLVD. | STATE ROAD |
|-------------|-------------|------------|
| A.D.T. 1970 | = 15,600    | 12,000     |
| A.D.T. 1975 | = 20,900    | 16,200     |
| D.H.V.      | = 1.505     | 1.165      |
| D.          | = 60        | 60         |
| T.          | = 8%        | 8%         |
| V.          | = 25 MPH    | 25 MPH     |



BEGIN PROJECT 5991-0-14  
 STA. 73 + 56  
 N 656,469.907  
 E 1,677,950.137  
 50 FEET WEST AND 1195 FEET SOUTH OF  
 THE NORTH ONE QUARTER CORNER OF SECTION 9,  
 T 15 N, R 7 W.  
 = STA. 73 + 56 OF PROJ. T 4140(4)

END PROJECT 5991-0-14  
 STA. 85 + 16  
 N 657,629.820  
 E 1,677,935.971  
 50 FEET WEST AND 35 FEET SOUTH OF THE  
 NORTH ONE QUARTER CORNER OF SECTION 9,  
 T 15 N, R 7 W  
 = STA. 85 + 16 OF PROJ. U-08-3(37)

Layout  
 Scale 0 2 MI.  
 Total Net Length of Centerline = 0.220 MI. URBAN

**Conventional Signs**

|  |         |                               |       |
|--|---------|-------------------------------|-------|
| County Line  | -----   | Culverts in Place             | ----- |
| Township or Range Line                                   | -----   | Culverts Required             | ----- |
| Section Line   | -----   | Drop Inlet                    | ----- |
| New Right of Way Line                                    | -----   | Power Pole                    | ----- |
| Present Right of Way Line                                | -----   | Telephone or Telegraph Pole   | ----- |
| Wire Fence   | -----   | Right of Way Markers          | ----- |
| Corporate or City Limits                                 | -----   | Reference Stake for Hubs Only | ----- |
| Property Line  | -----   | Marsh                         | ----- |
| Traveled Way or P.E.                                     | -----   | Hedge                         | ----- |
| Railroads  | -----   | Trees                         | ----- |
| Base or Survey Line                                      | -----   | Ground Elevation              | ----- |
| Caution Symbol<br>(combustible fluids<br>under pressure) | CAUTION | Grade Elevation               | ----- |

APPROVED FOR  
 CITY OF LA CROSSE  
 BY

11-19-75  
 DATE Arnold A. Mullerbach  
 CITY ENGINEER

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

Surveyor: CITY OF LA CROSSE District Checker: RVR  
 Designer: CITY OF LA CROSSE CO Checker: ILJ

Correct:  
 Date 10/6/75 L.H. Schneider  
 District Engineer

Recommended for Approval:  
 Date 11/12/75 J.C. Hennrich  
 Chief of Facilities Development

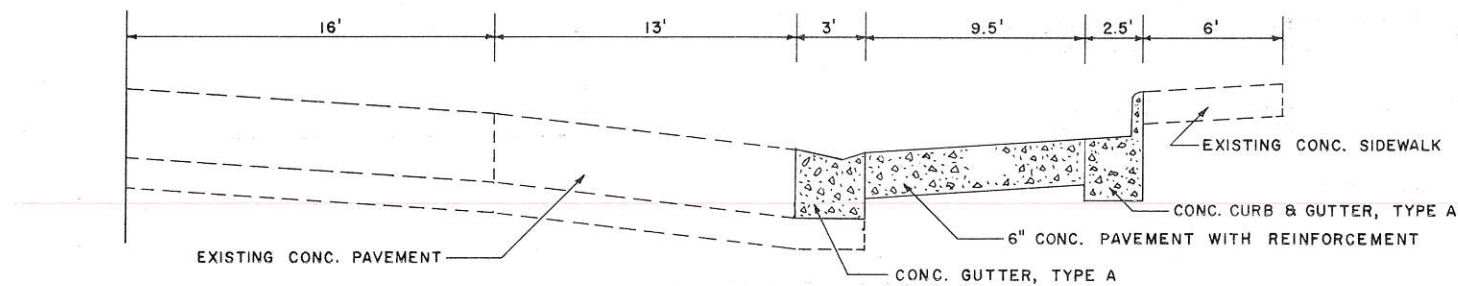
Approved:  
 Date 11-14-75 H.J. Siedler  
 State Highway Engineer

U.S. DEPARTMENT OF TRANSPORTATION  
 FEDERAL HIGHWAY ADMINISTRATION  
 REGION 5 WISCONSIN DIVISION

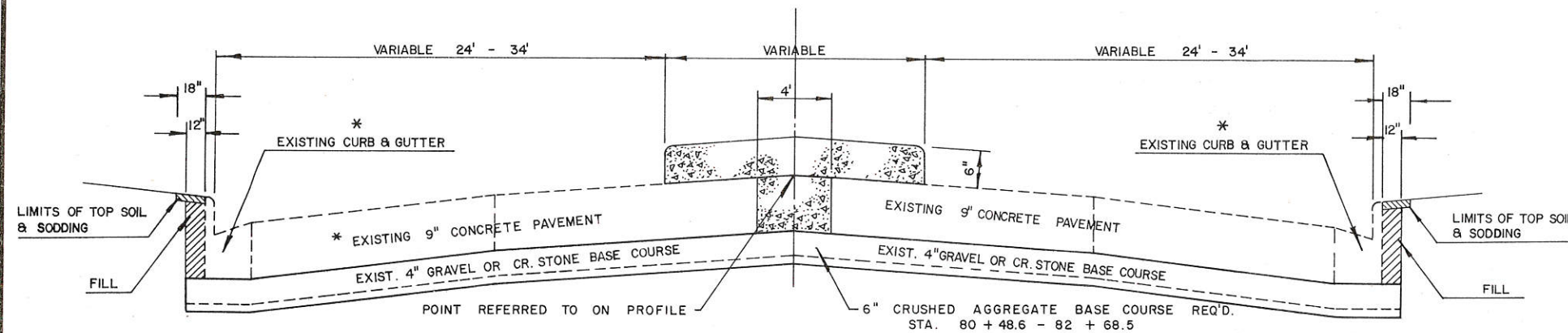
Approved:  
 Date \_\_\_\_\_  
 Division Engineer

NOTE:  
 ALL COORDINATES SHOWN ON THIS PLAN  
 ARE REFERENCED TO THE WISCONSIN  
 COORDINATE SYSTEM SOUTH ZONE.



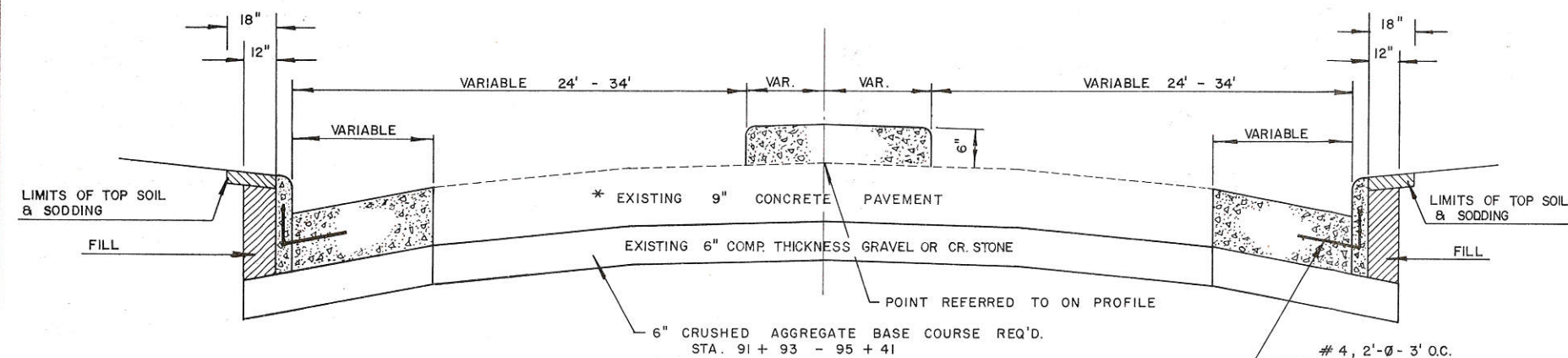


TYPICAL SECTION OF BUS STOP ON LOSEY BLVD.

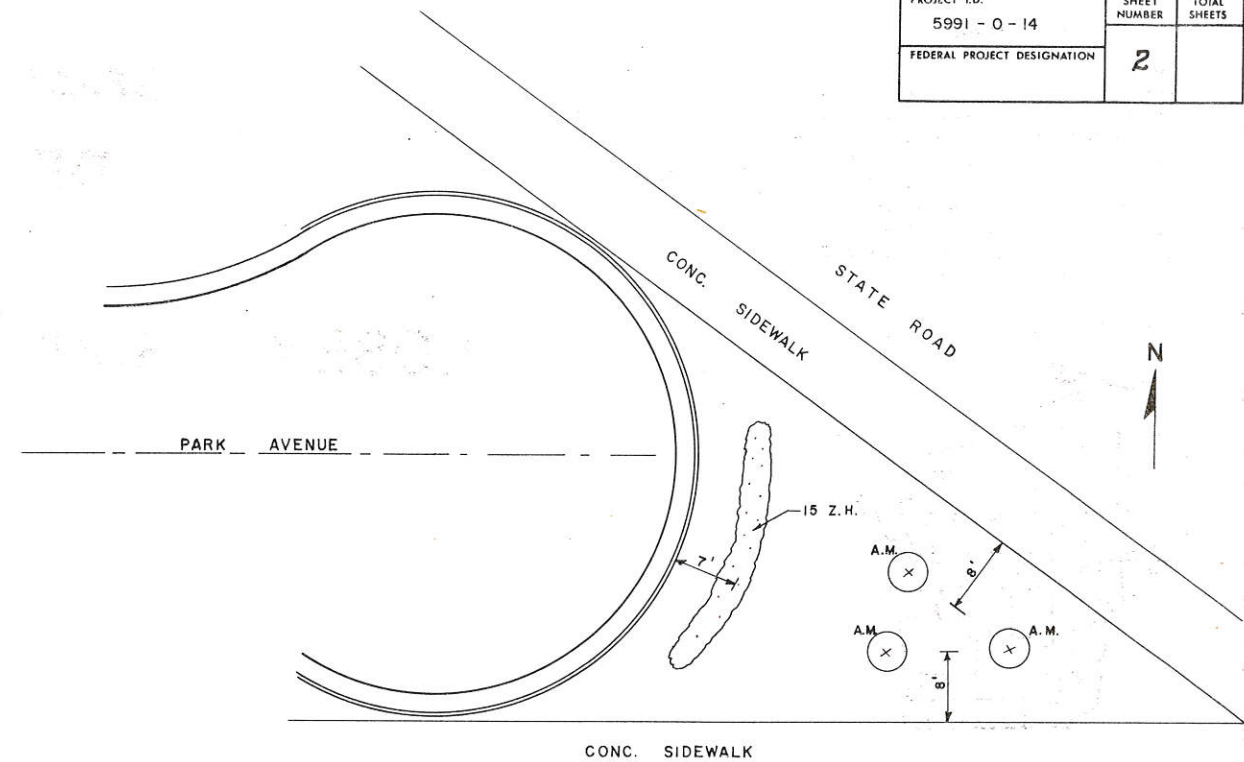


TYPICAL SECTION LOSEY BLVD.

\* REMOVE & REPLACE AS INDICATED ON PLAN

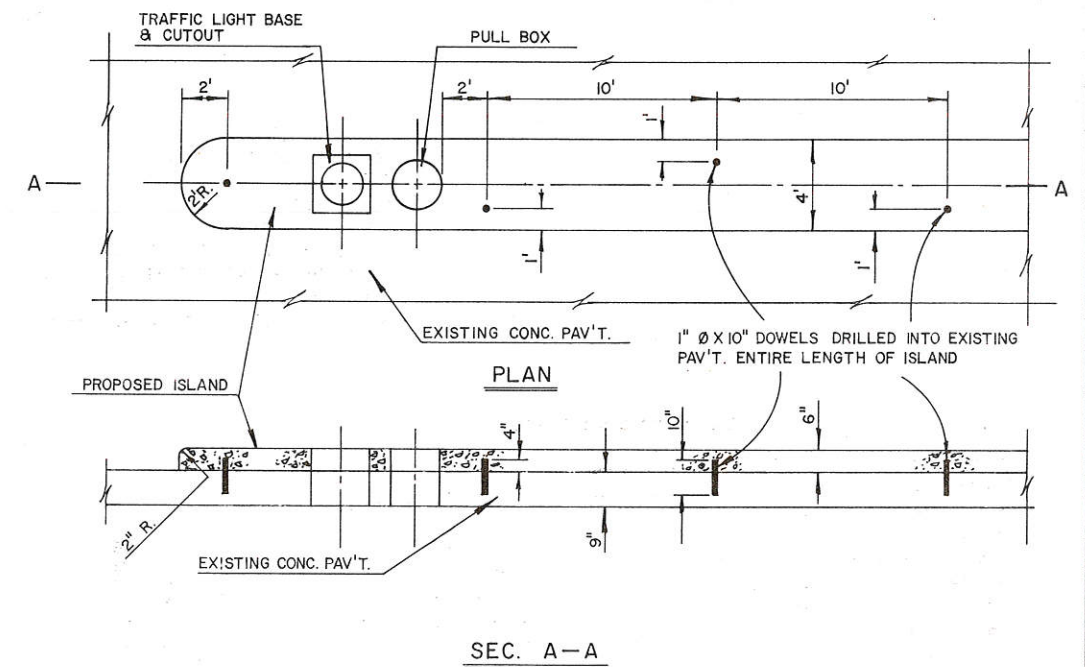


TYPICAL SECTION STATE ROAD



PLANT DATA

| SYMBOL | COMMON NAME          | SCIENTIFIC NAME           | SIZE WHEN PLANTED | MIN. BALL SIZE | MIN. HOLE SIZE | COMPOST QUARTS EACH | FERTILIZER UNITS REQ'D | MULCH DIA. |
|--------|----------------------|---------------------------|-------------------|----------------|----------------|---------------------|------------------------|------------|
| A.M.   | MAPLE AMUR           | ACER GINNALA              | 4'-5' HT., B.B.B. | 15" X 11"      | 28" X 18"      | 15                  | 3                      | 48"        |
| Z.H.   | HONEYSUCKLE BLUELEAF | LONICERA KOROLKOWI ZABELI | 3'-4' HT., BR.    |                | 20" X 15"      | 10                  | 2                      |            |



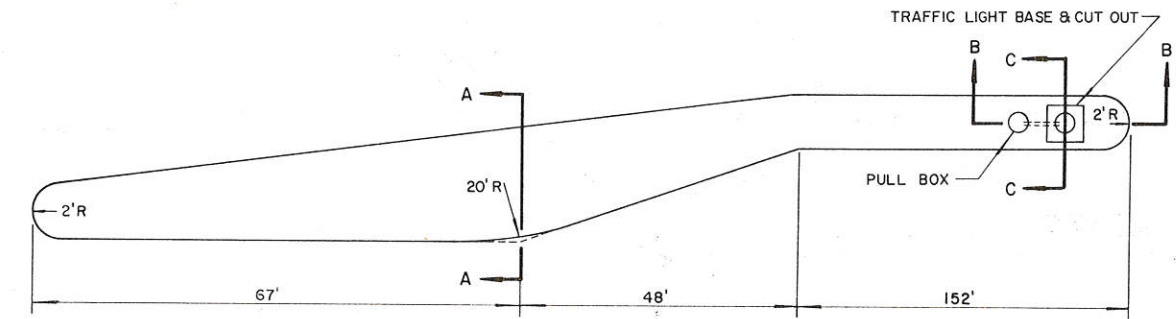
PROPOSED TRAFFIC ISLAND



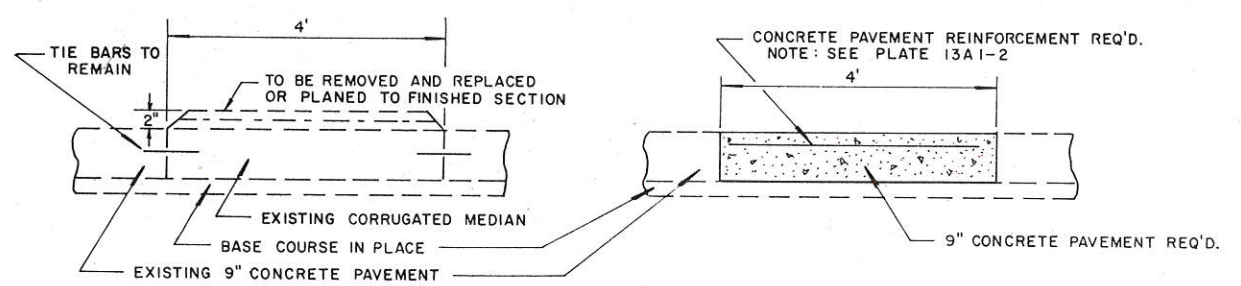
STANDARD ABBREVIATIONS

|          |                               |        |                          |            |                                       |
|----------|-------------------------------|--------|--------------------------|------------|---------------------------------------|
| A.D.T.   | AVERAGE DAILY TRAFFIC         | G.     | GARAGE                   | R.R.       | RAILROAD                              |
| AH.      | AHEAD                         | GAL.   | GALLON                   | R.C.C.P.   | REINFORCED CONCRETE CULVERT PIPE      |
| ET. AL.  | AND OTHERS                    | H.P.   | HIGH POINT               | R.C.P.S.S. | REINFORCED CONCRETE PIPE, STORM SEWER |
| BK.      | BACK                          | H.     | HOUSE                    | REQ'D      | REQUIRED                              |
| B        | BARN                          | H.T.   | HOUSE TRAILER            | RT.        | RIGHT                                 |
| B.M.     | BENCH MARK                    | HOR.   | HORIZONTAL               | R.H.F.     | RIGHT HAND FORWARD                    |
| BIT.     | BITUMINOUS                    | IN.    | INCHES                   | R/W        | RIGHT OF WAY                          |
| BLVD.    | BOULEVARD                     | Δ or I | INTERSECTION ANGLE       | RD.        | ROAD                                  |
| BLDGS.   | BUILDINGS                     | I.H.   | INTERSTATE HIGHWAY       | SALV.      | SALVAGED                              |
| C.B.     | CATCH BASINS                  | I.P.   | IRON PIN                 | SAN.       | SANITARY                              |
| ¢        | CENTERLINE                    | L.F.   | LINEAL FEET              | S.         | SOUTH                                 |
| Δ        | CENTRAL ANGLE OR DELTA        | LT.    | LEFT                     | SHR.       | SHRINKAGE                             |
| CH. CH.  | CHANNEL CHANGE                | L.H.F. | LEFT HAND FORWARD        | S.W.       | SIDEWALK                              |
| CL.      | CLASS                         | L      | LENGTH OF CURVE          | STD.       | STANDARD                              |
| C.M.C.P. | CORRUGATED METAL CULVERT PIPE | L.S.   | LUMP SUM                 | S.T.H.     | STATE TRUNK HIGHWAY                   |
| CONC.    | CONCRETE                      | L.H.E. | LIMITED HIGHWAY EASEMENT | STA.       | STATION                               |
| CONST.   | CONSTRUCTION                  | M.H.   | MANHOLE                  | S.S.       | STORM SEWER                           |
| C.P.     | CULVERT PIPE                  | MAX.   | MAXIMUM                  | ST.        | STREET                                |
| C.T.H.   | COUNTY TRUNK HIGHWAY          | MI.    | MILE                     | S.E.       | SUPERELEVATION                        |
| CWT.     | HUNDRED WEIGHT                | MIN.   | MINIMUM                  | SUBD.      | SUBDIVISION                           |
| C.Y.     | CUBIC YARD                    | MON.   | MONUMENT                 | S.Y.       | SQUARE YARD                           |
| D        | DEGREE OF CURVE               | MCP.L. | MUNICIPAL                | SURF.      | SURFACE                               |
| D.       | DIRECTIONAL DISTRIBUTION      | N      | NORTH                    | T          | TRUCK PERCENTAGE                      |
| D.H.V.   | DESIGN HOUR VOLUME            | PAV'T. | PAVEMENT                 | T.         | TANGENT LENGTH OF CURVE               |
| DIS.     | DISCHARGE                     | P.C.   | POINT OF CURVATURE       | TEMP.      | TEMPORARY                             |
| E.       | EAST                          | P.I.   | POINT OF INTERSECTION    | T.P.       | TELEPHONE POLE                        |
| ELEV.    | ELEVATION                     | P.T.   | POINT OF TANGENCY        | T          | TRANSIT LINE                          |
| EMB.     | EMBANKMENT                    | P.C.C. | PORTLAND CEMENT CONCRETE | UNCL.      | UNCLASSIFIED                          |
| EXC.     | EXCAVATION                    | P.E.   | PRIVATE ENTRANCE         | V          | DESIGN SPEED                          |
| F - F    | FACE TO FACE                  | P.L.   | PROPERTY LINE            | VAR.       | VARIABLE                              |
| F.E.     | FIELD ENTRANCE                | PP     | POWER POLE               | V.C.       | VERTICAL CURVE                        |
| F.L.     | FLOW LINE                     | PROJ.  | PROJECT                  | VERT.      | VERTICAL                              |
| FT.      | FOOT ( FEET )                 | R.     | RADIUS                   | W          | WEST                                  |

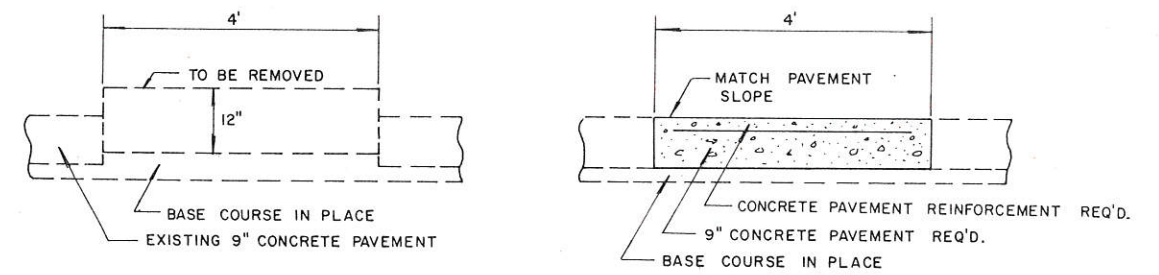
|                        |           |
|------------------------|-----------|
| STATE PROJECT NUMBER   | SHEET NO. |
| 5991-0-14              | 2.1       |
| TRAFFIC ISLAND DETAILS |           |



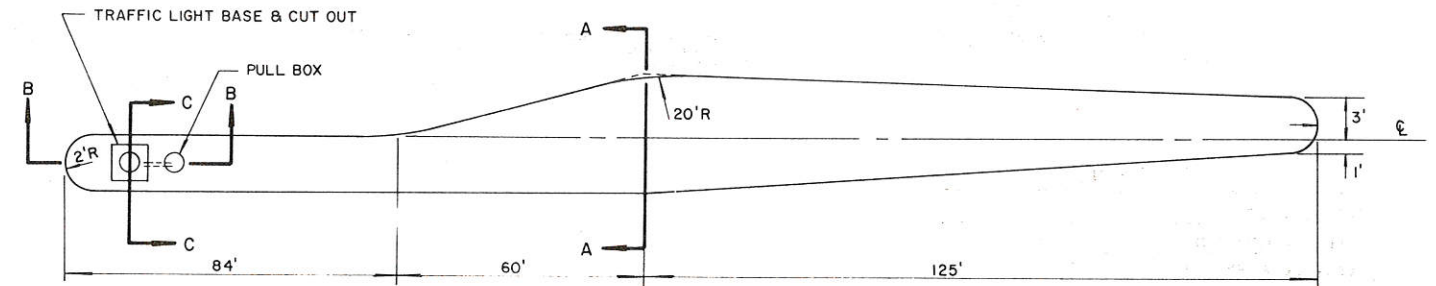
TRAFFIC ISLAND DETAILS  
STA. 90+45-93+12



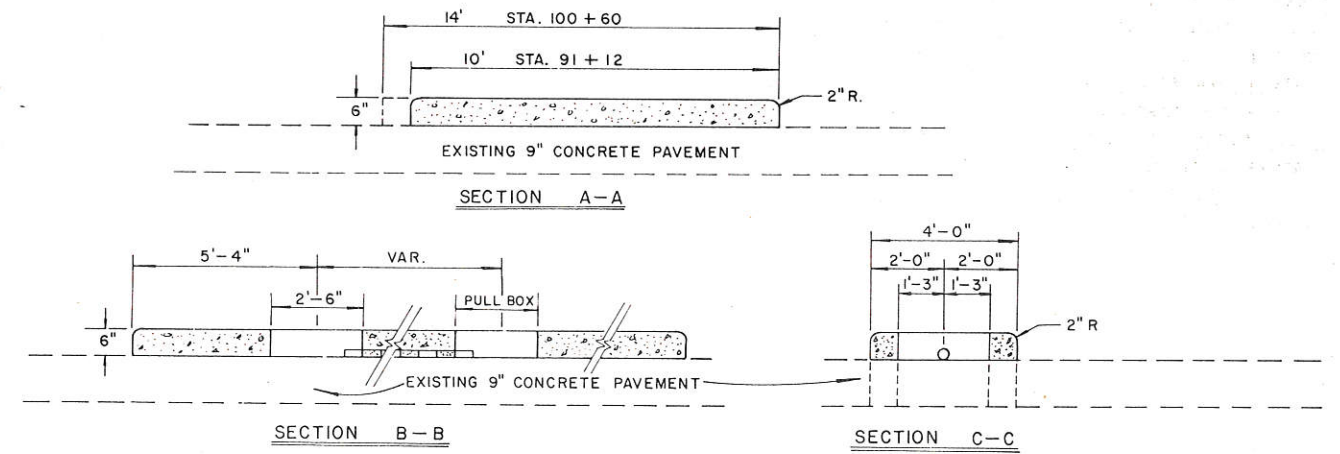
DETAILS FOR REMOVAL OF CORRUGATED MEDIAN



DETAILS FOR REMOVAL OF ISLANDS



STA. 99+16 - 101+85

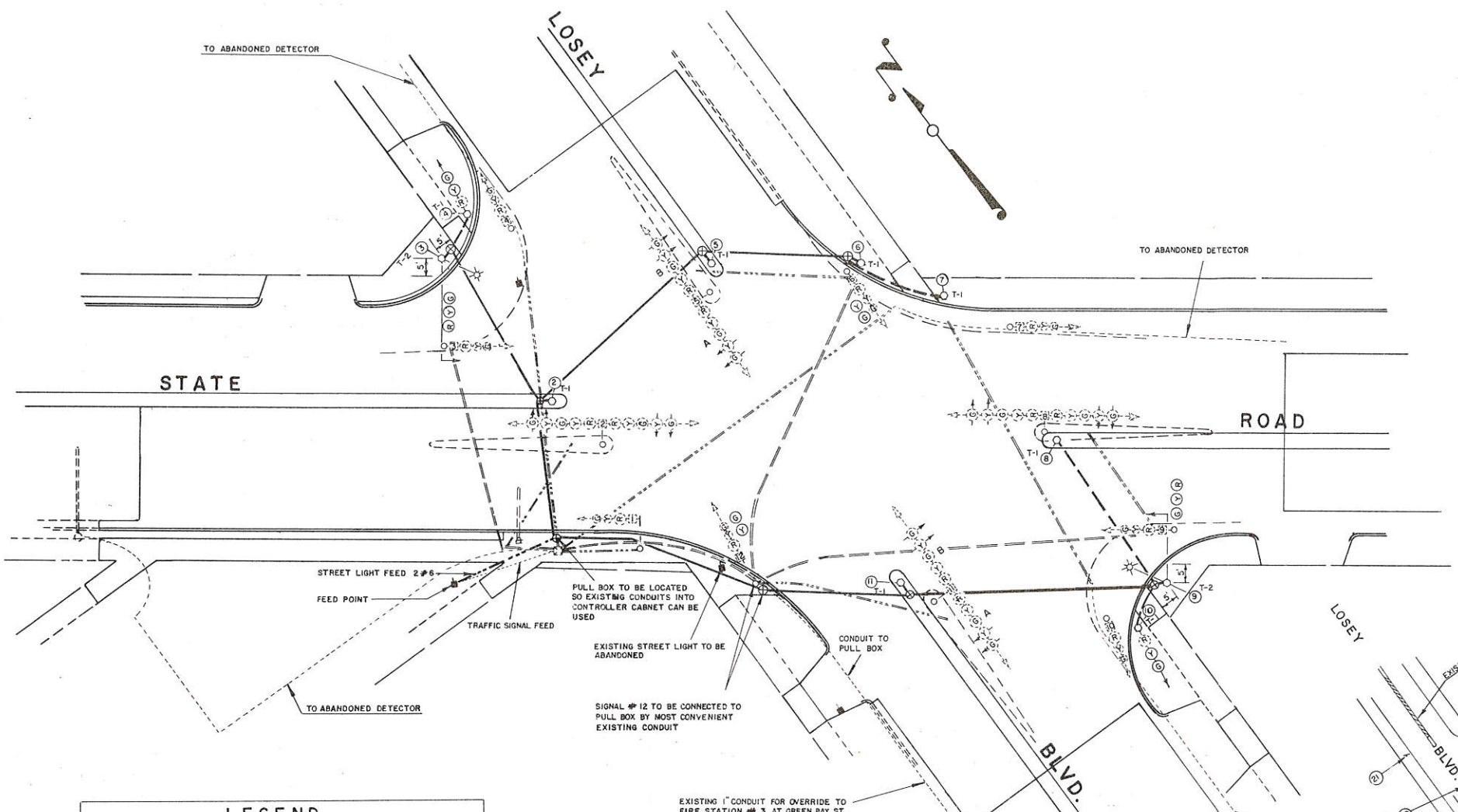




**SEQUENCE OF OPERATION**  
PRETIMED DIAL #1 5A.M. TO MIDNIGHT

| INTERVAL NO.                                | LOSEY BLVD. |         |       |       | STATE RD. |            |      | INTERVAL LENGTH SEC. | %   | DIAL READING |
|---|-------------|---------|-------|-------|-----------|------------|------|----------------------|-----|--------------|
|   | 5A, 11A     | 5B, 11B | 6, 10 | 4, 12 | 2, 8      | 1, 3, 7, 9 |      |                      |     |              |
| 1   | R           | R       | R     | R     | R         | R          | 8.0  | 10                   | 10  |              |
| 2   | R           | R       | R     | R     | R         | R          | 3.2  | 4                    | 14  |              |
| 3   | G           | G       | G     | G     | R         | R          | 23.2 | 29                   | 43  |              |
| 4   | Y           | Y       | Y     | Y     | R         | R          | 4.0  | 5                    | 48  |              |
| 5   | R           | R       | R     | R     | R         | R          | 1.6  | 2                    | 50  |              |
| 6   | R           | R       | R     | R     | R         | R          | 8.0  | 10                   | 60  |              |
| 7   | R           | R       | R     | R     | R         | R          | 3.2  | 4                    | 64  |              |
| 8   | R           | R       | R     | R     | G         | G          | 23.2 | 29                   | 93  |              |
| 9   | R           | R       | R     | R     | Y         | Y          | 4.0  | 5                    | 98  |              |
| 10  | R           | R       | R     | R     | R         | R          | 1.6  | 2                    | 100 |              |
| TOTAL CYCLE                                 |             |         |       |       |           |            |      | 80                   |     |              |
| FIRE STATION OVERRIDE                       |             |         |       |       |           |            |      |                      |     |              |
| PROGRAMMED FOR EMERGENCY FLASHING OPERATION |             |         |       |       |           |            |      |                      |     |              |
| FY FY FY FY FR FR                           |             |         |       |       |           |            |      |                      |     |              |

NOTE: YELLOW CHANGE PERIOD REQUIRED TO PRECEDE AND FOLLOW FIRE STATION PRE-EMPTION INTERVAL



**LEGEND**

|                              |           |
|------------------------------|-----------|
| SERVICE POLE                 | ⊕         |
| PULL BOX REQUIRED            | ⊕         |
| CONTROLLER EXISTING          | ⊕         |
| CONDUIT REQ'D. - 3"          | —         |
| CONDUIT EXIST. - 2"          | ---       |
| CONDUIT REQ'D. - 2"          | ---       |
| CONDUIT EXIST. - 1 1/2"      | ---       |
| CONDUIT REQ'D. - 1 1/2"      | ---       |
| CONDUIT EXIST. - 1"          | ---       |
| TYPE 1 CONC. BASE REQ'D.     | ○-○ T-1   |
| TYPE 2 CONC. BASE REQ'D.     | ○-○ T-2   |
| PEDESTAL MOUNT SIGNAL EXIST. | ○-○ R Y G |
| MAST ARM SIGNAL REQ'D.       | ○-○ R Y G |
| STREET LIGHT REQ'D.          | ⊙         |
| 12" SIGNAL FACES REQ'D.      | ○-○ R Y G |

**SIGNS IN PLACE**

| SIGN NUMBER | CODE                         |
|-------------|------------------------------|
| 3, 10, 13   | R7-4 NO STOPPING OR STANDING |
| 11          | SI-1 SCHOOL ADVANCE SIGN     |
| 51          | W3-3A SIGNAL AHEAD           |
| 10          | R3-8A LANE - USE CONTROL     |
| 24          | R2-1 SPEED LIMIT 25 M.P.H.   |

**SIGNS IN PLACE - RELOCATE**

| SIGN NUMBER | CODE   |
|-------------|--|
| 46          | D9-2 HOSPITAL  |
| 12          | R4-7 & R4-7P KEEP RIGHT  |
| 41          | R2-1 SPEED LIMIT 25 M.P.H.   |
| 48          | R2-1 SPEED LIMIT 25 M.P.H. (TRUCK DRIVERS, FOLLOW STATE AND FEDERAL HIGHWAYS ONLY) |

**SIGNS IN PLACE - REMOVE**

| SIGN NUMBER    | CODE                                    |
|----------------|---|
| 36, 39, 41, 48 | R7-1 NO PARKING ANYTIME                 |
| 2              | R7-4 NO STOPPING OR STANDING            |
| 40, 44         | R7-107 NO PARKING BUS STOP              |
| 37             | DO NOT PASS IF YELLOW LINE IN YOUR LANE |

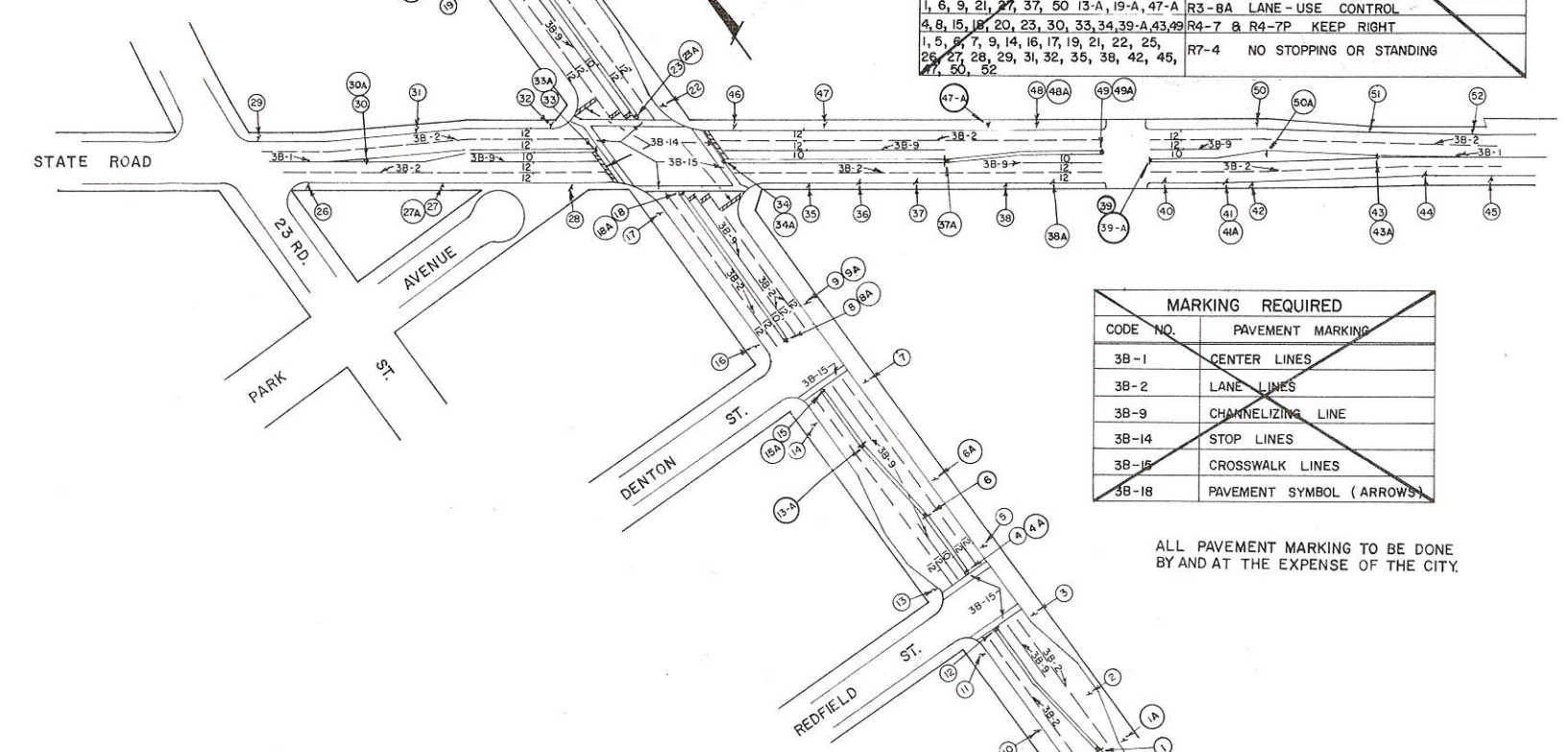
**SIGNS REQUIRED**

| SIGN NUMBER   | CODE                         |
|---|------------------------------|
| 1, 6, 9, 21, 27, 37, 50, 13-A, 19-A, 47-A   | R3-8A LANE - USE CONTROL     |
| 4, 8, 15, 18, 20, 23, 30, 33, 34, 39-A, 43, 49  | R4-7 & R4-7P KEEP RIGHT      |
| 1, 5, 6, 7, 9, 14, 16, 17, 19, 21, 22, 25, 26, 27, 28, 29, 31, 32, 35, 38, 42, 45, 47, 50, 52 | R7-4 NO STOPPING OR STANDING |

**MARKING REQUIRED**

| CODE NO. | PAVEMENT MARKING         |
|----------|--------------------------|
| 3B-1     | CENTER LINES             |
| 3B-2     | LANE LINES               |
| 3B-9     | CHANNELIZING LINE        |
| 3B-14    | STOP LINES               |
| 3B-15    | CROSSWALK LINES          |
| 3B-18    | PAVEMENT SYMBOL (ARROWS) |

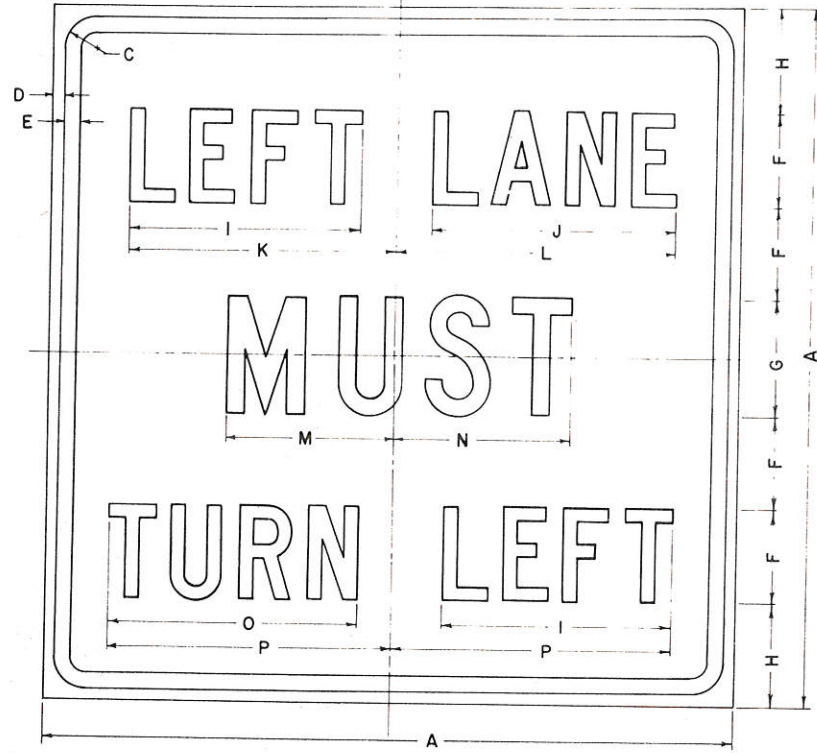
ALL PAVEMENT MARKING TO BE DONE BY AND AT THE EXPENSE OF THE CITY.



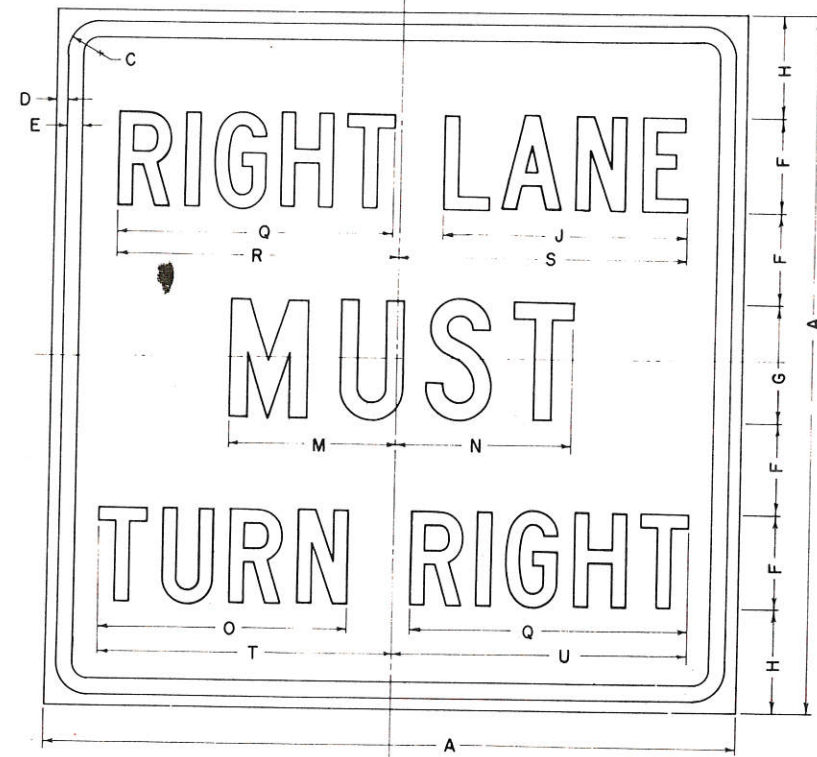


NOTES:

1. Sign is Type II
2. Color:
  - Background — Reflectorized White
  - Message — Black
3. Face Material — Reflective Sheeting
4. Lettering — Series "C"



R3-7L



R3-7R

This plate replaces OLD CODES R58, R62, R96, R107.

| SIZE       | CODE SIZE | A  | B | C     | D     | E   | F | G | H     | I  | J      | K       | L      | M     | N       | O      | P      | Q       | R       | S      | T      | U        | V | W | X | Y | Z |
|------------|-----------|----|---|-------|-------|-----|---|---|-------|----|--------|---------|--------|-------|---------|--------|--------|---------|---------|--------|--------|----------|---|---|---|---|---|
| Minimum    | 1         | 30 |   | 1 3/8 | 1 1/2 | 3/4 | 4 | 5 | 4 1/2 | 10 | 10 5/8 | 11 9/16 | 12 1/8 | 7 1/4 | 7 11/16 | 10 3/4 | 12 1/4 | 12 1/16 | 12 5/16 | 12 1/2 | 12 3/4 | 12 13/16 |   |   |   |   |   |
| Standard   | 2         | 30 |   | 1 3/8 | 1 1/2 | 3/4 | 4 | 5 | 4 1/2 | 10 | 10 5/8 | 11 9/16 | 12 1/8 | 7 1/4 | 7 11/16 | 10 3/4 | 12 1/4 | 12 1/16 | 12 5/16 | 12 1/2 | 12 3/4 | 12 13/16 |   |   |   |   |   |
| Oversize   | 3         |    |   |       |       |     |   |   |       |    |        |         |        |       |         |        |        |         |         |        |        |          |   |   |   |   |   |
| Exp-way    | 4         |    |   |       |       |     |   |   |       |    |        |         |        |       |         |        |        |         |         |        |        |          |   |   |   |   |   |
| Interstate | 5         |    |   |       |       |     |   |   |       |    |        |         |        |       |         |        |        |         |         |        |        |          |   |   |   |   |   |

STANDARD SIGNS  
R 3-7

STATE HIGHWAY COMMISSION OF WISCONSIN

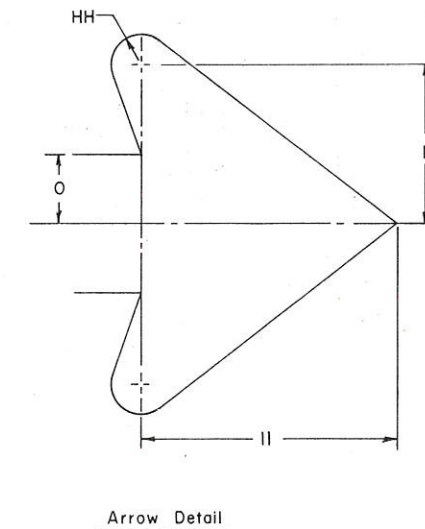
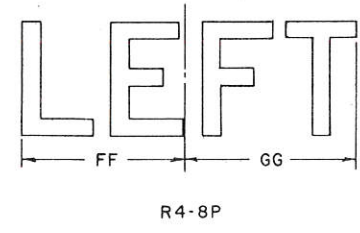
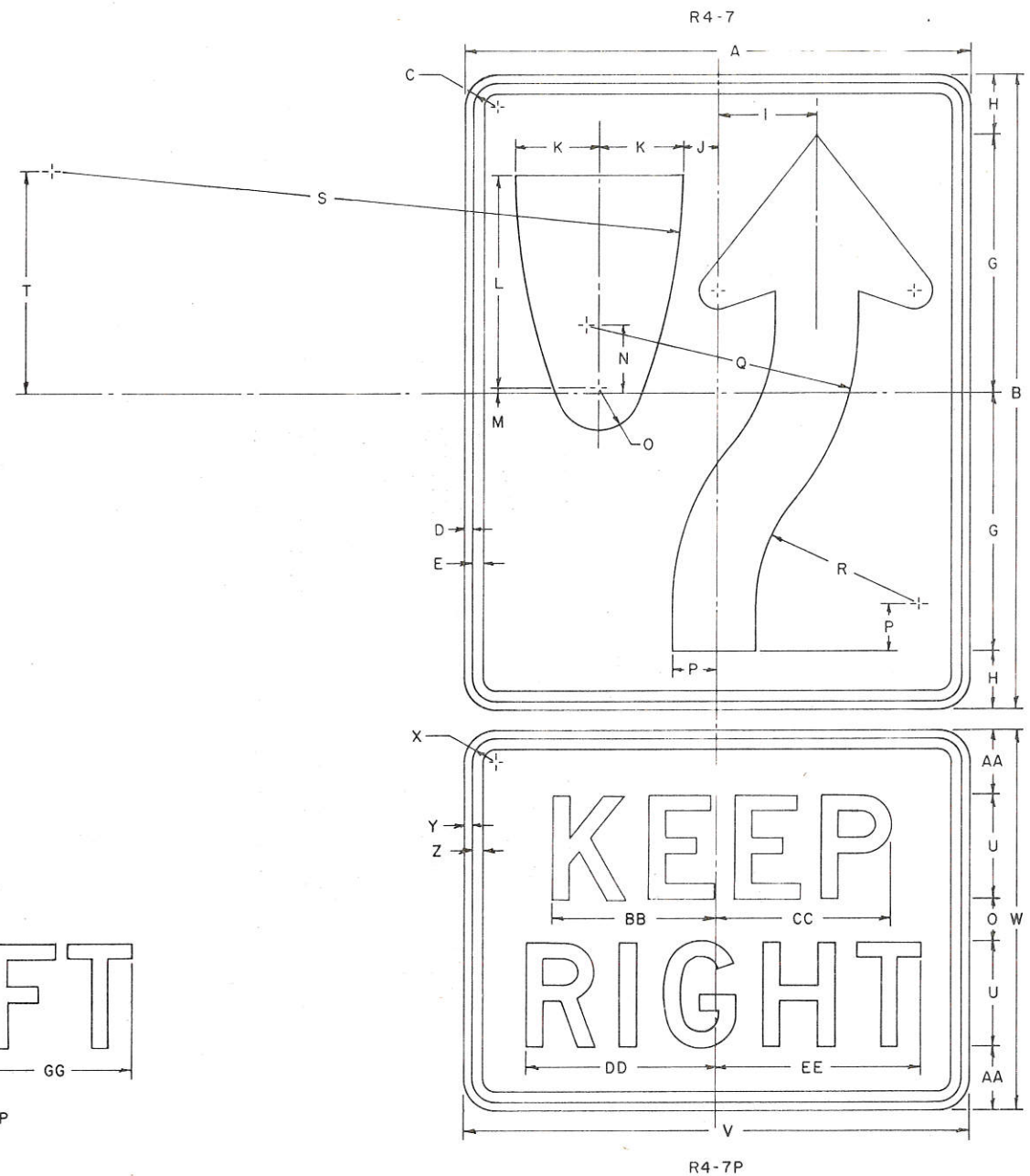
APPROVED *E. L. Roettiger*

DATE 5-16-62 PLATE NO. R3-7.1



NOTES:

- Sign is Type II
- Color:
  - Background - ReflectORIZED White
  - Message - Black
- Face Material - Reflective Sheeting
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- R4-8 is the same as R4-7 except legend is reversed, educational sign R4-8P is used instead of R4-7P.
- All letters Series "D".
- This drawing may be scaled for Standard Size only.



| SIZE     | CODE SIZE | A     | B      | C       | D      | E       | F       | G      | H       | I       | J      | K  | L       | M      | N      | O  | P      | Q       | R       | S  | T  | U  | V  | W  | X     | Y     | Z     |  |
|----------|-----------|-------|--------|---------|--------|---------|---------|--------|---------|---------|--------|----|---------|--------|--------|----|--------|---------|---------|----|----|----|----|----|-------|-------|-------|--|
| Minimum  | 1         | 18    | 24     | 1 1/8   | 3/8    | 3/8     |         | 9 3/16 | 2 13/16 | 3 1/2   | 1 1/4  | 3  | 7 5/8   | 3 1/16 | 2 7/16 | 2  | 1 9/16 | 9 11/16 | 5 7/8   | 27 | 9  | 4  | 18 | 15 | 1 1/8 | 3 3/8 | 3 3/8 |  |
| Standard | 2         | 24    | 30     | 1 1/8   | 3/8    | 1/2     |         | 12 1/4 | 2 3/4   | 4 5/8   | 1 5/8  | 4  | 10 1/8  | 1 1/4  | 3 1/4  | 2  | 2 1/8  | 12 7/8  | 7 7/8   | 36 | 12 | 5  | 24 | 18 | 1 1/8 | 3 3/8 | 1 1/2 |  |
| Oversize | 3         |       |        |         |        |         |         |        |         |         |        |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |
| Exp-way  | 4         | 36    | 48     | 1 3/4   | 1/2    | 5/8     |         | 18 3/8 | 5 5/8   | 6 15/16 | 2 7/16 | 6  | 15 3/16 | 3 3/8  | 4 7/8  | 4  | 3 3/8  | 19 5/16 | 11 3/16 | 54 | 18 | 8  | 36 | 30 | 1 3/8 | 1 1/2 | 5 8/8 |  |
| Freeway  | 5         | 48    | 60     | 2 1/4   | 3/4    | 1       |         | 24 1/2 | 5 1/2   | 9 1/4   | 3 1/4  | 8  | 20 1/4  | 1 1/2  | 6 1/2  | 4  | 4 1/4  | 25 3/4  | 15 3/4  | 72 | 24 | 10 | 48 | 36 | 1 3/4 | 1 1/2 | 5 8/8 |  |
|          |           | AA    | BB     | CC      | DD     | EE      | FF      | GG     | HH      | II      | JJ     | KK | LL      | MM     | NN     | OO | PP     | QQ      | RR      | SS | TT | UU | VV | WW | XX    | YY    | ZZ    |  |
| Minimum  | 1         | 2 1/2 | 6 3/16 | 6 19/32 | 7 3/16 | 7 13/16 | 4 29/32 | 5 1/4  | 5 5/8   | 5 1/2   | 2      |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |
| Standard | 2         | 3     | 7 3/4  | 8 1/4   | 9      | 9 3/4   | 7       | 7 1/2  | 7 7/8   | 7 3/8   | 3      |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |
| Oversize | 3         |       |        |         |        |         |         |        |         |         |        |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |
| Exp-way  | 4         | 5     | 12 3/8 | 13 3/16 | 14 3/8 | 15 5/8  | 9 13/16 | 10 1/2 | 1 5/16  | 11 1/16 | 5      |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |
| Freeway  | 5         | 6     | 15 1/2 | 16 1/2  | 18     | 19 1/2  | 14      | 15     | 1 3/4   | 14 3/4  | 5      |    |         |        |        |    |        |         |         |    |    |    |    |    |       |       |       |  |

Date Drawn - 11-19-71 Date Redrawn - 9-9-72 <sup>CS</sup>

Date Revised - 11-16-71  
7-18-72

**STANDARD SIGN**  
R4-7 & R4-8 — R4-7P & R4-8P  
Old Code: R4-8

WISCONSIN DIVISION OF HIGHWAYS

APPROVED  
*N. F. [Signature]*  
STATE HIGHWAY ENGINEER

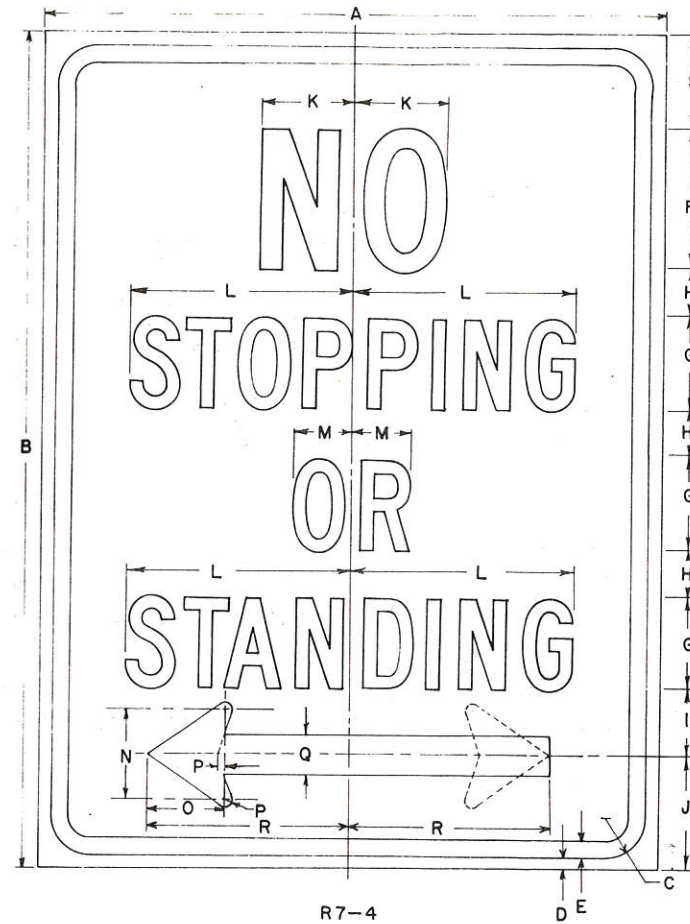
DATE 10/26/72 PLATE NO. R4-7.2

Replaces Old Plate: R4-8



NOTES:

1. Sign is Type II
2. Color:  
Background — Reflectorized White  
Message — Red
3. Face Material — Reflective Sheeting
4. All Letters Series "C"
5. R7-4L (Left Arrow)  
R7-4R (Right Arrow)  
R7-4D (Double Arrow)
6. Background Non-Reflectorized When So Specified In The Order.
7. This Drawing May Be Scaled For Standard Size Only.
8. Dimensions For Sizes Other Than Standard Size Are Not Proportional.



| SIZE       | CODE SIZE | A  | B  | C     | D   | E   | F | G | H     | I     | J     | K       | L      | M      | N     | O     | P    | Q     | R       | S     | T | U | V | W | X | Y | Z |
|------------|-----------|----|----|-------|-----|-----|---|---|-------|-------|-------|---------|--------|--------|-------|-------|------|-------|---------|-------|---|---|---|---|---|---|---|
| Minimum    | 1         | 12 | 18 | 1 1/8 | 3/8 | 3/8 | 3 | 2 | 1     | 1 1/2 | 2 1/2 | 2       | 4 7/8  | 1 1/4  | 1 3/4 | 1 1/2 | 1/8  | 3/4   | 3 7/8   | 2     |   |   |   |   |   |   |   |
| Standard   | 2         | 18 | 24 | 1 1/8 | 3/8 | 1/2 | 4 | 3 | 1 1/8 | 2     | 3 1/4 | 2 11/16 | 7 1/4  | 1 7/8  | 2 5/8 | 2 1/4 | 3/16 | 1 1/8 | 5 13/16 | 2 3/8 |   |   |   |   |   |   |   |
| Oversize   | 3         | 24 | 30 | 1 1/8 | 3/8 | 1/2 | 5 | 3 | 1 3/4 | 3     | 4 3/4 | 3 3/8   | 9 1/16 | 2 5/16 | 3 1/2 | 3     | 1/4  | 1 1/2 | 7 3/4   | 3     |   |   |   |   |   |   |   |
| Exp-way    | 4         |    |    |       |     |     |   |   |       |       |       |         |        |        |       |       |      |       |         |       |   |   |   |   |   |   |   |
| Interstate | 5         |    |    |       |     |     |   |   |       |       |       |         |        |        |       |       |      |       |         |       |   |   |   |   |   |   |   |

**STANDARD SIGN**  
**R7-4**

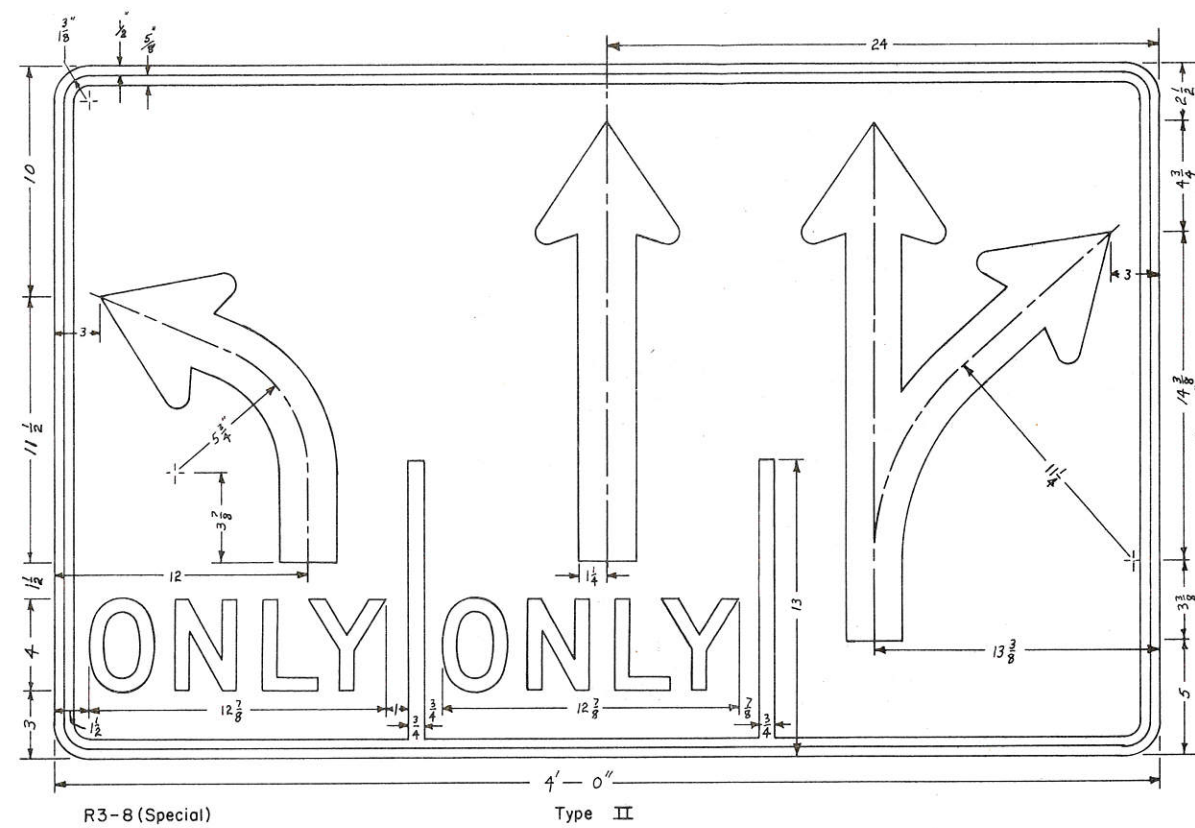
*STATE HIGHWAY COMMISSION OF WISCONSIN*

APPROVED *E. L. Rutledge*

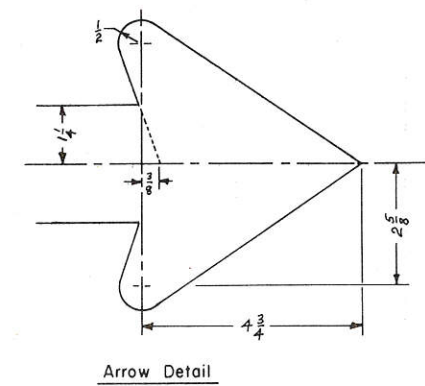
DATE 3-31-65 PLATE NO. R7-4.3



|                     |              |              |
|---------------------|--------------|--------------|
| PROJECT I. D.       | SHEET NUMBER | TOTAL SHEETS |
| 5991-0-14           | 2.6          |              |
| PROJECT DESIGNATION |              |              |



1. Background - Reflective White
2. Message - Black - Series "D"





# ESTIMATE OF QUANTITIES

CONTRACT NO. 1  
GRADING, BASE, P.C. CONCRETE SURFACING

|                      |          |
|----------------------|----------|
| STATE PROJECT NUMBER | SHEET NO |
| 5991-0-14            | 3        |

| STATION TO STATION     | NET LENGTH OF CENTER LINE | REMOVING PAVEMENT | REMOVING BITUMINOUS SURFACE | REMOVING GUTTER | REMOVING CURB & GUTTER | REMOVING CONCRETE SIDEWALK | REMOVING CATCH BASINS | REMOVING INLETS | UNCLASSIFIED EXCAVATION | CRUSHED AGGREGATE BASE COURSE | BITUMINOUS CONCRETE PAVEMENT | BITUMINOUS MATERIAL FOR SURFACE COURSE | CONCRETE PAVEMENT 6-INCH | CONCRETE PAVEMENT 9-INCH | CONCRETE DRIVEWAY | CONCRETE ALLEY | CONCRETE PAVEMENT REINFORCEMENT | CONCRETE CURB, TYPE "D" | CONCRETE GUTTER, TYPE "A" | CONCRETE CURB & GUTTER, 30-INCH TYPE "A" | CONCRETE SIDEWALK, 4-INCH | CONCRETE SIDEWALK, 6-INCH | REINFORCED CONCRETE PIPE, CL. III STORM SEWER, 12-INCH | CATCH BASINS, TYPE 1 |
|------------------------|---------------------------|-------------------|-----------------------------|-----------------|------------------------|----------------------------|-----------------------|-----------------|-------------------------|-------------------------------|------------------------------|--|--------------------------|--------------------------|-------------------|----------------|---------------------------------|-------------------------|---------------------------|--|---------------------------|---------------------------|--|----------------------|
|                        |                           | 20401             | 20402                       | 20404           | 20405                  | 20406                      | 20415                 | 20416           | 20503                   | 30403                         | 40701                        | 40702                                  | 40906                    | 40909                    | 40931             | 40932          | 40950                           | 60102                   | 60111                     | 60123                                    | 60204                     | 60206                     | 60825  | 61101                |
| UNIT                   | LIN. FT.                  | S.Y.              | S.Y.                        | L.F.            | L.F.                   | S.Y.                       | EACH                  | EACH            | C.Y.                    | C.Y.                          | TON                          | TON                                    | S.Y.                     | S.Y.                     | S.Y.              | S.Y.           | S.Y.                            | L.F.                    | L.F.                      | L.F.                                     | S.F.                      | S.F.                      | L.F.   | EACH                 |
| STA.73+56 TO STA.85+16 | 1160                      | 4980              | 350                         | 76              | 1869                   | 408                        | 4                     | 2               | 1936                    | 1300                          | 158                          | 9.5                                    | 341                      | 5151                     | 421               | 32             | 341                             | 1928                    | 472                       | 842                                      | 4475                      | 312                       | 74   | 1                    |
| <b>TOTAL</b>           | <b>1160</b>               | <b>4980</b>       | <b>350</b>                  | <b>76</b>       | <b>1869</b>            | <b>408</b>                 | <b>4</b>              | <b>2</b>        | <b>1936</b>             | <b>1300</b>                   | <b>158</b>                   | <b>9.5</b>                             | <b>341</b>               | <b>5151</b>              | <b>421</b>        | <b>32</b>      | <b>341</b>                      | <b>1928</b>             | <b>472</b>                | <b>842</b>                               | <b>4475</b>               | <b>312</b>                | <b>74</b>  | <b>1</b>             |

| INLETS TYPE 1 | CATCH BASIN COVERS, TYPE B | ADJUSTING CATCH BASIN COVERS | ADJUSTING MANHOLE COVERS | ADJUSTING INLET COVERS | METAL CONDUIT 1-1/2" | METAL CONDUIT 2-INCH | METAL CONDUIT 3-INCH | TREES MAPLE AMUR 4-5 FT. HT. | SHRUBS HONEYSUCKLE BLUELEAF 3-4 FT. HT. | WOOD POSTS 4 x 4" x 12' | WOOD POSTS 4 x 4" x 14' | SIGNS TYPE II, REFLECTIVE | MOVING SIGNS | REMOVING SIGNS | REMOVING CONCRETE ISLANDS | REMOVING CORRUGATED MEDIAN | FURNISHING, INSTALLING & RELOCATING TRAFFIC CONTROL DEVICES. | CONCRETE SAFETY ISLANDS | TOPSOIL | SODDING | WOOD POSTS 4 X 4" X 16' |
|---------------|----------------------------|------------------------------|--------------------------|------------------------|----------------------|----------------------|----------------------|------------------------------|---|-------------------------|-------------------------|---------------------------|--------------|----------------|---------------------------|----------------------------|--|-------------------------|---------|---------|-------------------------|
| 61121         | 61132                      | 61181                        | 61182                    | 61183                  | 61312                | 61313                | 61315                | 63201                        | 63261                                   | 63402                   | 63403                   | 63702                     | 63801        | 63805          | 90001                     | 90002                      | 90003  | 90004                   | 62501   | 63101   | 63404                   |
| EACH          | EACH                       | EACH                         | EACH                     | EACH                   | L.F.                 | L.F.                 | L.F.                 | EACH                         | EACH                                    | EACH                    | EACH                    | S.F.                      | L.S.         | L.S.           | S.Y.                      | S.Y.                       | L.S.   | S.F.                    | S.Y.    | S.Y.    | EACH                    |
| 2             | 1                          | 1                            | 11                       | 2                      | 52                   | 152                  | 363                  | 3                            | 15                                      | 28                      | 22                      | 242.25                    | 1            | 1              | 110                       | 258                        | 1  | 8541                    | 604     | 604     | 2                       |
| 2             | 1                          | 1                            | 11                       | 2                      | 52                   | 152                  | 363                  | 3                            | 15                                      | 28                      | 22                      | 242.25                    | 1            | 1              | 110                       | 258                        | 1  | 8541                    | 604     | 604     | 2                       |

GENERAL NOTES

1. WHEN THE QUANTITY OF THE ITEM OF BASE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.
2. TOPSOIL SHALL BE PLACED AS SHOWN ON THE PLANS TO AN APPROXIMATE DEPTH OF FOUR (4) INCHES AT THE TIME OF PLACEMENT.
3. ALL COORDINATES SHOWN ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COORDINATE SYSTEM SOUTH ZONE.
4. THE EXACT LOCATION OF PRIVATE ENTRANCES TO BE DETERMINED IN THE FIELD BY THE ENGINEER.
5. ADD 600.99 FEET TO CONVERT FROM CITY DATUM TO USGS (1929 ADJ).
6. SAW CUTS DENOTED ON THE PLANS SHALL BE INCLUDED IN THE COST OF PAVEMENT REMOVAL.
7. LOCATION OF GAS LINES AS SHOWN ON PLAN ARE APPROXIMATE.

UTILITIES INVOLVED

CITY OF LA CROSSE

SANITARY SEWER  
STORM SEWER  
WATER MAIN

NORTHERN STATES POWER COMPANY

ELECTRIC DISTRIBUTION  
GAS DISTRIBUTION

LA CROSSE TELEPHONE COMPANY

TELEPHONE CONDUIT  
AERIAL CABLE

STANDARD DETAIL DRAWINGS

- 8A5-1 - CATCH BASIN, MANHOLE & INLET COVERS
- 8A6-1 - CATCH BASIN, TYPE 1 & 2
- 8C4-1 - INLETS, TYPE 1 & 3
- 8D1-2 - CONCRETE CURB, GUTTER, COMBINATION CURB & GUTTER
- 8D5-1 - CURB RAMPS FOR HANDICAPPED PERSONS
- 9B2-1 - METAL CONDUIT & FIBER CONDUIT
- 9B3-2 - TRAFFIC SIGNAL & TRAFFIC COUNTER DETAILS
- 13A1-2 - CONCRETE PAVEMENT REINFORCEMENT
- 13C1-2 - LONGITUDINAL JOINTS - CONCRETE PAVEMENT
- 13C4-3 - TRANSVERSE JOINTS IN NON-REINFORCED CONCRETE PAVEMENT
- 15C1-4 - CONSTRUCTION BARRICADES & STANDARD SIGNS



REMOVING PAVEMENT

Losey Boulevard

| Sta.    | Sta.  | Location     | S.Y. |
|---------|-------|--------------|------|
| 80+48.5 | 82+69 | C/L          | 1477 |
| 81+25   |       | Driveway Lt. | 37   |
| 82+80   |       | Driveway Lt. | 56   |
| 83+10   |       | Lt.          | 51   |
| 79+75   |       | Rt.          | 49   |
| 80+45   |       | Rt.          | 39   |

State Road

|            |          |              |      |
|------------|----------|--------------|------|
| 90+10      | 91+93    | Lt.          | 305  |
| 91+93      | 93+47    | C/L          | 930  |
| 94+20 BK   | 95+40 AH | C/L          | 1075 |
| 91+40      | 93+25    | Lt.          | 454  |
| 91+93      | 93+00    | Rt.          | 86   |
| 94+00      |          | Rad. Lt.     | 35   |
| 94+80      |          | Rad. Rt.     | 40   |
| Cul de Sac |          | Driveway Lt. | 42   |
| Cul de Sac |          | " Rt.        | 47   |
| 91+00      |          | " Lt.        | 6    |
| 95+40      |          | " Rt.        | 53   |
| 95+65      |          | " Rt.        | 11   |
| 95+80      |          | " Rt.        | 53   |
| 97+25      |          | " Lt.        | 57   |
| 99+75      |          | " Rt.        | 25   |
| 100+00     |          | " Lt.        | 52   |

REMOVING CATCH BASINS

State Road

| Sta.  | Location | Each |
|-------|----------|------|
| 91+89 | Rt.      | 1    |
| 92+73 | Rt.      | 1    |
| 94+43 | Lt.      | 1    |
| 94+84 | Rt.      | 1    |

REMOVING INLETS

State Road

| Sta.   | Location | Each |
|--------|----------|------|
| 100+30 | Lt.      | 1    |
| 100+30 | Rt.      | 1    |

REMOVING CONCRETE SIDEWALK

Losey Blvd.

| Sta.  | Sta.  | Location | S.Y. |
|-------|-------|----------|------|
| 77+12 | 77+20 | Lt.      | 11   |
| 80+16 | 80+32 | Rt.      | 11   |
| 80+65 | 80+89 | Rt.      | 16   |
| 80+95 | 81+08 | Lt.      | 9    |
| 81+40 | 81+58 | Lt.      | 19   |
| 81+66 | 81+87 | Rt.      | 14   |
| 82+40 | 82+71 | Lt.      | 21   |
| 82+98 | 83+21 | Lt.      | 15   |

State Road

|                      |       |     |     |
|----------------------|-------|-----|-----|
| Park Ave. Cul de Sac |       | Rt. | 11  |
| " "                  | " "   | Lt. | 19  |
| 89+83                | 91+58 | Lt. | 117 |
| 91+90                | 92+45 | Lt. | 37  |
| 92+70                | 92+94 | Lt. | 17  |
| 92+80                | 93+02 | Rt. | 15  |
| 94+94                | 95+06 | Rt. | 8   |
| 95+06                | 95+24 | Rt. | 16  |
| 95+55                | 95+76 | Rt. | 20  |
| 98+77                | 99+11 | Rt. | 32  |

CRUSHED AGGREGATE BASE COURSE

Losey Blvd.

| Sta.      | Sta.    | Location     | C.Y. |
|-----------|---------|--------------|------|
| 80+48.6   | 82+68.5 | C/L          | 236  |
| 73+56     | 75+16   | Bus Stop Rt. | 23   |
| 76+00     | 77+60   | Bus Stop Lt. | 23   |
| 82+22     | 84+31   | Bus Stop Rt. | 34   |
| Driveways |         |              | 17   |
| 79+63     | 80+48.5 | Curb         | 6    |

State Road

|               |        |          |     |
|---------------|--------|----------|-----|
| 90+10         | 101+80 | C/L      | 763 |
| Park Avenue   |        | C/L      | 46  |
| Driveways     |        |          | 36  |
| 90+00         | 103+28 | Curb Lt. | 30  |
| 91+81         | 93+85  | Curb Rt. | 23  |
| Park Avenue   |        | Curb     | 10  |
| Undistributed |        |          | 53  |

REMOVING BITUMINOUS SURFACE

State Road

| Sta. | Sta.        | Location | S.Y. |
|------|-------------|----------|------|
|      | Park Avenue | C/L      | 350  |

REMOVING GUTTER

State Road

| Sta.  | Sta.    | Location | L.F. |
|-------|---------|----------|------|
| 96+61 | 96+82   | Rt.      | 21   |
| 99+86 | 100+22  | Rt.      | 35   |
| 80+29 | 80+48.5 | Rt.      | 20   |

REMOVING CURB AND GUTTER

Losey Blvd.

| Sta.  | Sta.  | Location | L.F. |
|-------|-------|----------|------|
| 73+56 | 75+16 | Rt.      | 160  |
| 76+90 | 77+60 | Lt.      | 160  |
| 79+92 | 80+29 | Rt.      | 37   |
| 80+91 | 81+11 | Lt.      | 20   |
| 82+69 | 84+31 | Rt.      | 162  |

State Road

|                            |        |     |     |
|----------------------------|--------|-----|-----|
| Park Ave. Cul de Sac       |        | Rt. | 77  |
| Park Ave. & State Rd. Rad. |        | Lt. | 20  |
| 90+00                      | 90+10  | Lt. | 10  |
| 95+40.5                    | 96+99  | Lt. | 159 |
| 95+57                      | 95+74  | Rt. | 17  |
| 96+09                      | 96+61  | Rt. | 52  |
| 96+82                      | 97+45  | Rt. | 63  |
| 97+45                      | 99+64  | Rt. | 219 |
| 97+45                      | 100+84 | Lt. | 339 |
| 100+22                     | 102+00 | Rt. | 178 |
| 101+32                     | 103+28 | Lt. | 196 |

REMOVING CONCRETE ISLANDS

Losey Blvd.

| Sta.    | Sta.  | Location | S.Y. |
|---------|-------|----------|------|
| 73+94.5 | 75+13 | C/L      | 85   |
| 75+83   | 75+98 | C/L      | 6    |
| 78+31   | 78+46 | C/L      | 6    |
| 79+16   | 79+31 | C/L      | 6    |
| 85+00   | 85+15 | C/L      | 7    |

REMOVING CORRUGATED MEDIAN

Losey Blvd.

| Sta.  | Sta.    | Location | S.Y. |
|-------|---------|----------|------|
| 75+98 | 78+31   | C/L      | 103  |
| 79+31 | 80+48.5 | C/L      | 52   |
| 82+69 | 85+00   | C/L      | 103  |

UNCLASSIFIED EXCAVATION

State Road

| Sta.  | Sta.   | Location | C.Y. |
|-------|--------|----------|------|
| 90+10 | 103+00 | C/L      | 1526 |

Losey Blvd.

|         |         |              |     |
|---------|---------|--------------|-----|
| 73+56   | 75+16   | Bus Stop Rt. | 71  |
| 76+00   | 77+60   | " Lt.        | 71  |
| 82+22   | 84+31   | " Rt.        | 103 |
| 80+48.6 | 81+30   | C/L          | 96  |
| 82+07   | 82+68.5 | C/L          | 69  |

BITUMINOUS CONCRETE PAVEMENT

Losey Blvd.

| Sta.  | Sta.    | Location | Ton | Material    |
|-------|---------|----------|-----|-------------|
| 79+90 | 80+14   | Rt.      | 5.5 | Tons<br>0.4 |
| 83+23 | 83+25.5 | Lt.      | 0.6 |             |

State Road

|                        |        |     |       |     |
|------------------------|--------|-----|-------|-----|
| Park Avenue Cul de Sac |        |     | 48    | 2.9 |
| 94+43                  | 103+29 | Lt. | 103.9 | 6.2 |

6" CONCRETE PAVEMENT

| Sta.  | Sta.  | Location       | S.Y. |
|-------|-------|----------------|------|
| 73+56 | 75+16 | Rt. - Bus Stop | 106  |
| 76+00 | 77+60 | Lt. - Bus Stop | 106  |
| 82+22 | 84+31 | Rt. - Bus Stop | 129  |



CONCRETE PAVEMENT 9"

| Sta.                | Sta.    | Location | S.Y. |
|---------------------|---------|----------|------|
| 80+48.5             | 82+68.5 | C/L      | 1418 |
| <u>Losely Blvd.</u> |         |          |      |
| 90+00               | 91+93   | Lt.      | 463  |
| 91+93               | 95+41   | C/L      | 1961 |
| 95+41               | 102+00  | Rt.      | 408  |
| 95+41               | 103+28  | Lt.      | 901  |

CONCRETE DRIVEWAYS

| Sta.                  | Sta.   | Location | S.Y. |
|-----------------------|--------|----------|------|
| <u>Losely Blvd.</u>   |        |          |      |
| 79+65                 | 79+90  | Rt.      | 35   |
| 80+14                 | 80+39  | Rt.      | 35   |
| 80+94                 | 81+09  | Lt.      | 21   |
| 82+97                 | 83+22  | Lt.      | 35   |
| <u>State Road</u>     |        |          |      |
| 90+90                 | 91+16  | Lt.      | 26   |
| 91+48                 | 91+74  | Lt.      | 26   |
| 92+26                 | 92+52  | Lt.      | 26   |
| Park Ave.-Cul de Sac  |        | Lt.      | 23   |
| Park Ave.-Cul de Sac  |        | Rt.      | 12   |
| 95+33                 | 95+59  | Rt.      | 26   |
| 95+73                 | 95+99  | Rt.      | 26   |
| 96+55                 | 96+81  | Rt.      | 26   |
| 98+82                 | 99+22  | Rt.      | 40   |
| 98+82                 | 99+22  | Lt.      | 40   |
| 101+83                | 102+26 | Lt.      | 24   |
| <u>CONCRETE ALLEY</u> |        |          |      |
| Sta.                  | Sta.   | Location | S.Y. |
| Park Ave.-Cul de Sac  |        | Rt.      | 32   |

CONCRETE CURB, Type "D"

| Sta.              | Sta.   | Location | L.F. |
|-------------------|--------|----------|------|
| <u>State Road</u> |        |          |      |
| 90+00             | 90+87  | Lt.      | 90   |
| 91+18             | 91+45  | Lt.      | 31   |
| 91+75             | 92+24  | Lt.      | 45   |
| 91+81             | 93+25  | Rt.      | 144  |
| N.W. Corner       |        |          | 75   |
| S.W. Corner       |        |          | 69   |
| N.E. Corner       |        |          | 69   |
| S.E. Corner       |        |          | 75   |
| 94+61 AH          | 97+50  | Lt.      | 290  |
| 95+61             | 95+72  | Rt.      | 12   |
| 96+01             | 96+53  | Rt.      | 56   |
| 96+83             | 97+50  | Rt.      | 69   |
| 97+50             | 98+71  | Rt.      | 121  |
| 99+13             | 102+00 | Rt.      | 287  |
| 97+50             | 98+71  | Lt.      | 121  |
| 99+13             | 101+93 | Lt.      | 283  |
| 102+37            | 103+28 | Lt.      | 91   |

CONCRETE GUTTER, TYPE "A"

| Sta.                | Sta.  | Location | L.F. |
|---------------------|-------|----------|------|
| <u>Losely Blvd.</u> |       |          |      |
| 73+56               | 75+16 | Rt.      | 142  |
| 76+00               | 77+60 | Lt.      | 142  |
| 82+22               | 84+31 | Rt.      | 188  |

CONCRETE CURB & GUTTER, 30-Inch, TYPE "A"

| Sta.                    | Sta.  | Location | L.F. |
|-------------------------|-------|----------|------|
| <u>Losely Boulevard</u> |       |          |      |
| 73+56                   | 75+16 | Rt.      | 163  |
| 76+00                   | 77+60 | Lt.      | 163  |
| 79+62                   | 80+48 | Rt.      | 86   |
| 80+91                   | 81+11 | Lt.      | 20   |
| 82+22                   | 84+31 | Rt.      | 211  |
| 82+90                   | 83+26 | Lt.      | 36   |
| <u>State Road</u>       |       |          |      |
| Park Ave. Cul de Sac    |       |          | 163  |

CONCRETE PAVEMENT REINFORCEMENT

| Sta.  | Sta.  | Location     | S.Y. |
|-------|-------|--------------|------|
| 73+56 | 75+16 | Rt. Bus Stop | 106  |
| 76+00 | 77+60 | Lt. Bus Stop | 106  |
| 82+22 | 84+31 | Rt. Bus Stop | 129  |

CONCRETE SIDEWALK - 6"

| Sta.                | Sta.  | Location | S.F. |
|---------------------|-------|----------|------|
| <u>Losely Blvd.</u> |       |          |      |
| 80+16               | 80+32 | Rt.      | 96   |
| 80+95               | 81+08 | Lt.      | 78   |
| 82+98               | 83+21 | Lt.      | 138  |

CONCRETE SIDEWALK, 4"

| Sta.                | Sta.  | Location | S.F. |
|---------------------|-------|----------|------|
| <u>Losely Blvd.</u> |       |          |      |
| 80+65               | 80+72 | Rt.      | 51   |
| 81+40               | 81+60 | Lt.      | 207  |
| 81+78               | 81+87 | Rt.      | 65   |
| 82+68               | 82+91 | Lt.      | 324  |

State Road

| Sta.                   | Sta.  | Location | S.F.   |
|------------------------|-------|----------|--------|
| <u>State Road</u>      |       |          |        |
| Park Avenue Cul de Sac |       | Lt.      | 96     |
| 89+93                  | 90+90 | Lt.      | 909.5  |
| 91+16                  | 91+48 | Lt.      | 272    |
| 91+73                  | 92+26 | Lt.      | 399.5  |
| 91+81                  | 93+42 | Rt.      | 1368.5 |
| 92+52                  | 92+73 | Lt.      | 178.5  |
| N.W. Corner            |       | Lt.      | 201.   |
| S.E. Corner            |       | Rt.      | 180.0  |
| 95+20                  | 95+33 | Rt.      | 104.0  |
| 95+59                  | 95+73 | Rt.      | 119.0  |

REINFORCED CONCRETE PIPE, CL. III - STORM SEWER, 12"

| Sta.                 | Sta. | Location | L.F. |
|----------------------|------|----------|------|
| <u>State Road</u>    |      |          |      |
| Park Ave. Cul de Sac |      | --       | 60   |
| 100+30               |      | Lt.      | 10   |
| 100+30               |      | Rt.      | 4    |

CATCH BASINS, TYPE 1

| Station              | Location | Each | Ma. | Cover (Type) |
|----------------------|----------|------|-----|--------------|
| <u>State Road</u>    |          |      |     |              |
| Park Ave. Cul de Sac | C/L      | 1    | 1   | A (Salvaged) |
| 75+30                | Rt.      | --   | --  | B            |

INLETS, TYPE 1

| Station           | Location | Each | Adjusting Cover (Type A) |
|-------------------|----------|------|--------------------------|
| <u>State Road</u> |          |      |                          |
| 100+30            | Rt.      | 1    | Salvaged                 |
| 100+30            | Lt.      | 1    | Salvaged                 |

ADJUSTING MANHOLE COVERS

| Station             | Location | Each |
|---------------------|----------|------|
| <u>Losely Blvd.</u> |          |      |
| 81+22               | Rt.      | 1    |
| 81+38               | Lt.      | 1    |
| 81+45               | Lt.      | 1    |
| 81+70               | Rt.      | 1    |
| 83+02               | Lt.      | 1    |
| <u>State Road</u>   |          |      |
| 91+15               | Lt.      | 1    |
| 92+56               | Lt.      | 1    |
| 92+84               | Rt.      | 1    |
| 94+40               | Lt.      | 1    |
| 96+35               | Lt.      | 1    |
| 98+37               | Lt.      | 1    |

ADJUSTING CATCH BASIN COVERS

| Station              | Each |
|----------------------|------|
| Park Ave. Cul de Sac | 1    |

SIGNS AND POSTS

| Sign No. | Code                                       | Size    | Sign Area Type II | Same Post as Sign No. | 4x4x12' | Wood Posts 4x4x14' | 4x4x16' | Remarks              |
|----------|--|---------|-------------------|-----------------------|---------|--------------------|---------|----------------------|
| 1        | R3-7L                                      | 30"x30" | 6.25              |                       | 1       |                    |         |                      |
| 1A       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 2        | R7-4                                       | --      | --                |                       |         |                    |         | Remove               |
| 3        | R7-4                                       | --      | --                |                       |         |                    |         | To remain in place   |
| 4        | R4-7                                       | 24"x30" | 5.0               |                       |         | 1                  |         |                      |
| 4A       | R4-7P                                      | 24"x18" | 3.0               | 4                     |         |                    |         |                      |
| 5        | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 6        | R3-7L                                      | 30"x30" | 6.25              |                       | 1       |                    |         |                      |
| 6A       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 7        | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 8        | R4-7                                       | 24"x30" | 5.0               |                       |         | 1                  |         |                      |
| 8A       | R4-7P                                      | 24"x18" | 3.0               | 8                     |         |                    |         |                      |
| 9        | R3-8                                       | 30"x48" | 10.0              |                       | 1       |                    |         |                      |
| 9A       | R7-4D                                      | 18"x24" | 3.0               | 9                     |         |                    |         |                      |
| 10       | R7-4 & R3-8                                | --      | --                |                       |         |                    |         |                      |
| 11       | S1-1                                       | --      | --                |                       |         |                    |         |                      |
| 12       | R4-7 & R4-7P                               | --      | --                |                       |         |                    |         |                      |
| 13       | R7-4                                       | --      | --                |                       |         |                    |         |                      |
| 13A      | R3-7L                                      | 30"x30" | 6.25              |                       | 1       |                    |         |                      |
| 14       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 15       | R4-7                                       | 24"x30" | 5.0               |                       |         | 1                  |         |                      |
| 15A      | R4-7P                                      | 24"x18" | 3.0               | 15                    |         |                    |         |                      |
| 16       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 17       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 18       | R4-7                                       | 24"x30" | 5.0               |                       |         |                    |         | Mount on signal post |
| 18A      | R4-7P                                      | 24"x18" | 3.0               | 18                    |         |                    |         |                      |
| 19       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 19A      | R3-8                                       | 30"x48" | 10.0              |                       |         |                    |         |                      |
| 20       | R4-7                                       | 24"x30" | 5.0               |                       | 2       |                    |         |                      |
| 20A      | R4-7P                                      | 24"x18" | 3.0               | 20                    | 1       |                    |         |                      |
| 21       | R7-4L                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 22       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 23       | R4-7                                       | 24"x30" | 5.0               |                       |         |                    |         | Mount on signal post |
| 23A      | R4-7P                                      | 24"x18" | 3.0               | 23                    |         |                    |         |                      |
| 24       | R2-1                                       | --      | --                |                       |         |                    |         |                      |
| 25       | R7-4R                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 26       | R7-4L                                      | 18"x24" | 3.0               |                       | 1       |                    |         | To remain in place   |
| 27       | R3-8                                       | 30"x48" | 10.0              |                       |         |                    |         |                      |
| 27A      | R7-4D                                      | 18"x24" | 3.0               | 27                    | 1       |                    |         |                      |
| 28       | R7-4D                                      | 18"x24" | 3.0               |                       |         |                    |         |                      |
| 29       | R7-4R                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 30       | R4-7                                       | 24"x30" | 5.0               |                       |         | 1                  |         |                      |
| 30A      | R4-7P                                      | 24"x18" | 3.0               |                       |         |                    |         | Mount on signal post |
| 31       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 32       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 33       | R4-7                                       | 24"x30" | 5.0               |                       |         |                    |         | Mount on signal post |
| 33A      | R4-7P                                      | 24"x18" | 3.0               | 33                    |         |                    |         |                      |
| 34       | R4-7                                       | 24"x30" | 5.0               |                       | 1       |                    |         |                      |
| 34A      | R4-7P                                      | 24"x18" | 3.0               | 34                    |         |                    |         | Mount on signal post |
| 35       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 36       | R7-1                                       | --      | --                |                       |         |                    |         |                      |
| 37       | Do Not Pass If Yellow Line is in your lane |         |                   |                       |         |                    |         |                      |
| 37A      | R3-7L                                      | 30"x30" | 6.25              |                       |         |                    |         | Remove               |
| 38       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         | Remove               |
| 38A      | R7-1                                       | --      | --                |                       |         |                    |         |                      |
| 39       | R4-7                                       | 24"x30" | 5.0               |                       |         | 1                  |         | Remove               |
| 39A      | R4-7P                                      | 24"x18" | 3.0               | 39                    |         |                    |         |                      |
| 40       | R7-107                                     | --      | --                |                       |         |                    |         |                      |
| 41       | R2-1                                       | --      | --                |                       |         |                    |         |                      |
| 41A      | R7-1                                       | --      | --                |                       |         |                    |         |                      |
| 42       | R7-4D                                      | 18"x24" | 3.0               |                       |         |                    |         | Remove               |
| 43       | R4-7                                       | 24"x30" | 5.0               |                       | 1       |                    |         | Relocate             |
| 43A      | R4-7P                                      | 24"x18" | 3.0               | 43                    |         |                    |         | Remove               |
| 44       | R7-107                                     | --      | --                |                       |         |                    |         |                      |
| 45       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         | Remove               |
| 46       | D9-2                                       | --      | --                |                       |         |                    |         | Relocate             |
| 47       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |
| 47A      | R3-8                                       | 30"x48" | 10.0              |                       |         |                    |         |                      |
| 48       | Notice to Truckers                         |         |                   |                       |         |                    |         |                      |
| 48A      | R7-1                                       | --      | --                |                       |         |                    |         | Relocate             |
| 49       | R4-7                                       | 24"x30" | 5.0               |                       |         |                    |         | Remove               |
| 49A      | R4-7P                                      | 24"x18" | 3.0               | 49                    |         |                    |         |                      |
| 50       | R7-4D                                      | 18"x24" | 3.0               |                       |         |                    |         |                      |
| 50A      | R3-7L                                      | 30"x30" | 6.25              |                       | 1       |                    |         |                      |
| 51       | W3-3A                                      | --      | --                |                       | 1       |                    |         | To remain in place   |
| 52       | R7-4D                                      | 18"x24" | 3.0               |                       | 1       |                    |         |                      |

M 4202(001)/5991-0-14



SODDING

| <u>Losey Blvd.</u>          |                 | <u>State Road</u> |                       |
|-----------------------------|-----------------|-------------------|-----------------------|
| <u>Sta.</u>                 | <u>Location</u> | <u>S.Y.</u>       | <u>Topsoil (S.Y.)</u> |
| 73+56                       | Rt.             | 83                | 83                    |
| 76+00                       | Lt.             | 83                | 83                    |
| 80+40                       | Rt.             | 33                | 33                    |
| 81+09                       | Lt.             | 36                | 36                    |
| 82+22                       | Rt.             | 90                | 90                    |
| <u>Park Ave. Cul de Sac</u> |                 |                   |                       |
| 96+81                       | Rt.             | 164               | 164                   |
|                             |                 | 115               | 115                   |

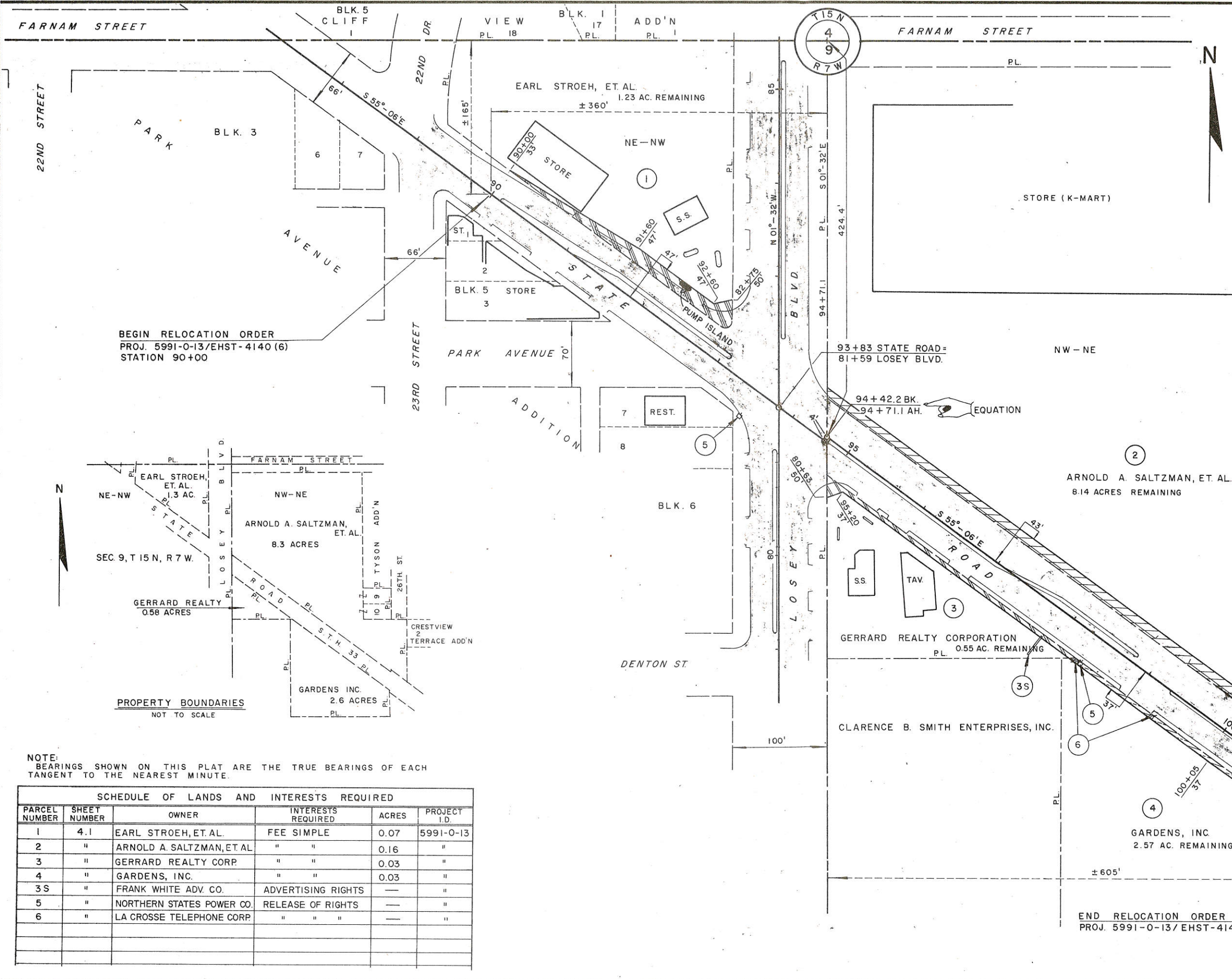
CONCRETE SAFETY ISLANDS

| <u>Losey Blvd.</u> |                 | <u>State Road</u> |  |
|--------------------|-----------------|-------------------|--|
| <u>Sta.</u>        | <u>Location</u> | <u>Sq. Ft.</u>    |  |
| 73+95              | C/L             | 486               |  |
| 75+82              | C/L             | 1062              |  |
| 79+15              | C/L             | 819               |  |
| 82+15              | C/L             | 1206              |  |
| <u>State Road</u>  |                 |                   |  |
| 90+45              | C/L             | 1404              |  |
| 94+73 AH           | C/L             | 1575              |  |
| 99+16              | C/L             | 1989              |  |

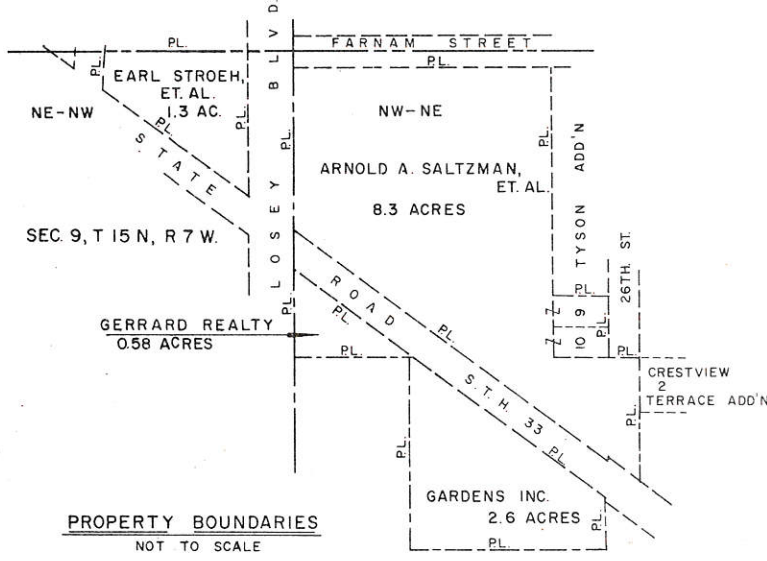
SIGNAL EQUIPMENT

| <u>Quantity</u> | <u>Description</u>  |
|-----------------|---|
| 4               | 13 Ft. Pedestal, T-1 Base, complete with anchor bolts, side of post mountings                             |
| 2               | Signal Lamp Dimmers   |
| 2               | T-2 Conc. base, anchor bolts, aluminum combination street light and signal standard                       |
| 2               | 6 Ft. mast arm for street light   |
| 2               | 15 Ft. Aluminum Horizontal Thrombone mast arm, signal mountings.  |
| 2               | One-way, three section, 12" R, Y & G signal head horizontal mounted.                                      |
| 2               | 5" back plate and 400 watt mercury vapor lighting package complete with photo cell and deluxe white lamp. |
| 4               | T-1 concrete base, complete with anchor bolts.  |
| 3               | 5" backplates for 12" Y signal section with signal lamp dimmer, 12" R Y G top of post mount.              |
| 3               | One-way, three section, 12" R, Y, & G top of post mounted signal.   |
| 1               | One-way, three section, 12" Y with lamp dimmer, 12" G, top of post mount.                                 |
| 1               | 5" back plate for 12" Y & G signal heads  |





|                |  |                     |              |
|----------------|--|---------------------|--------------|
| REVISION DATE  | PROJECT I.D.<br>5991-0-13  | SHEET NUMBER<br>4.1 | TOTAL SHEETS |
|                | FEDERAL PROJECT DESIGNATION<br>EHST-4140 (6)   |                     |              |
|                | PLAT OF RIGHT OF WAY REQUIRED<br>TOPICS:<br>LOSEY BLVD. - CITY OF LA CROSSE<br>STATE ROAD INTERSECTION<br>LA CROSSE COUNTY |                     |              |
|                | SCALE<br>0 50 100 Ft.  | DATE<br>8-2-73      |              |
| Const. Project | 5991-0-14  |                     | 4            |



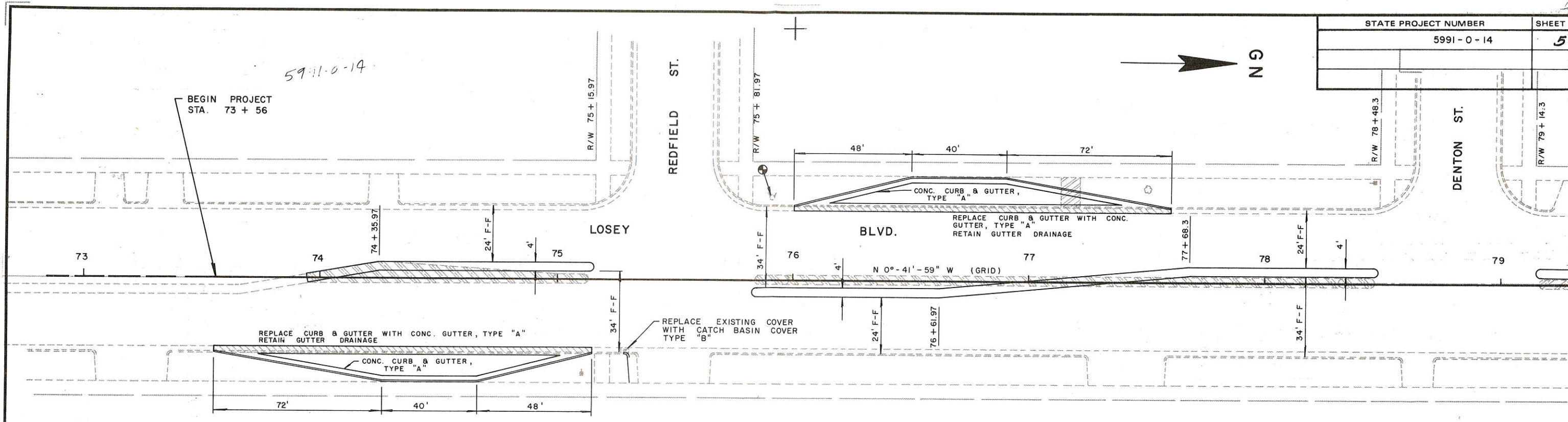
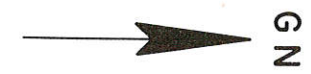
NOTE: BEARINGS SHOWN ON THIS PLAT ARE THE TRUE BEARINGS OF EACH TANGENT TO THE NEAREST MINUTE.

| SCHEDULE OF LANDS AND INTERESTS REQUIRED |              |                            |                    |       |              |
|--|--------------|----------------------------|--------------------|-------|--------------|
| PARCEL NUMBER                            | SHEET NUMBER | OWNER                      | INTERESTS REQUIRED | ACRES | PROJECT I.D. |
| 1  | 4.1          | EARL STROEH, ET AL.        | FEE SIMPLE         | 0.07  | 5991-0-13    |
| 2  | "            | ARNOLD A. SALTZMAN, ET AL. | " "                | 0.16  | "            |
| 3  | "            | GERRARD REALTY CORP.       | " "                | 0.03  | "            |
| 4  | "            | GARDENS, INC.              | " "                | 0.03  | "            |
| 3S                                       | "            | FRANK WHITE ADV. CO.       | ADVERTISING RIGHTS | —     | "            |
| 5  | "            | NORTHERN STATES POWER CO.  | RELEASE OF RIGHTS  | —     | "            |
| 6  | "            | LA CROSSE TELEPHONE CORP.  | " " "              | —     | "            |



59-1-0-14

BEGIN PROJECT STA. 73 + 56



LEGEND

- PAVEMENT REMOVAL
- CURB AND GUTTER REMOVAL
- BITUMINOUS PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- CORRUGATED MEDIAN & ISLAND REMOVAL

B.M. STA. 72 + 53 TOP NUT OF HYDRANT ELEV. 65.67 (CITY DATUM)  
 B.M. STA. 75 + 92 TOP NUT OF HYDRANT ELEV. 66.01 (CITY DATUM)

|          |  |                           |       |       |        |       |       |        |        |        |       |       |        |       |       |       |       |       |       |       |       |       |    |     |
|----------|--|---------------------------|-------|-------|--------|-------|-------|--------|--------|--------|-------|-------|--------|-------|-------|-------|-------|-------|-------|-------|-------|-------|----|-----|
|          |  | 2' EAST - WEST CENTERLINE |       |       |        |       |       |        |        |        |       |       |        |       |       |       |       |       |       |       |       |       |    |     |
| 62       |  |                           |       |       |        |       |       |        |        |        |       |       |        |       |       |       |       |       |       |       |       | 62    |    |     |
| 61       |  |                           |       |       |        |       |       |        |        |        |       |       |        |       |       |       |       |       |       |       |       | 61    |    |     |
| 60       |  |                           |       |       |        |       |       |        |        |        |       |       |        |       |       |       |       |       |       |       |       | 60    |    |     |
| WEST 16' |  | 62.33                     | 62.30 | 62.22 | 62.17  | 62.10 | 62.07 | 62.08  | 61.95  | 61.86  | 61.79 | 61.67 | 61.62  | 61.53 | 61.52 | 61.42 | 61.42 | 61.35 | 61.33 | 61.29 | 61.21 | 61.42 |    |     |
| WEST 2'  |  | 62.63                     | 62.55 |       | 62.30  | 62.27 | 62.16 | 62.10  | 62.05  | 62.00  | 61.91 | 61.88 | 61.81  | 61.77 | 61.68 | 61.66 | 61.60 | 61.55 | 61.50 | 61.45 | 61.56 |       |    |     |
| EAST 2'  |  |                           | 62.55 | 62.48 | 62.40  | 62.30 | 62.28 | 62.18  | 62.11  | 62.05  | 61.99 | 61.93 | 61.89  | 61.83 | 61.77 | 61.69 | 61.67 | 61.60 | 61.53 | 61.49 | 61.43 | 61.55 |    |     |
| EAST 16' |  | 62.30                     | 62.23 | 62.18 | 62.11  | 62.06 | 62.02 | 61.97  | 61.87  | 61.82  | 61.75 | 61.67 | 61.64  | 61.58 | 61.52 | 61.46 | 61.42 | 61.34 | 61.30 | 61.24 | 61.18 | 61.26 |    |     |
|          |  | 73                        | +70   | 74    | +35.97 | +65   | 75    | +15.97 | +48.97 | +81.97 | 76    | +25   | +61.97 | +75   | 77    | +25   | +50   | +68.3 | 78    | +25   | +48.3 | +75   | 79 | +10 |









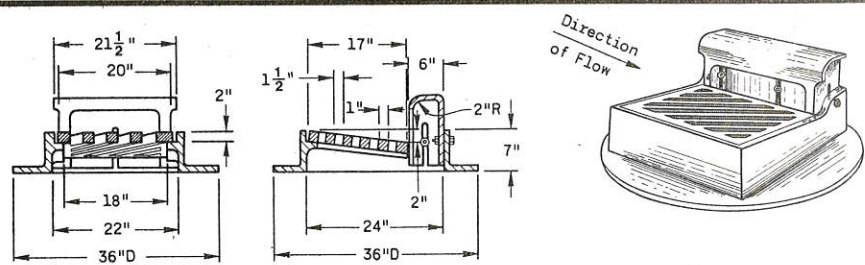




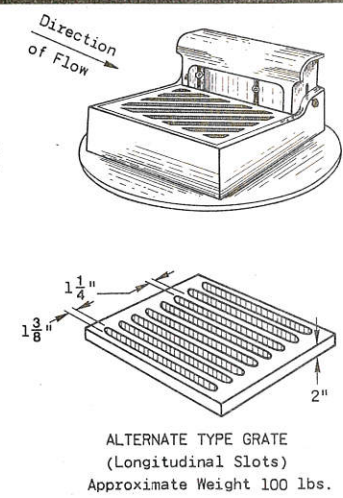




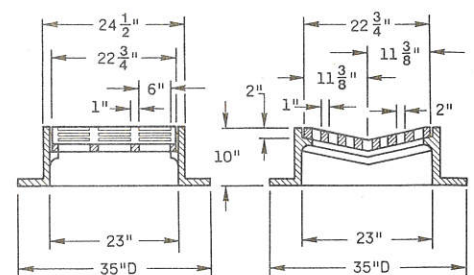




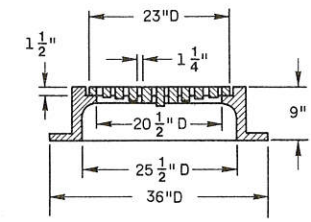
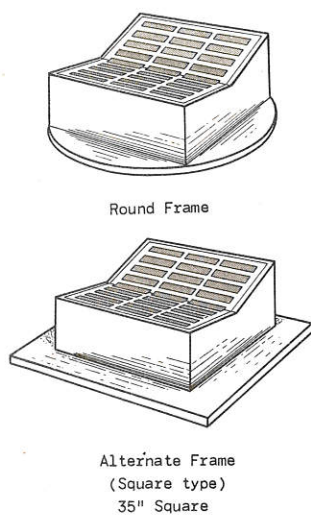
**TYPE "A"**  
 (Approximate Weight 405 lbs.)  
 Frame Weight 250 lbs.  
 Grate Weight 85 lbs.  
 Box Weight 70 lbs.



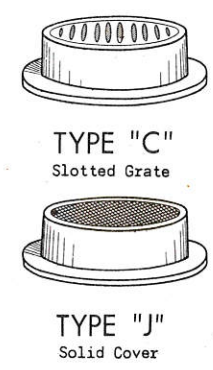
**ALTERNATE TYPE GRATE**  
 (Longitudinal Slots)  
 Approximate Weight 100 lbs.



**TYPE "B"**  
 (Approximate Weight 395 lbs.)  
 Frame Weight 285 lbs.  
 Grate Weight 110 lbs.

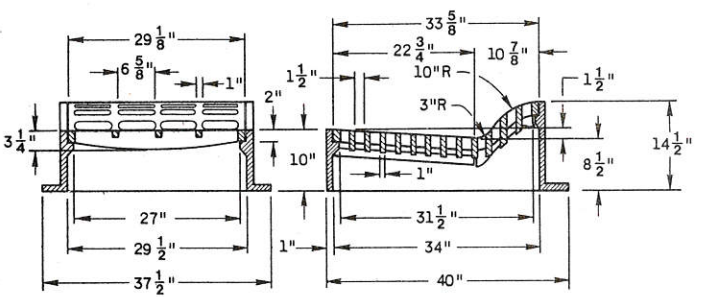


**TYPE "C" - TYPE "J"**  
 Frame Weight 250 lbs.  
 Slotted Grate Weight 125 lbs.  
 Solid Cover Weight 150 lbs.

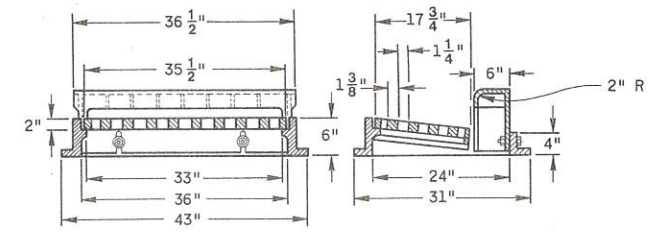
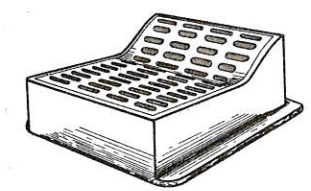


Diagonal Slots shall be oriented to the direction of flow as shown hereon. Hence RIGHT and LEFT Grates shall be furnished depending on direction of flow. (See Sketch Below)

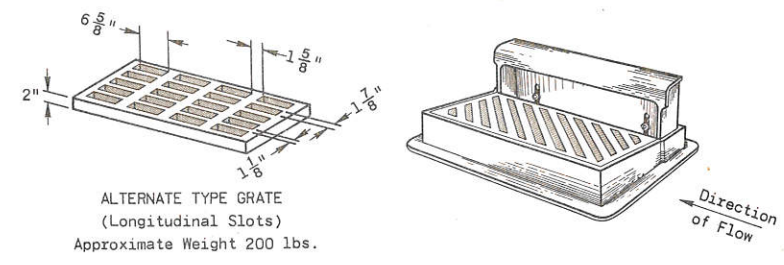
Longitudinal slot type grates may be used ONLY where bicycles are prohibited.



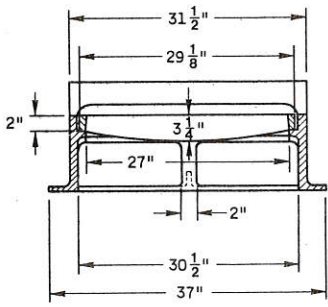
**TYPE "F"**  
 (Approximate Weight 850 lbs.)  
 Frame 515 lbs.  
 Back grate 160 lbs.  
 Front grate 175 lbs.



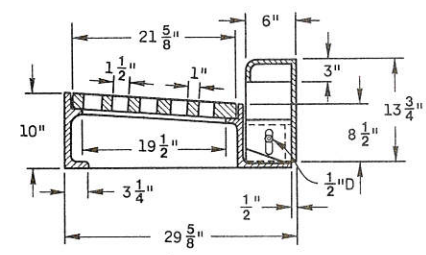
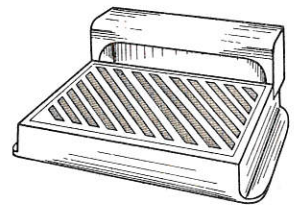
NOTE: Curb Box height adjustable 6" to 9"



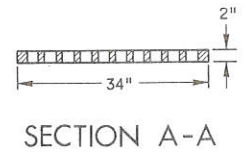
**ALTERNATE TYPE GRATE**  
 (Longitudinal Slots)  
 Approximate Weight 200 lbs.



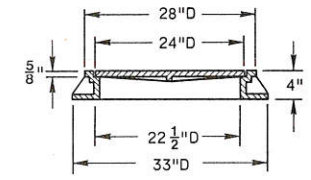
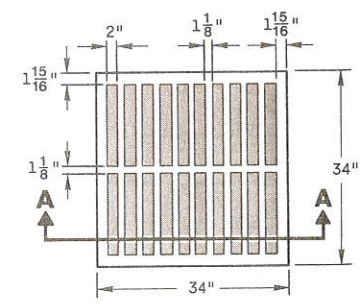
**TYPE "WM"**  
 (Approximate Weight 670 lbs.)  
 Frame Weight 350 lbs.  
 Grate Weight 185 lbs.  
 Box Weight 135 lbs.



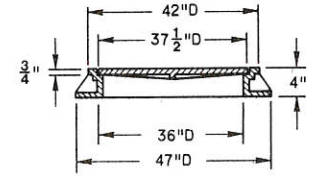
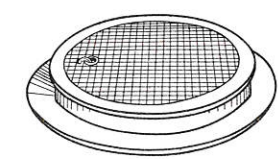
**TYPE "H"**  
 (Approximate Weight 510 lbs.)  
 Frame Weight 220 lbs.  
 Grate Weight 175 lbs.  
 Box Weight 115 lbs.



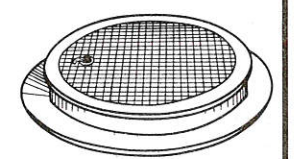
**TYPE "MS"**  
 (Approximate Grate Weight 285 lbs.)



**TYPE "L"**  
 (Approximate Weight 220 lbs.)



**TYPE "M"**  
 (Approximate Weight 535 lbs.)



**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Detail drawings for proposed alternate designs for Catch Basin, Manhole and Inlet Covers shall be submitted to the Engineer for approval providing that such alternate designs make provision for equivalent capacity and strength.

All Catch Basin, Manhole and Inlet Covers which are placed in vehicular traffic areas shall be "Non-Rocking" type.

Adjustment of the cover to grade may be accomplished by the use of mortar and brick, or by Precast Concrete Grade Rings (AASHTO Designation M-199). Maximum adjustment shall be 8 inches.

Curb box height to be adjusted 4 to 9 inches, unless otherwise noted, after the form is in place.

The actual weight of covers may vary within 5 per cent, plus or minus, of the approximate weight.

**CATCH BASIN MANHOLE AND INLET COVERS**

State of Wisconsin  
 Department of Transportation  
 Division of Highways

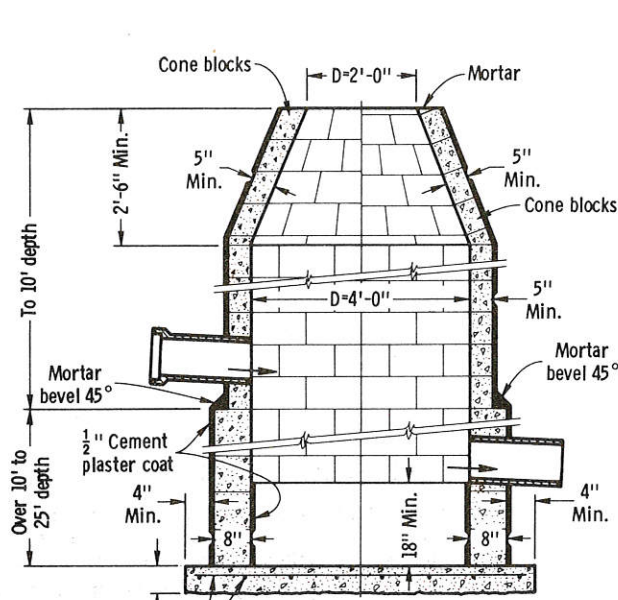
RECOMMENDED FOR APPROVAL:  
 4-30-74  
 DATE  
 APPROVED  
 5-02-74  
 DATE

*J. C. Henning*  
 CHIEF OF FACILITIES DEVELOPMENT

*H. J. Sidler*  
 STATE HIGHWAY ENGINEER

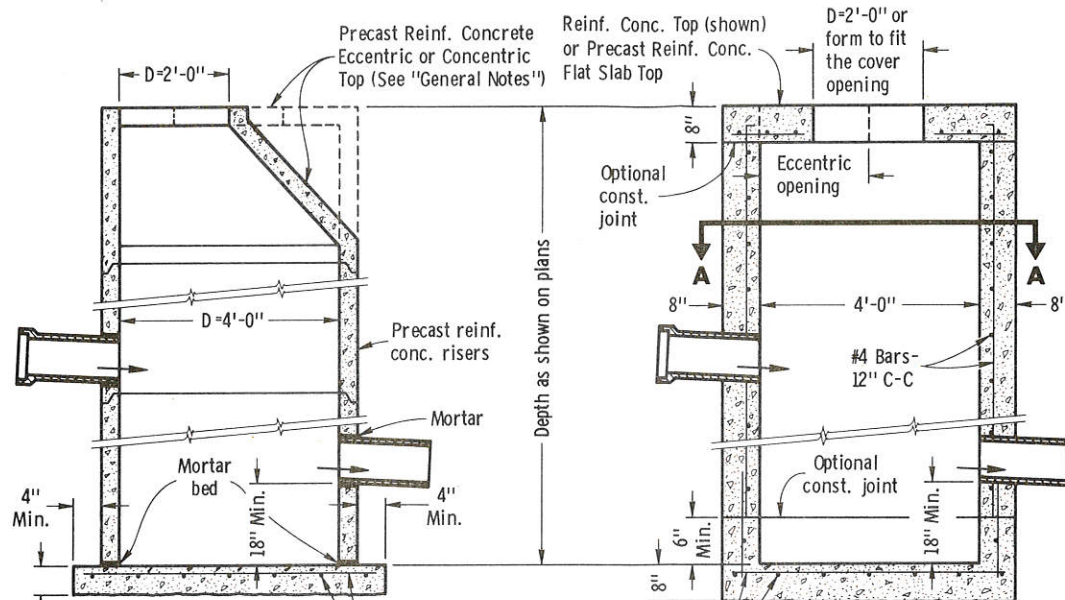
S.D.D. 8A5-1



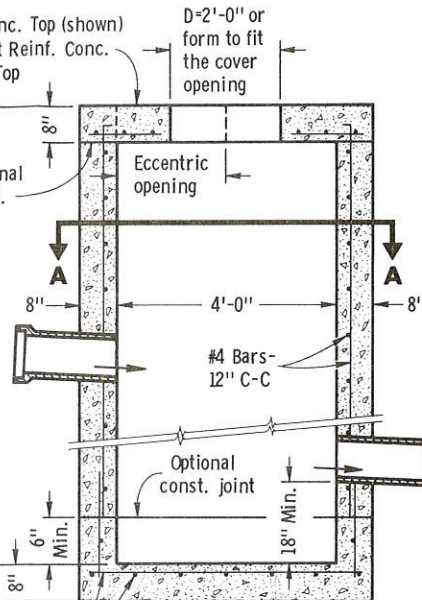


#4 Bars-12" C-C to 10' depth  
6" C-C over 10' to 25' depth  
See "General Notes" for application of concentric top.

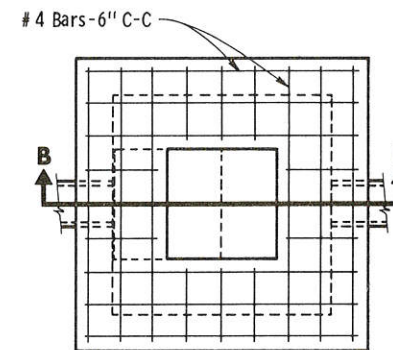
CONCRETE BLOCK



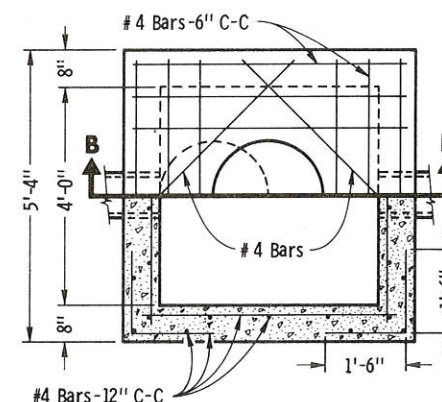
PRECAST REINFORCED CONCRETE



SECTION B-B  
REINFORCED CONCRETE

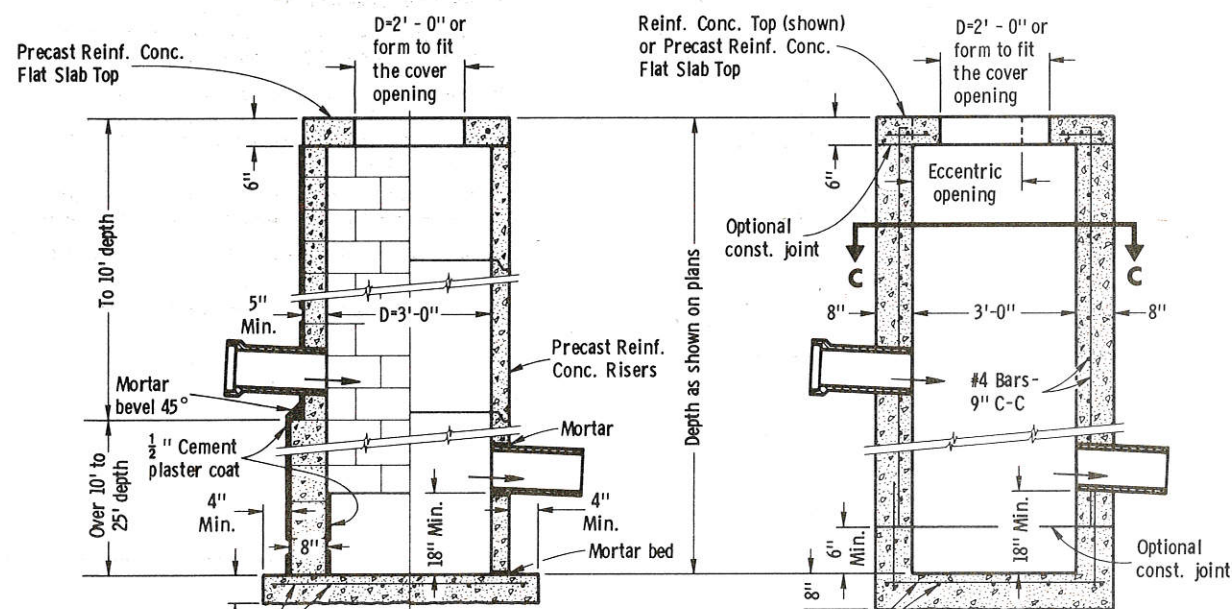


PLAN VIEW SHOWING  
ALTERNATE OPENING



HALF SECTION A-A

CATCH BASINS TYPE 1



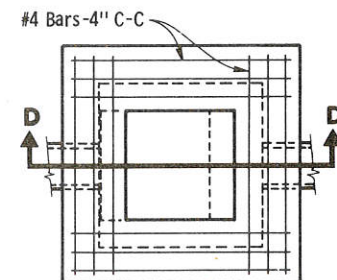
#4 Bars-12" C-C to 10' depth  
9" C-C over 10' to 25' depth

CONCRETE  
BLOCK

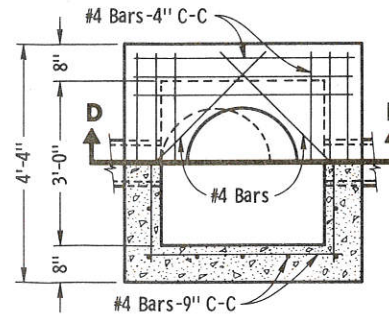
PRECAST  
REINFORCED  
CONCRETE

#4 Bars-12" C-C to 10' depth  
9" C-C over 10' to 25' depth

SECTION D-D  
REINFORCED CONCRETE



PLAN VIEW SHOWING  
ALTERNATE OPENING



HALF SECTION C-C

CATCH BASINS TYPE 2

GENERAL NOTES

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Detailed drawings for proposed alternate designs for underground drainage structures shall be submitted to the Engineer for approval providing that such alternate designs make provision for equivalent capacity and strength.

All drainage structures are designated on the plans as "Manholes 1 - C", "Catch Basins 1 - B", "Inlets 1 - H", etc. The first digit designates the masonry portion of the structure, and the following letter designates the type of cover to be used to comprise the complete unit.

Flat Slab Tops, Cone Tops (Eccentric or Concentric) shall conform to AASHTO Designation M-199.

Precast Reinforced Concrete Bases shall conform to Flat Slab Tops requirements of AASHTO Designation M-199.

Precast Reinforced Bases shall be placed on a bed of material at least 6 inches in depth, which meets the requirements for Granular Backfill. This bedding shall be compacted and provide uniform support for the entire area of the base.

Precast Reinforced Concrete Cone Tops (Eccentric or Concentric) or Precast Reinforced Concrete Flat Slab Tops may be used on concrete block structures. The Cone Tops shall be installed on a bed of mortar.

Eccentric Cone Tops may be used on all structures, and Concentric Cone Tops shall be used only on structures 5 feet or less in depth, unless otherwise directed by the Engineer.

Steps meeting the following requirements shall be installed in all structures over 5 feet in depth: 16 inch C. C. maximum spacing; 4 inches clear from the wall at the center of the step; have a minimum width of 10 inches out to out of casting; be constructed of gray iron, ductile iron or alternate material approved by the Engineer. Bar steel reinforcement for steps will not be permitted.

All bar steel reinforcement shall be embedded 2 inches clear unless otherwise shown or noted.

Precast Reinforced Concrete Risers may be placed with tongue up or down.

CATCH BASINS TYPE 1 & 2

State of Wisconsin  
Department of Transportation  
Division of Highways

RECOMMENDED FOR APPROVAL:

4-30-74

DATE

APPROVED

5-02-74

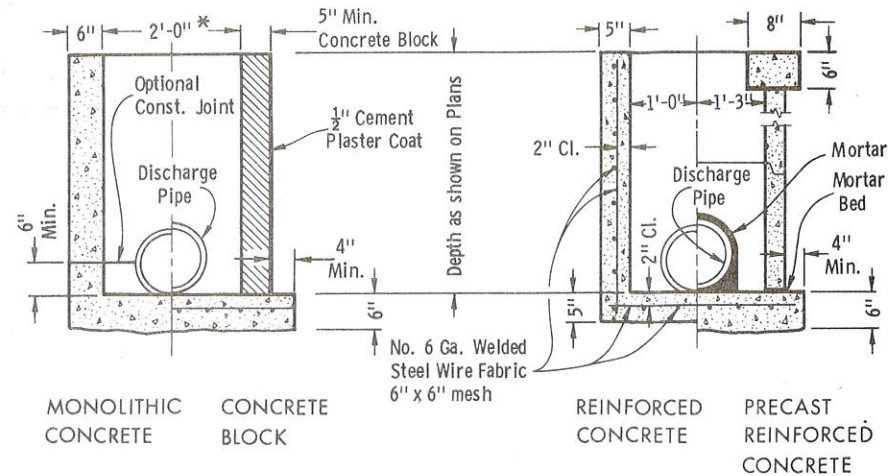
DATE

*J. C. Heenan*  
CHIEF OF FACILITIES DEVELOPMENT

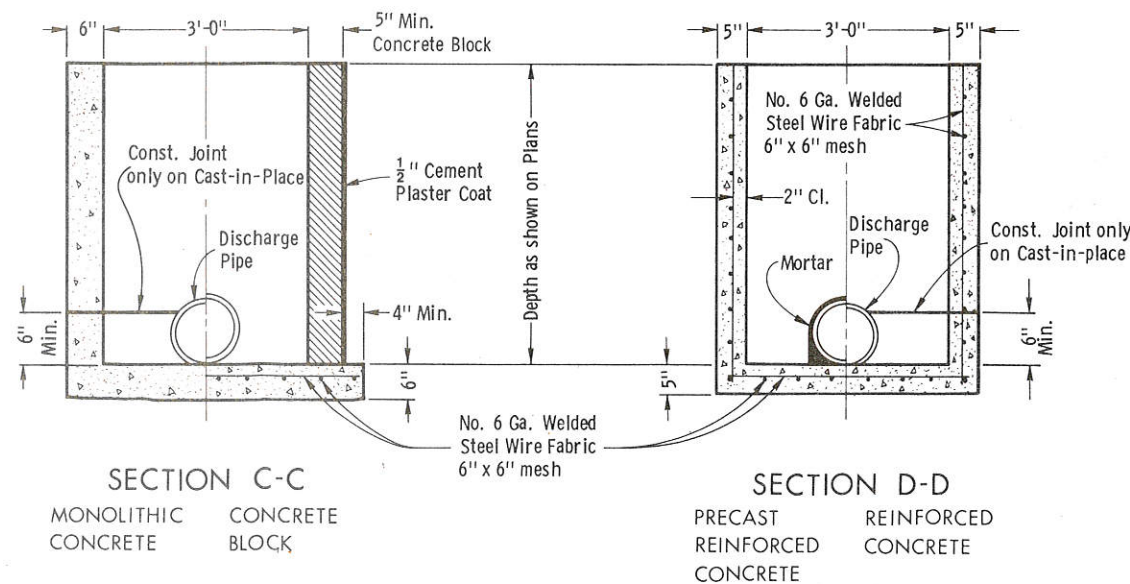
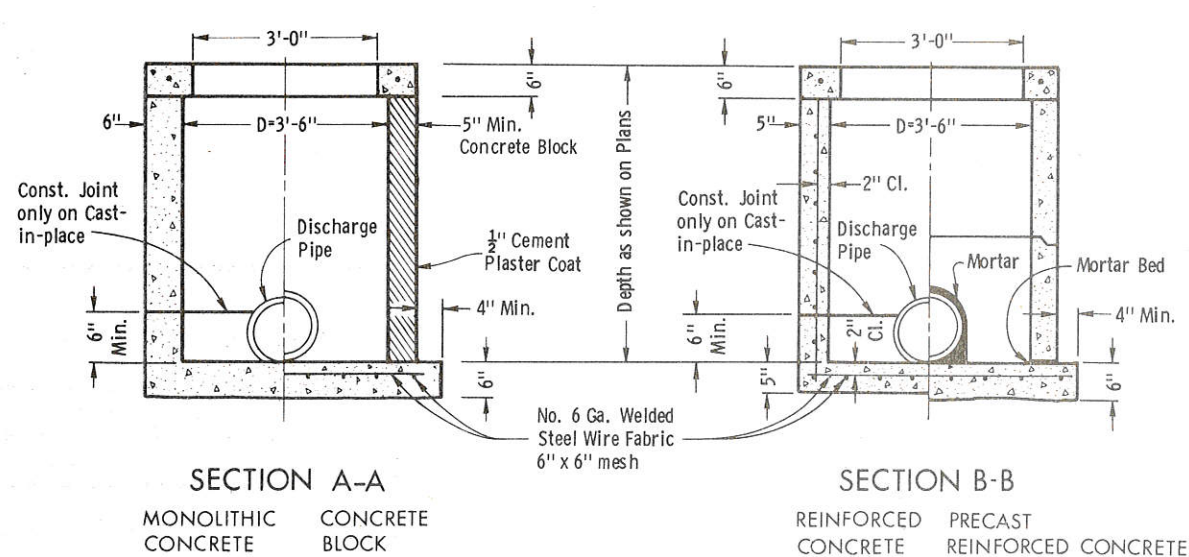
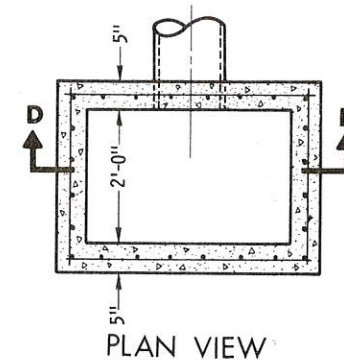
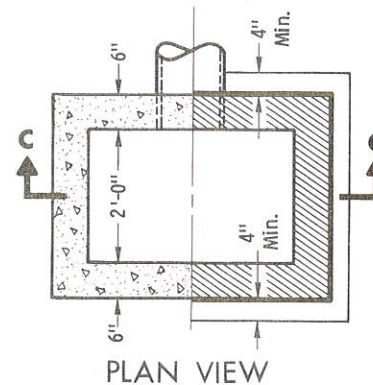
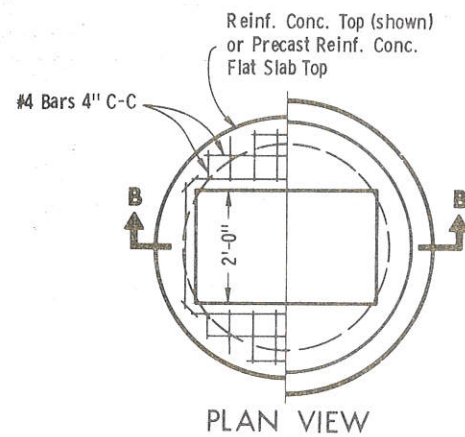
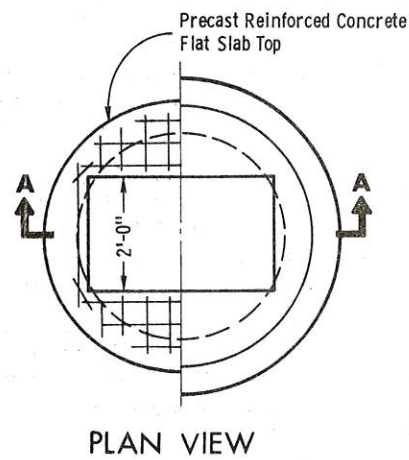
*H. J. Siddle*  
STATE HIGHWAY ENGINEER



\* Selection of Square or Circular Design will be based on the pipe sizes and the Inlet Cover being utilized.



**INLETS TYPE 1**



**INLETS TYPE 3**

**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Detailed drawings for proposed alternate designs for underground drainage structures shall be submitted to the Engineer for approval providing that such alternate designs make provision for equivalent capacity and strength.

Square Precast Inlet units shall conform to the requirements of the Catch Basins, Manholes and Inlets Section of the Standard Specifications.

All drainage structures are designated on the plans as "Manholes 1 - C", "Catch Basins 1 - B", "Inlets 1 - H", etc. The first digit designates the masonry portion of the structure, and the following letter designates the type of cover to be used to comprise the complete unit.

Flat Slab Tops, Cone Tops (Eccentric or Concentric) shall conform to AASHTO Designation M-199.

Precast Reinforced Concrete Bases shall conform to Flat Slab Tops requirements of AASHTO Designation M-199.

Precast Reinforced Bases shall be placed on a bed of material at least 6 inches in depth, which meets the requirements for Granular Backfill. This bedding shall be compacted and provide uniform support for the entire area of the base.

Precast Reinforced Concrete Flat Slab Tops may be used on the structures. The Tops shall be installed on a bed of mortar.

All bar steel reinforcement shall be embedded 2 inches clear unless otherwise shown or noted.

Precast Reinforced Concrete Risers may be placed with tongue up or down.

**INLETS TYPE 1 & 3**

State of Wisconsin  
Department of Transportation  
Division of Highways

RECOMMENDED FOR APPROVAL:

4-30-74  
DATE

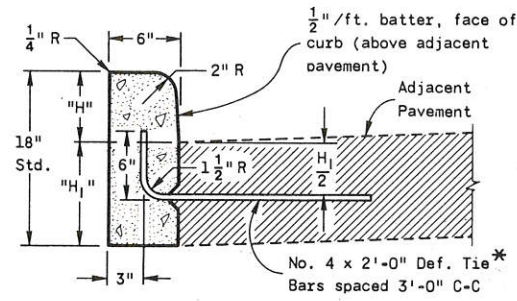
J. C. Hennrich  
CHIEF OF FACILITIES DEVELOPMENT

APPROVED

5-02-74  
DATE

H. J. Siodler  
STATE HIGHWAY ENGINEER

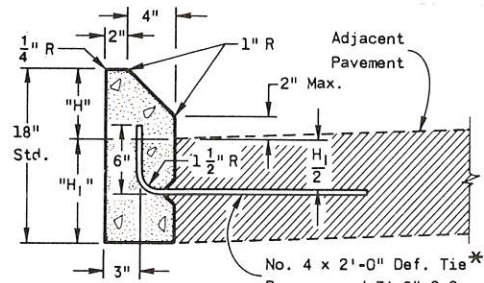




"H" = 9" Max. and 3 1/2" Min. and shall be 6" unless otherwise shown on the plans.  
 "H<sub>1</sub>" = Same as adjacent pavement thickness for rigid pavement and 12" for non-rigid pavement (Tie Bars omitted)

**TYPE "A" TYPE "D"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

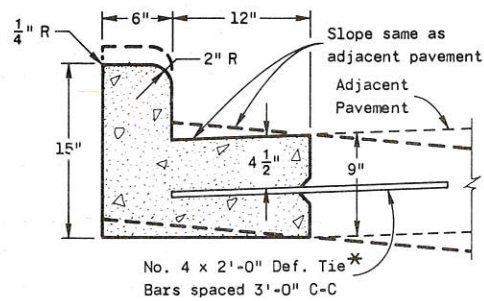
**CONCRETE CURB**



"H" = 6" Max. and 4" Min. and shall be 6" unless otherwise shown on the plans.  
 "H<sub>1</sub>" = Same as adjacent pavement thickness for rigid pavement and 12" for non-rigid pavement (Tie Bars omitted)

**TYPE "G" TYPE "J"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

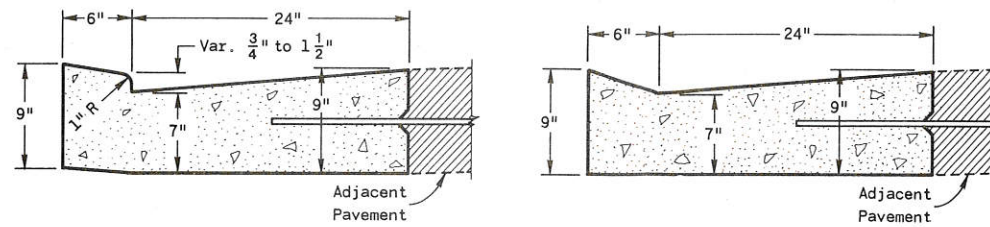
**CONCRETE CURB**  
 (MOUNTABLE)



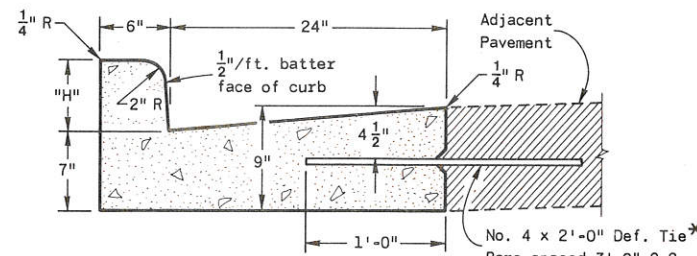
**TYPE "A" TYPE "D"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

**CONCRETE CURB & GUTTER 18"**

Reverse slope Curb & Gutter shown thus - - -



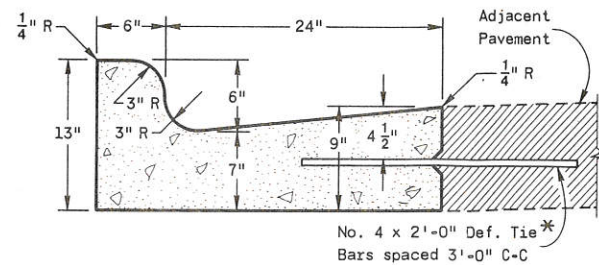
**ALTERNATE ENTRANCES**  
**CONCRETE CURB & GUTTER 30"**



"H" = 9" Max. 3 1/2" Min. and shall be 6" unless otherwise shown on the plans.

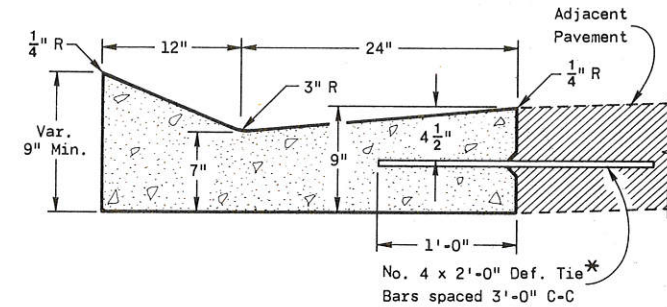
**TYPE "A" TYPE "D"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

**CONCRETE CURB & GUTTER 30"**



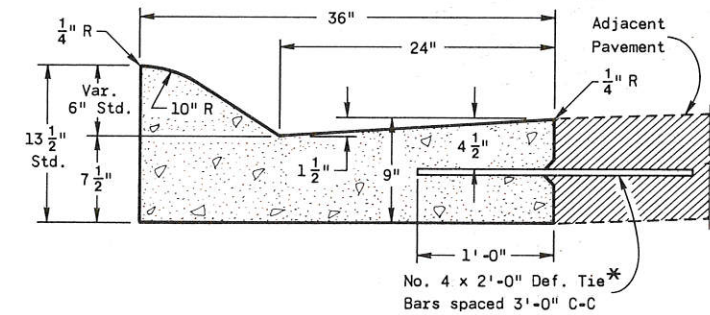
**TYPE "K" TYPE "L"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

**CONCRETE CURB & GUTTER 30"**



**TYPE "A" TYPE "D"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

**CONCRETE GUTTER 36"**



**TYPE "A" TYPE "D"**  
 (INCLUDING TIE BARS) (EXCLUDING TIE BARS)

**CONCRETE CURB & GUTTER 36"**  
 (MOUNTABLE)

**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Detailed drawings for proposed alternate designs for Curb, Gutter and Combination Curb and Gutter shall be submitted to the Engineer for approval providing that such alternate designs make provision for equivalent capacity and strength.

Joints shall not be sealed in Concrete Curb, or Concrete Curb & Gutter.

\* Where Curb and Gutter are poured adjacent to existing pavement, the Hook Bolt may be used as for "Longitudinal Joints - Concrete Pavement".

**CONCRETE CURB, GUTTER,  
 COMBINATION CURB & GUTTER**

State of Wisconsin  
 Department of Transportation  
 Division of Highways

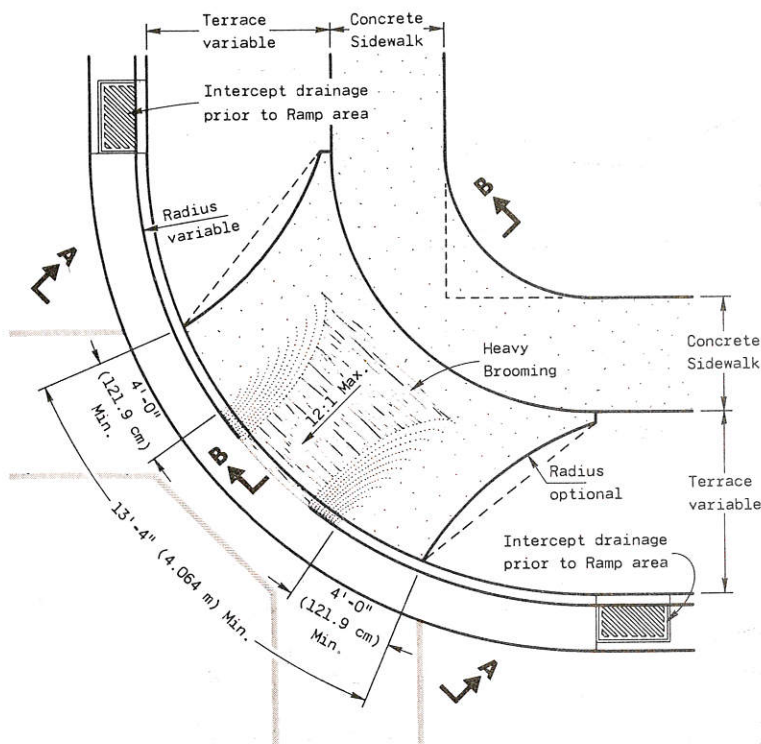
RECOMMENDED FOR APPROVAL:  
 9-12-73  
 DATE

J.C. Hennrich  
 CHIEF OF FACILITIES DEVELOPMENT

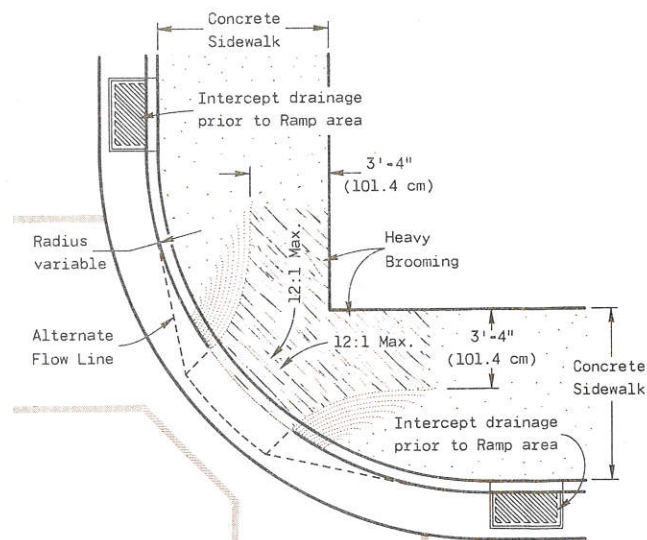
APPROVED  
 9-19-73  
 DATE

W.J. Siddle  
 STATE HIGHWAY ENGINEER

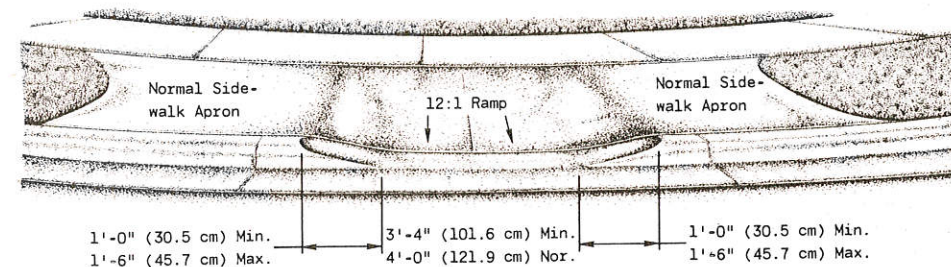




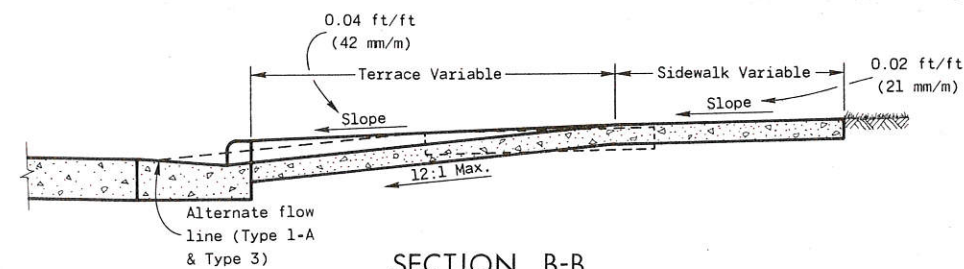
PLAN VIEW  
TYPE 1 RAMP  
(CENTER OF CORNER RADIUS)



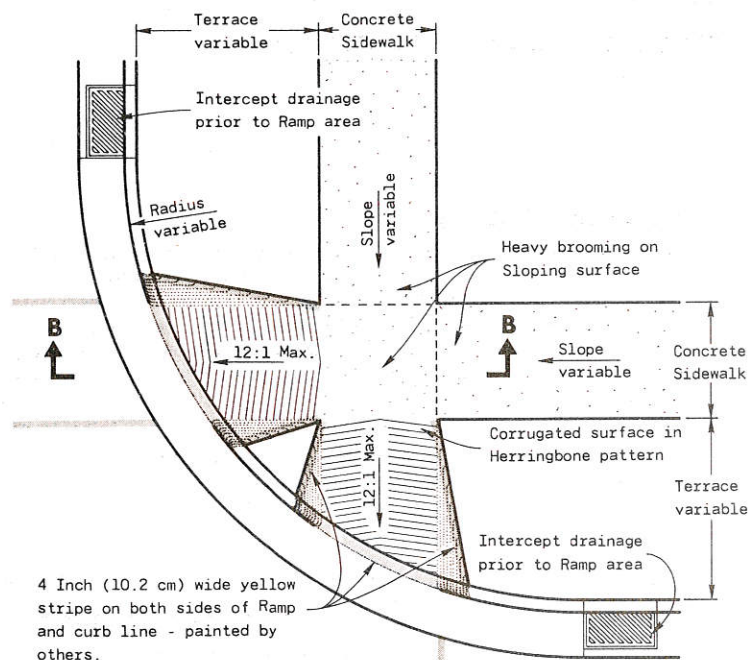
PLAN VIEW  
TYPE 1-A RAMP  
(NO TERRACE)



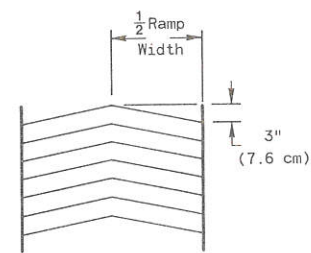
VIEW A-A



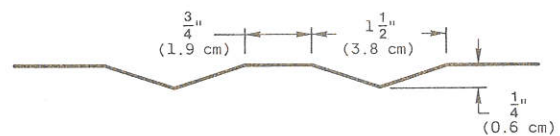
SECTION B-B



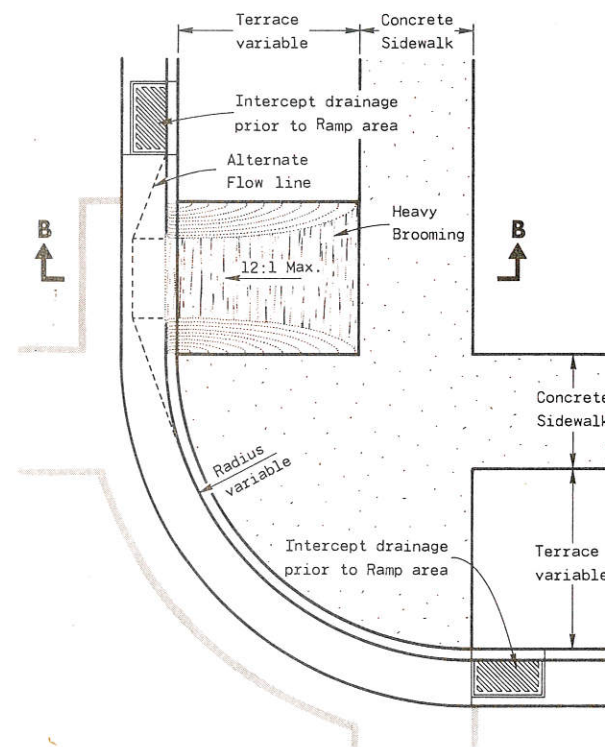
PLAN VIEW  
TYPE 2 RAMP  
(ON LINE WITH SIDEWALK)



DETAIL OF  
HERRINGBONE PATTERN



DETAIL OF  
SURFACE CORRUGATION



PLAN VIEW  
TYPE 3 RAMP  
(OUTSIDE OF CROSSWALK AREA)

**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.  
Ramps shall be built at 12:1 or flatter. When necessary, the Sidewalk elevation may be lowered to meet the high point on the Ramp.  
Type 1 or Type 1-A Ramps shall have a normal Sidewalk apron and Curb on both sides of Ramp. Entire Curb radius shall not be made into Ramp.  
Curb Ramps shall be measured and paid for as Concrete Sidewalk and Concrete Curb and Gutter.

| METHOD OF DIMENSIONING         |   |
|--------------------------------|---|
| 1'-0" (30.5 cm)                | → |
| 1'-0" (30.5 cm)                | ← |
| BASIS: 1 in. = EXACTLY 25.4 mm |   |

**CURB RAMPS FOR  
HANDICAPPED PERSONS**

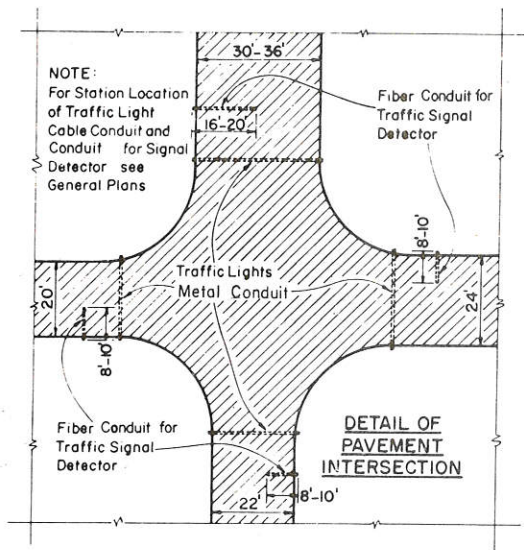
State of Wisconsin  
Department of Transportation  
Division of Highways

RECOMMENDED FOR APPROVAL:  
DATE: 1-23-74  
APPROVED: 3-21-74  
DATE: 3-21-74

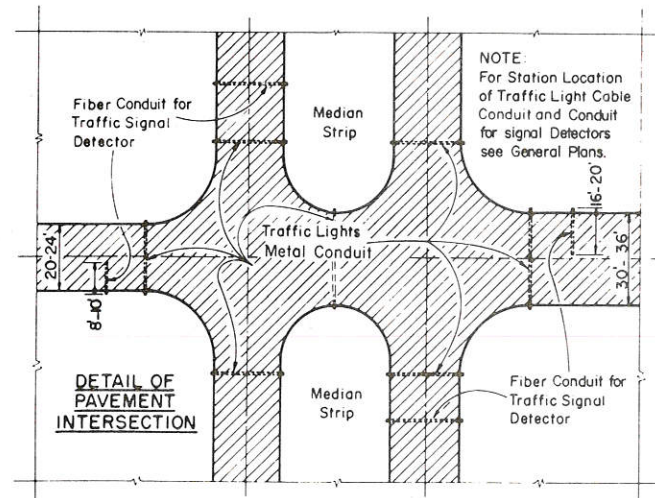
*J.C. McManis*  
CHIEF OF FACILITIES DEVELOPMENT

*W.J. Sinden*  
STATE HIGHWAY ENGINEER

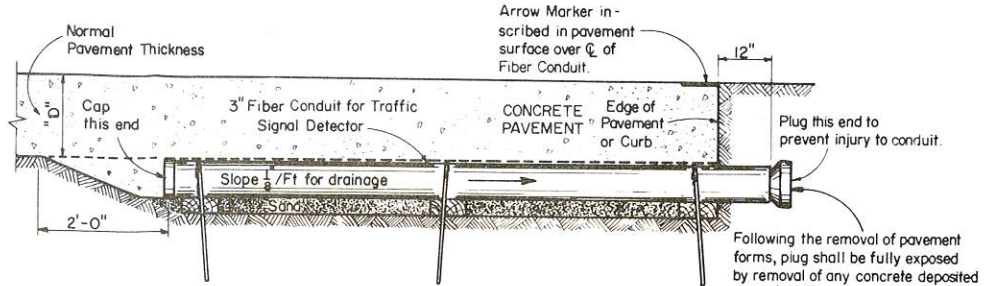




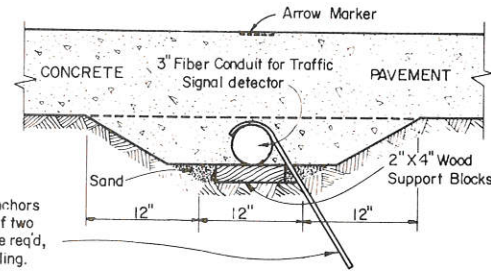
**PLAN VIEW**  
SHOWING RELATIVE POSITION OF  
TRAFFIC LIGHT CONDUITS AND  
TRAFFIC SIGNAL DETECTOR CONDUITS  
AT UNDIVIDED HIGHWAY INTERSECTIONS



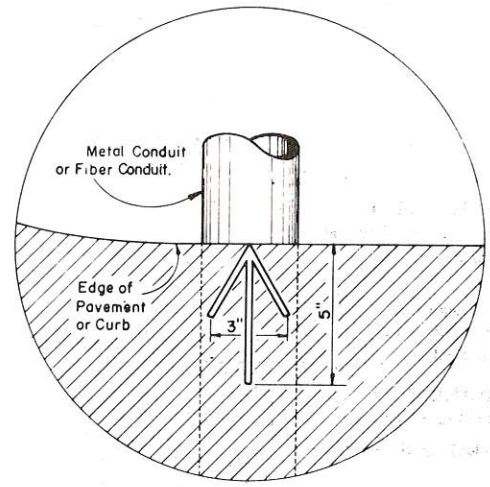
**PLAN VIEW**  
SHOWING RELATIVE POSITION OF  
TRAFFIC LIGHT CONDUITS AND  
TRAFFIC SIGNAL DETECTOR CONDUITS  
AT DIVIDED HIGHWAY INTERSECTIONS



**TRAFFIC SIGNAL DETECTOR FOR  
UNDIVIDED HIGHWAYS**

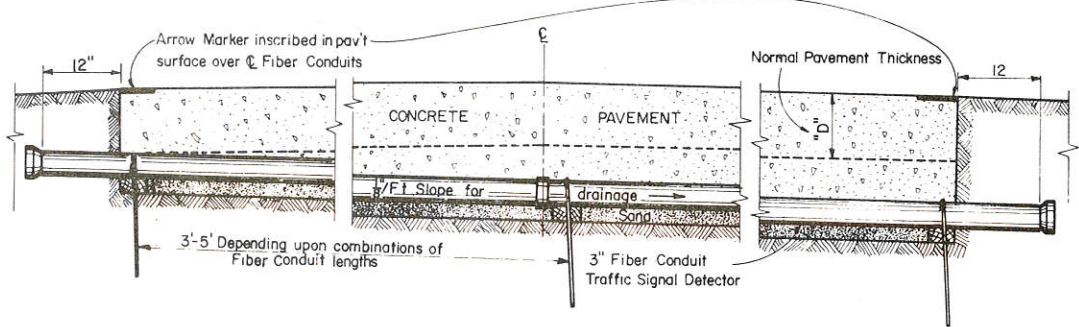


**SIDE & END ELEVATIONS  
SHOWING PLACEMENT DETAILS  
FOR TRAFFIC SIGNAL DETECTOR CONDUIT**

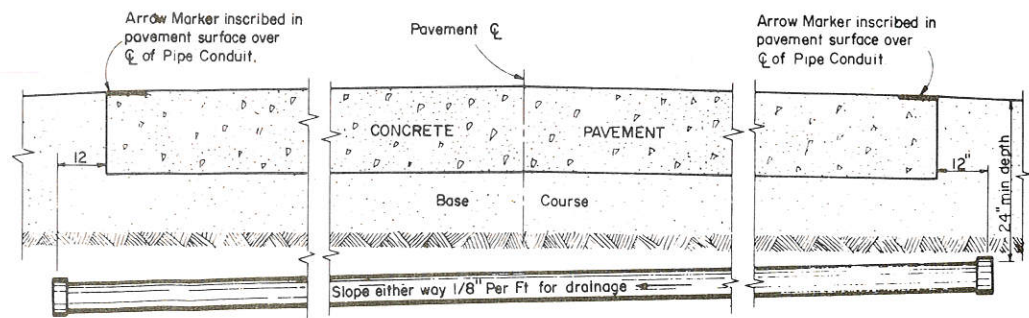


Arrow Marker to be inscribed in fresh concrete and/or bituminous surfacing  $\frac{1}{4}$  to  $\frac{3}{8}$  deep at each location where pipe conduit or fiber conduit are placed under rigid surfacing.

**PLAN VIEW - ARROW MARKER**



**TRAFFIC SIGNAL DETECTOR FOR  
DIVIDED HIGHWAYS**



**ELEVATION ON CENTERLINE  
SHOWING PLACEMENT DETAILS  
FOR TRAFFIC SIGNAL CONDUIT**

**GENERAL NOTES**  
Details of Construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications, and the applicable Special Provisions.

**MATERIALS**  
Metal Conduit shall be furnished and placed as shown hereon and in accord with the Standard Specifications.  
Fiber Conduit shall be furnished and placed as shown hereon and in accord with the Standard Specifications.

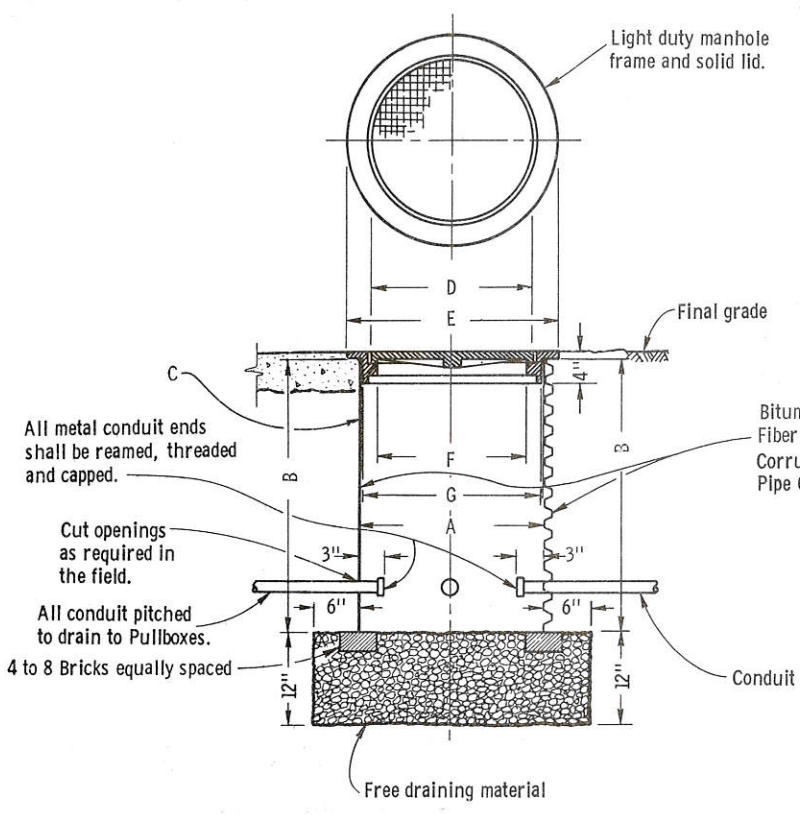
**MEASUREMENT & PAYMENT**  
The item of Fiber Conduit shall be measured and paid for by the linear foot complete in place and in accord with Standard Specifications

**CONDUIT SIZE**  
Unless shown or required otherwise on the plans, Metal Conduit shall be 2" I.D.

| METAL CONDUIT<br>&<br>FIBER CONDUIT   |   |
|---------------------------------------|---|
| STATE HIGHWAY COMMISSION OF WISCONSIN |   |
| RECOMMENDED FOR APPROVAL:             |   |
| DATE: 4-3-63                          | <i>J. D. Pelt</i><br>ENGINEER OF DESIGN         |
| APPROVED:                             |   |
| DATE: 4/5/63                          | <i>E. L. Ruetzger</i><br>STATE HIGHWAY ENGINEER |

S. D. D. 9B2-1

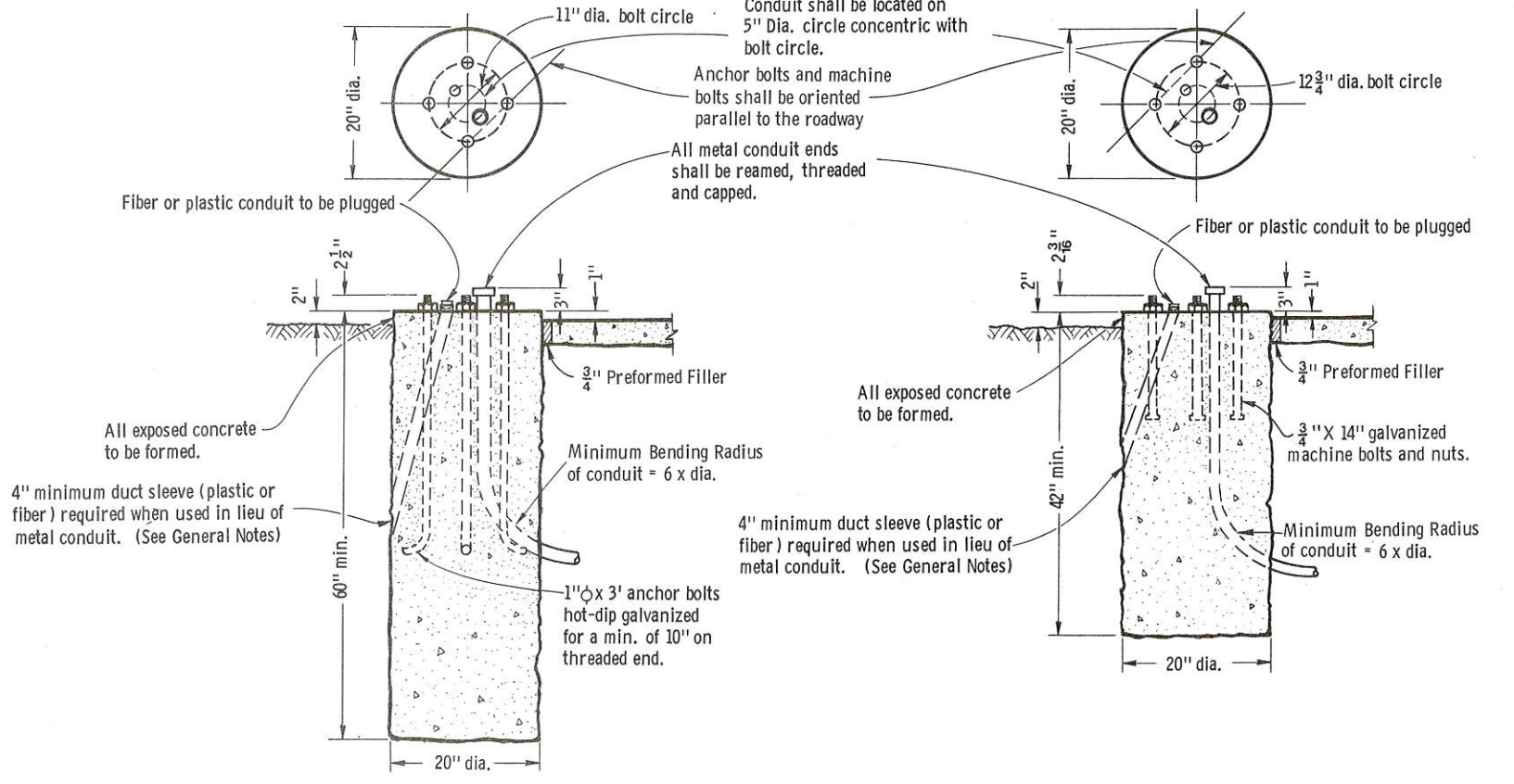




| PIPE TYPE          | BITUMINOUS FIBER |         |         |         | CORR. METAL |
|--------------------|------------------|---------|---------|---------|-------------|
| Pipe Dia. (Inside) | A 12"            | 18"     | 24"     | 24"     | 24"         |
| Pipe Length        | B 24"            | 24"     | 24"     | 36"     | 36"         |
| Wall Thickness     | C 0.4"           | 0.4"    | 0.4"    | 0.4"    | .064"       |
| Manhole Lid        | D 10 1/4"        | 16 1/4" | 22 1/4" | 22 1/4" | 22 1/4"     |
| Manhole Frame      | E 14 1/2"        | 20 1/2" | 26 1/2" | 26 1/2" | 26 1/2"     |
| Manhole Frame      | F 8 1/2"         | 14 1/2" | 20 1/2" | 20 1/2" | 20 1/2"     |
| Manhole Frame      | G 11 1/2"        | 17 1/2" | 23 1/2" | 23 1/2" | 23 1/2"     |
| Lid & Frame        | -- 55#           | 100#    | 160#    | 160#    | 160#        |

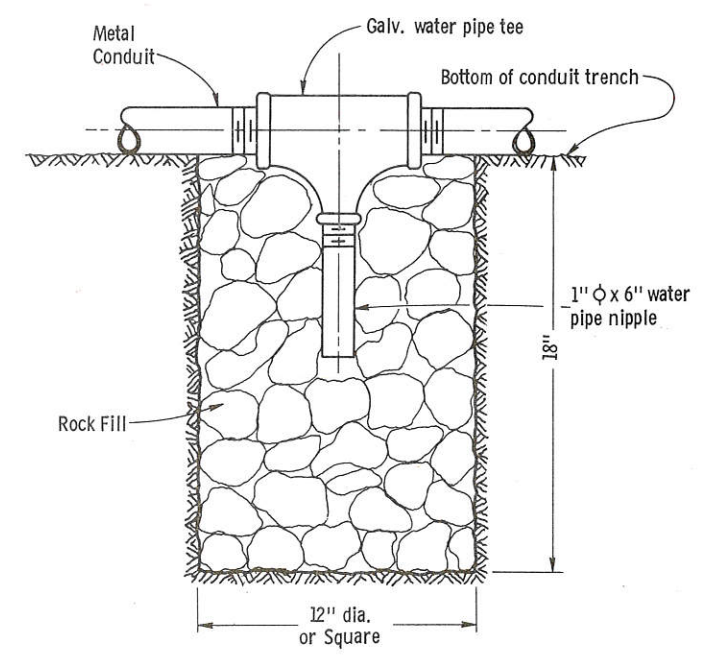
SHOWING INSTALLATION IN PAVED LOCATION      SHOWING INSTALLATION IN UNPAVED LOCATION

**PULL BOX AND DETECTOR BOX DETAIL**



SHOWING INSTALLATION IN UNPAVED LOCATION      SHOWING INSTALLATION IN PAVED LOCATION      SHOWING INSTALLATION IN UNPAVED LOCATION      SHOWING INSTALLATION IN PAVED LOCATION

**TRAFFIC SIGNAL BASE TYPE 2**



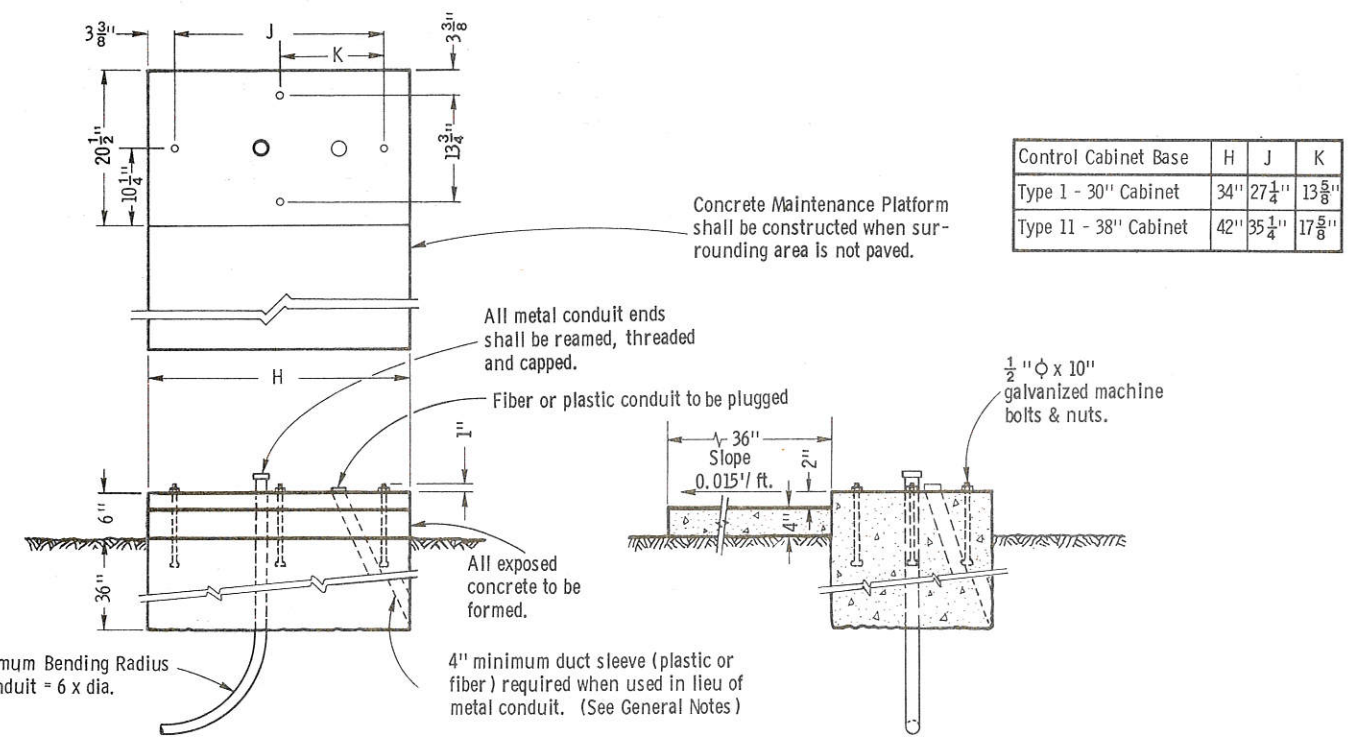
Note: Install as required at points in conduit for drainage.

**DRAIN SUMP FOR METAL CONDUIT**

**TRAFFIC SIGNAL BASE TYPE 1**

**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.  
 Conduit may be metal, fiber or plastic. Locate as required. 12-inch min. bending radius applies to metal conduit only.  
 Concrete masonry shall be grade "AA".  
 Conduit installed as a continuous system between Pullboxes shall have a min. depth of 12 inches and shall always be below the pavement.  
 Detailed drawings for proposed alternate designs for "Traffic Signal and Traffic Counter Details" shall be submitted to the Engineer for approval.



| Control Cabinet Base  | H   | J       | K       |
|-----------------------|-----|---------|---------|
| Type I - 30" Cabinet  | 34" | 27 1/4" | 13 5/8" |
| Type II - 38" Cabinet | 42" | 35 1/4" | 17 5/8" |

**TRAFFIC SIGNAL AND TRAFFIC COUNTER CONTROL CABINET BASE TYPE 1 and 2**

**TRAFFIC SIGNAL AND TRAFFIC COUNTER DETAILS**

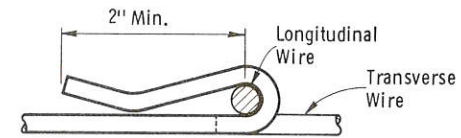
State of Wisconsin  
Department of Transportation  
Division of Highways

RECOMMENDED FOR APPROVAL: *L. C. Hennrich*  
DATE: 4-13-72  
CHIEF DESIGN ENGINEER

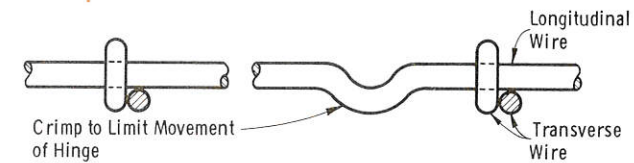
APPROVED: *S. E. Hicks*  
DATE: 4-13-72  
STATE HIGHWAY ENGINEER

S.D.D. 9B3-2

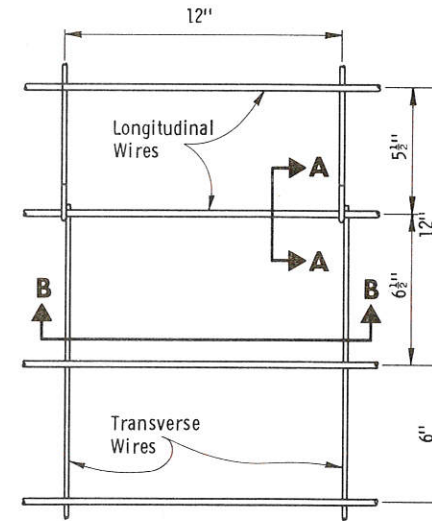




SECTION A-A



SECTION B-B



PLAN VIEW

OPTIONAL HINGE DETAIL

**GENERAL NOTES**

Details of construction and materials not shown hereon shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Alternate hinge designs may be used upon approval of the Engineer.

**WELDED STEEL WIRE FABRIC**

Welded Steel Wire Fabric shall conform to the requirements of the Standard Specification for Welded Steel Wire Fabric for Concrete Reinforcement A. A. S. H. O. Designation M 55 except as shown hereon.

**Welded Steel Wire Fabric Specifications:**

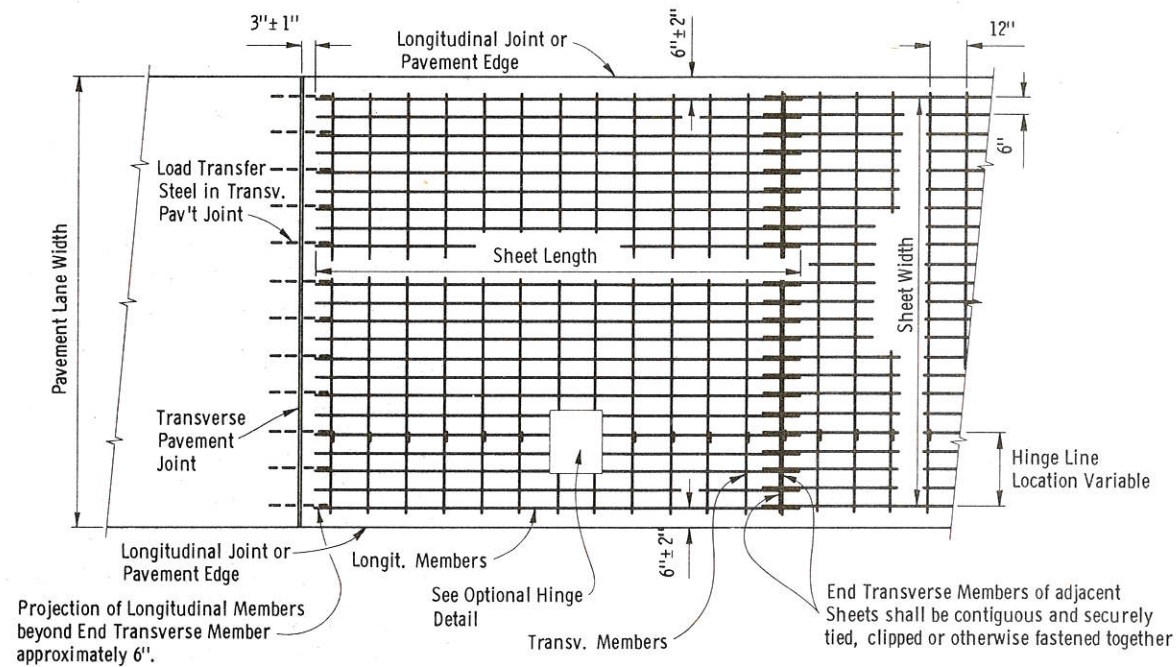
- Approximate weight per 100 sq. ft. = 69.0 lbs.
- Longitudinal Steel - Gage No. 0 = 0.3065" D at 6" C - C.
- Transverse Steel - Gage No. 4 = 0.2253" D at 12" C - C.

Side lap of adjacent sheets shall be approximately 6".

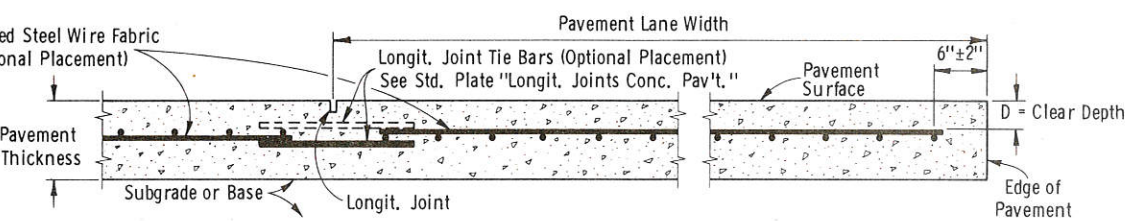
**SPECIAL REQUIREMENTS**

Welded Steel Wire Fabric shall be shipped to the job site in flat sheets.

One longitudinal hinge line will be permitted in each Welded Steel Wire Fabric sheet for convenience in shipping. This hinge shall encircle the longitudinal wire such that no more than one (1) inch of transverse movement of the hinge exists. The longitudinal wire around which the hinge rotates shall be crimped adjacent to the hinge such that no more than one (1) inch of longitudinal movement of the hinge exists.



PLAN VIEW



CROSS SECTION

**WELDED STEEL WIRE FABRIC**

| Pavement Thickness | "D"       |
|--------------------|-----------|
| 8"                 | 2"-4"     |
| 9"                 | 2"-4 1/2" |
| 10"                | 2"-5"     |

**CONCRETE PAVEMENT REINFORCEMENT**

State of Wisconsin  
Department of Transportation  
Division of Highways

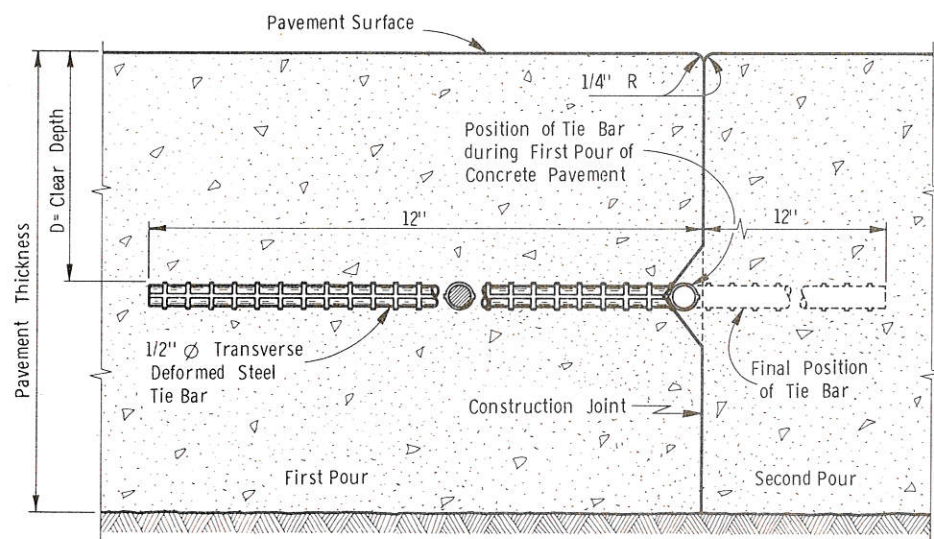
RECOMMENDED FOR APPROVAL:  
DATE 11-30-72

*J.C. Henning*  
CHIEF DESIGN ENGINEER

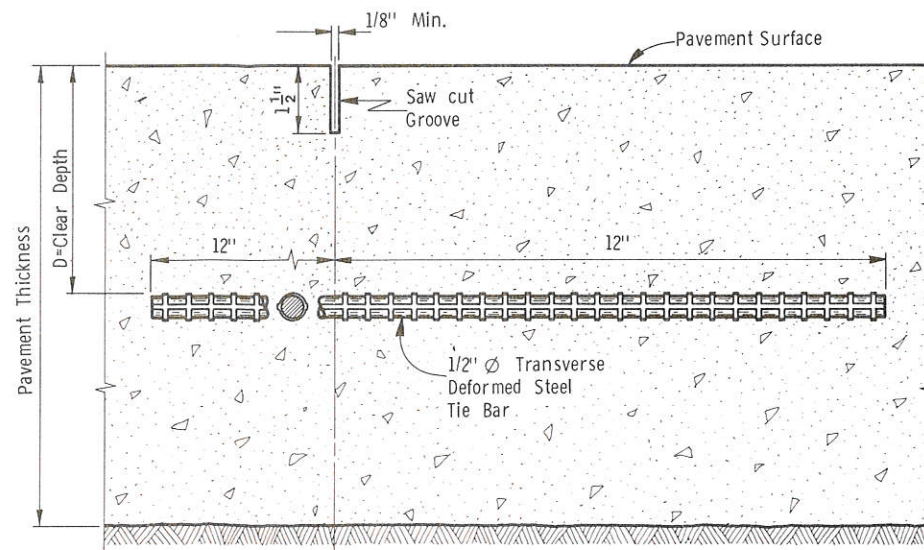
APPROVED  
DATE 12-1-72

*H.S. Siedler*  
STATE HIGHWAY ENGINEER



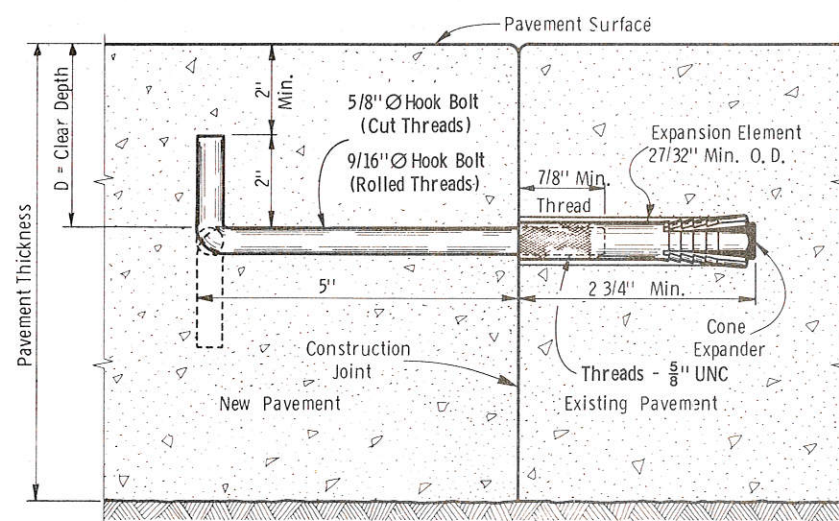


SECTION  
CONSTRUCTION JOINT  
(TIE BAR)

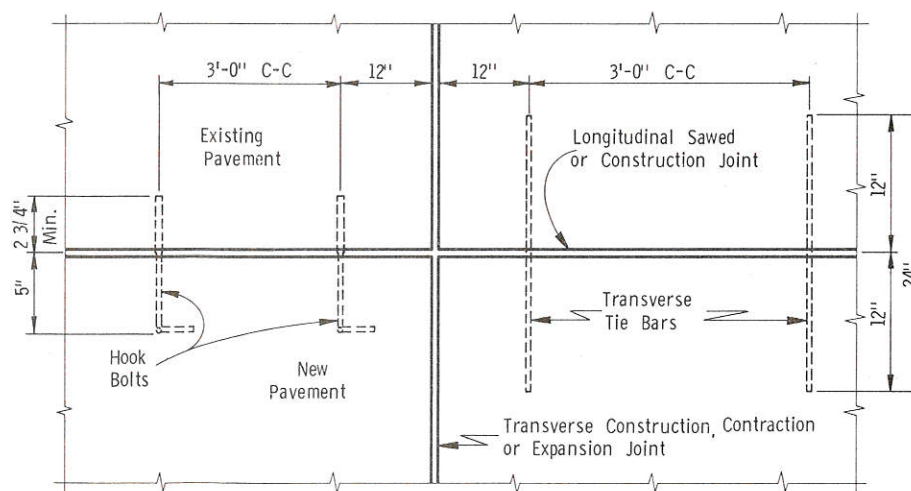


SECTION  
SAWED JOINT  
(TIE BAR)

| Pavement Thickness | "D"        |            |
|--------------------|------------|------------|
|                    | Tie Bar    | Hook Bolt  |
| 8"                 | 2 - 4 3/4" | 4 - 4 3/4" |
| 9"                 | 2 - 5 1/2" | 4 - 5 1/2" |
| 10"                | 2 - 5 3/4" | 4 - 5 3/4" |



SECTION  
CONSTRUCTION JOINT  
(HOOK BOLT)



PLAN VIEW  
Showing Location Details for  
Hook Bolts and Tie Bars

**GENERAL NOTES**

Details of construction, materials and workmanship not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Hook Bolts shall be used only when specified in the contract plans.

The Hook Bolts shall conform to ASTM specification A 307, except that the requirements of paragraph 1 (c) shall not apply.

The Expansion Anchor shall be an internally threaded anchor which consists of an externally slit Expansion Element and a single Cone Expander. The Expansion Element shall contain a minimum of three grips. The Expansion Element shall be threaded in such a manner as to prevent the machine bolt from coming in contact with the Cone Expander at any time.

The Expansion Anchor shall be set in existing pavement according to manufacturer's instructions. The holes shall be of the recommended diameter and depth and shall be drilled by methods recommended by the manufacturer of the particular anchor. The drilled holes shall be left rough, not reamed, and free from any drill dust.

Alternate designs of construction joint installations may be used upon written approval of the Engineer.

Longitudinal Joints shall not be sealed.

Tie Bars shall be placed at the required location by devices or methods approved by the Engineer.

LONGITUDINAL JOINTS  
CONCRETE PAVEMENT

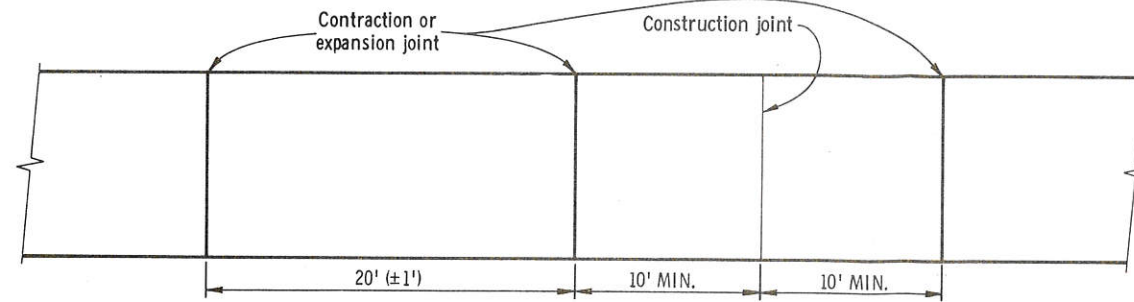
State of Wisconsin  
Department of Transportation  
Division of Highways

RECOMMENDED FOR APPROVAL:  
DATE 5/23/72  
APPROVED  
DATE 5/24/72

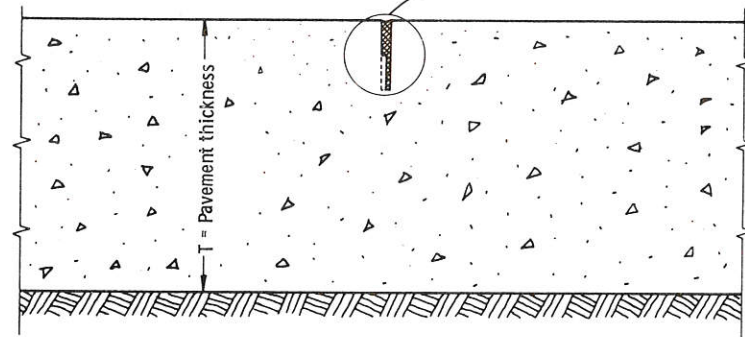
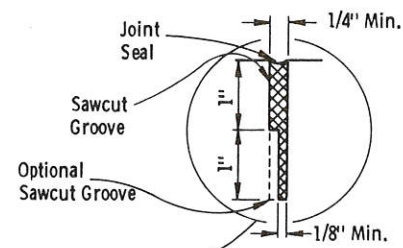
*L. C. Hennrich*  
CHIEF DESIGN ENGINEER

*S. E. Hicks*  
STATE HIGHWAY ENGINEER

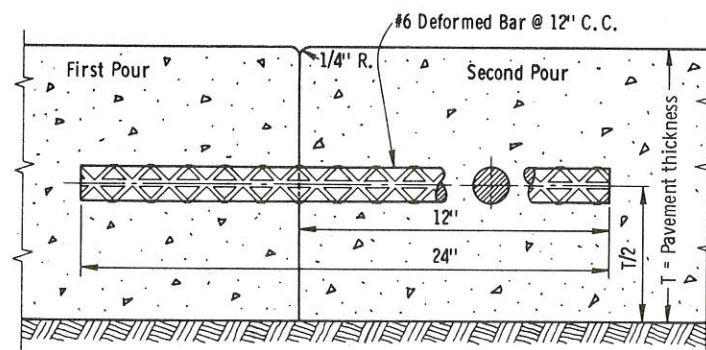




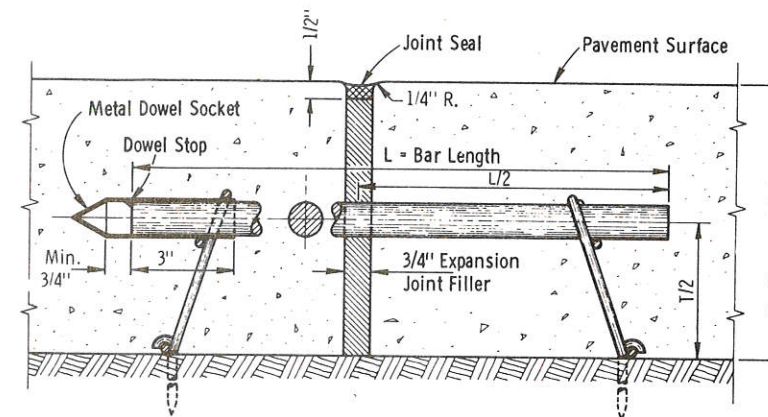
SCHMATIC SHOWING JOINT LOCATIONS



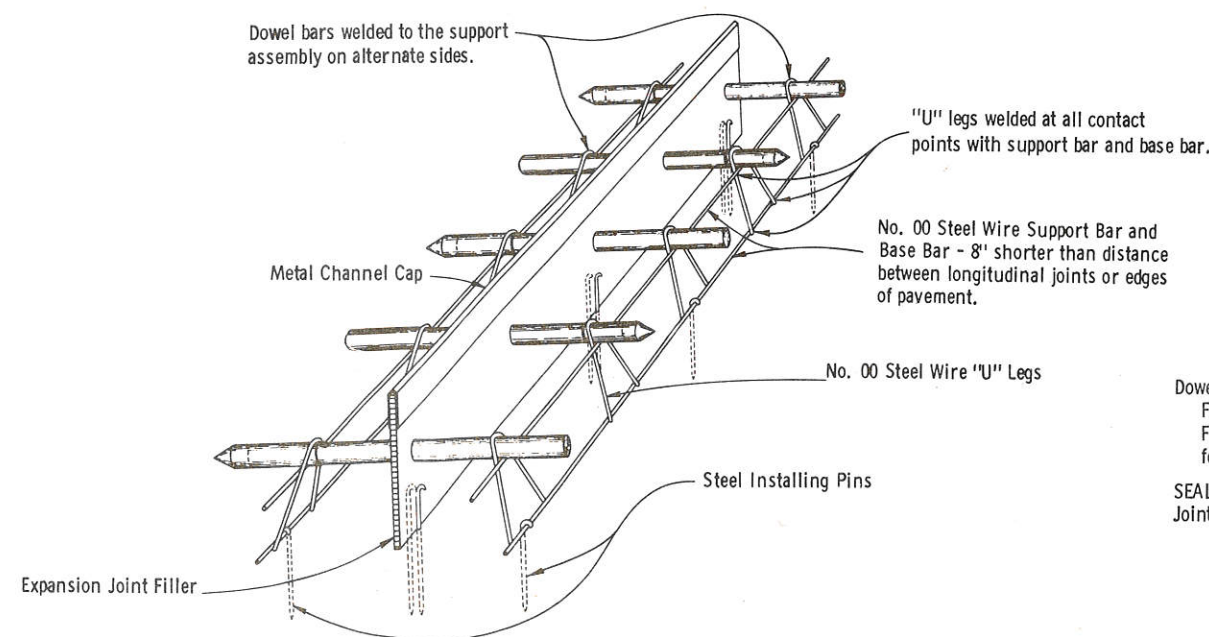
CONTRACTION JOINT



CONSTRUCTION JOINT



EXPANSION JOINT



INSTALLING DEVICE FOR LOAD TRANSFER DOWELS AND EXPANSION JOINT ASSEMBLY

**GENERAL NOTES**

Details of construction not shown on this drawing shall conform to the pertinent requirements of the Standard Specifications and the applicable Special Provisions.

Steel installing pins of sufficient number, length and rigidity shall be used to prevent movement of the joint assembly during construction operations.

**ALTERNATE DESIGNS**

Alternate designs for load transfer dowels at expansion joints and appurtenances other than shown here may be used upon written approval of the Engineer.

**CONTRACTION JOINTS**

Contraction joints shall be installed at 20' (± 1') spacing from adjacent contraction or expansion joints, except that lesser spacings shall be used:

1. At locations or spacing indicated on the plans.
2. As extensions of transverse joints or cracks in abutting pavement lanes.
3. At locations designated by the Engineer where there are manholes or other fixtures in the pavement.

**CONSTRUCTION JOINTS**

Construction joints shall be installed a minimum of 10' from the nearest joint.

Deformed bars shall be spaced at 12" C-C and 6" from the edge of pavement.

Deformed bars may be inserted after the concrete has been poured.

**EXPANSION JOINTS**

Expansion joints are required only at structure approaches and/or where shown on the plans. Locations may be shifted to avoid stationary fixtures in the pavement.

Expansion joint filler shall be secured with sufficient number of steel pins to prevent horizontal movement during the placing of concrete.

**DOWEL BARS**

Dowel bars shall be spaced at 12" C-C and 6" from the edge of pavement.

Dowel bars shall have at least one end sawed and be free of all burrs and protruding edges.

Dowel bars having one end sawed and one end sheared shall be oriented so that the sheared end is welded to the support assembly and the sawed end remains free.

Metal dowel socket (CAP), 1 1/8" or 1 5/8" Dia., 24 gauge, closed on one end shall be placed alternately on the free end of each dowel bar for proper pavement expansion.

Dowel bars shall be installed in accordance with the plans and the section of the Standard Specifications entitled "TRANSVERSE JOINTS IN CONCRETE PAVEMENT" except as hereinafter provided.

Dowel bars shall be coated by one of the following processes:

1. Type I - Adhesive thermoplastic resin system coating in accordance with Federal Specification L - C530 B except the coating thickness shall be 17 mils nominal (± 3 mils) and the adhesive thickness shall be 4 mils nominal (+ 4 mils, - 1 mil), or
2. Type II - Thermosetting epoxy system in accordance with Federal Specification L - C530B except the total minimum thickness shall be 10 mils. The bars shall be coated with SAE # 140 oil or similar lubricant after installing in the support assembly.

The ends of the dowel bars need not be coated.

Coating of the welds where the dowel bars are attached to the support assembly is not required.

Selection of Type I or Type II coating is optional; however, one type shall be used throughout the project.

Dowel Bar Dimensions:  
 For 7" P. C. Pavement = 1" x 18"  
 For 8" P. C. Pavement = 1" x 18"  
 For 9" P. C. Pavement = 1 1/4" x 18"

SEALING JOINTS  
 Joints shall be sealed as shown.

**TRANSVERSE JOINTS  
 IN NON-REINFORCED  
 CONCRETE PAVEMENT**

State of Wisconsin  
 Department of Transportation  
 Division of Highways

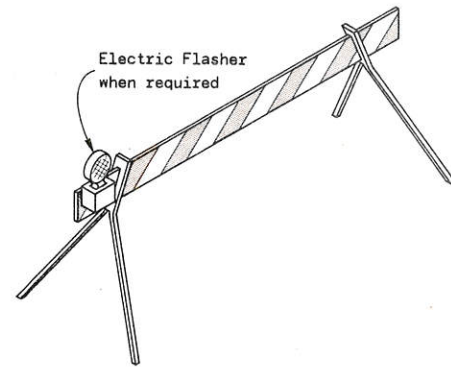
RECOMMENDED FOR APPROVAL:  
 DATE 6-12-73  
 APPROVED: J.C. Henning  
 DATE 6-19-73  
 STATE HIGHWAY ENGINEER



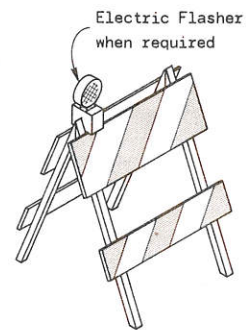
TABLE OF BARRICADE CHARACTERISTICS

| BARRICADE TYPE  | I                                       | II | III                |
|-----------------|---|----|--------------------|
| Height          | 3' (91.4 cm) Min.                       |    | 5' (152.4 cm) Min. |
| * Rail Width    | 8" (20.3 cm) Min. to 12" (30.5 cm) Max. |    |                    |
| Rail Length     | 2' (61.0 cm) Min. to Variable Maximum   |    |                    |
| ** Stripe Width | 6" (15.2 cm) at 45° Angle               |    |                    |
| Stripe Colors   | Reflectorized Orange & White            |    |                    |

\* Nominal dimensions when barricade is constructed of lumber.  
 \*\* May be 4" (10.2 cm) for rail lengths less than 3' (91.4 cm).



TYPICAL TYPE I BARRICADE

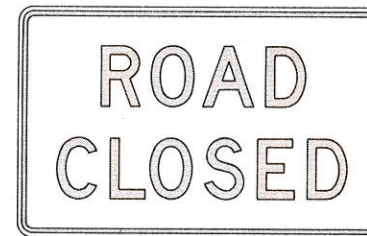


TYPICAL TYPE II BARRICADE



**W20-3**  
 48" (121.9 cm) x 48" (121.9 cm)  
 Black Lettering on Reflective Orange Background  
 Letter Series "D"  
 Letter height 7" (17.8 cm)

STANDARD SIGNS-TYPE II



**R11-2**  
 48" (121.9 cm) x 30" (76.2 cm)  
 Black Lettering on Reflective White Background  
 Letter Series "D"  
 Letter height 8" (20.3 cm)

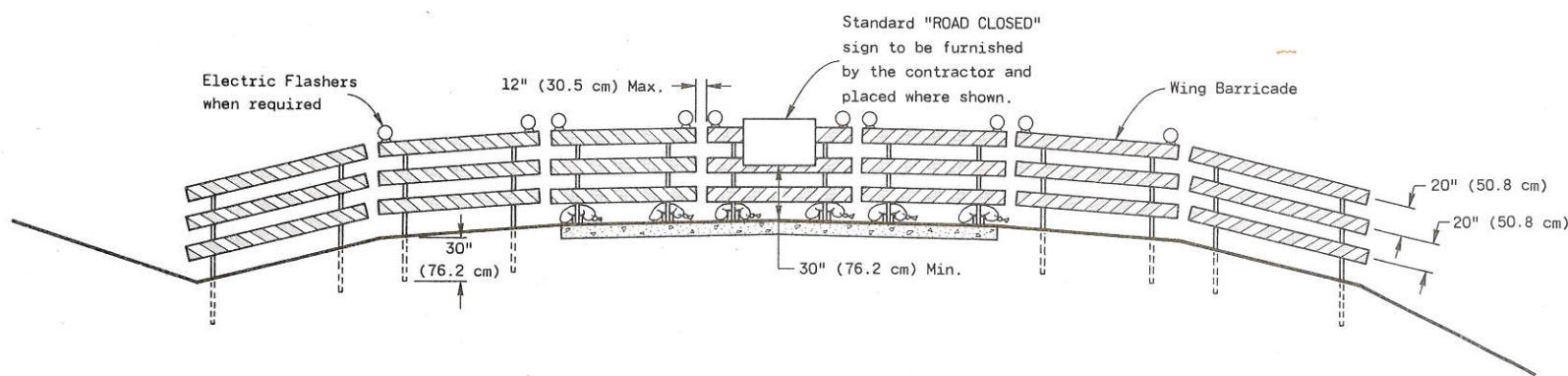
**GENERAL NOTES**

The contractor shall furnish, erect and maintain Barricades and Signs. Details regarding location, spacing, dimensions, fabrication, material, sign lettering, lighting devices and color of Barricades and Signs shall conform to this drawing, the Wisconsin Manual on Uniform Traffic Control Devices, the Standard Specifications, Special Provisions and/or plans.

Type III Barricades and Signs shall be erected at the termini of projects and at other road or street locations where it is necessary to control or eliminate public access to the construction area.

Type I and II Barricades shall be used on projects when traffic is to be maintained through the construction area.

The actual field location of barricade installations and advance signs shall be as directed by the Engineer.



TYPICAL INSTALLATION SHOWING TYPE III BARRICADE

**CONSTRUCTION BARRICADES**

**CONSTRUCTION BARRICADES & STANDARD SIGNS**

State of Wisconsin  
 Department of Transportation  
 Division of Highways

RECOMMENDED FOR APPROVAL:

6-6-75  
 DATE

*J.C. Henning*  
 CHIEF OF FACILITIES DEVELOPMENT

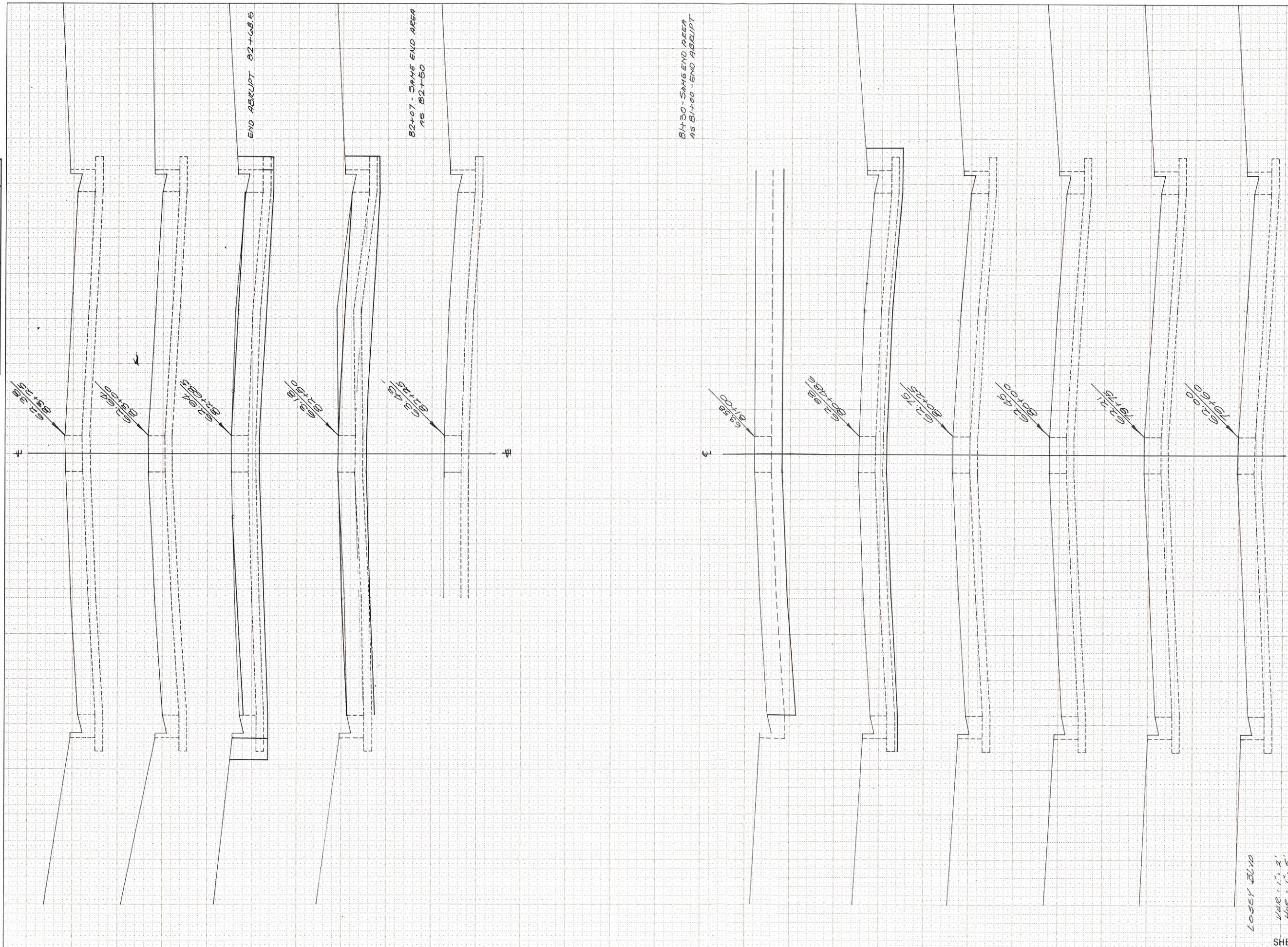
6-6-75  
 DATE

*H. J. Siedler*  
 STATE HIGHWAY ENGINEER



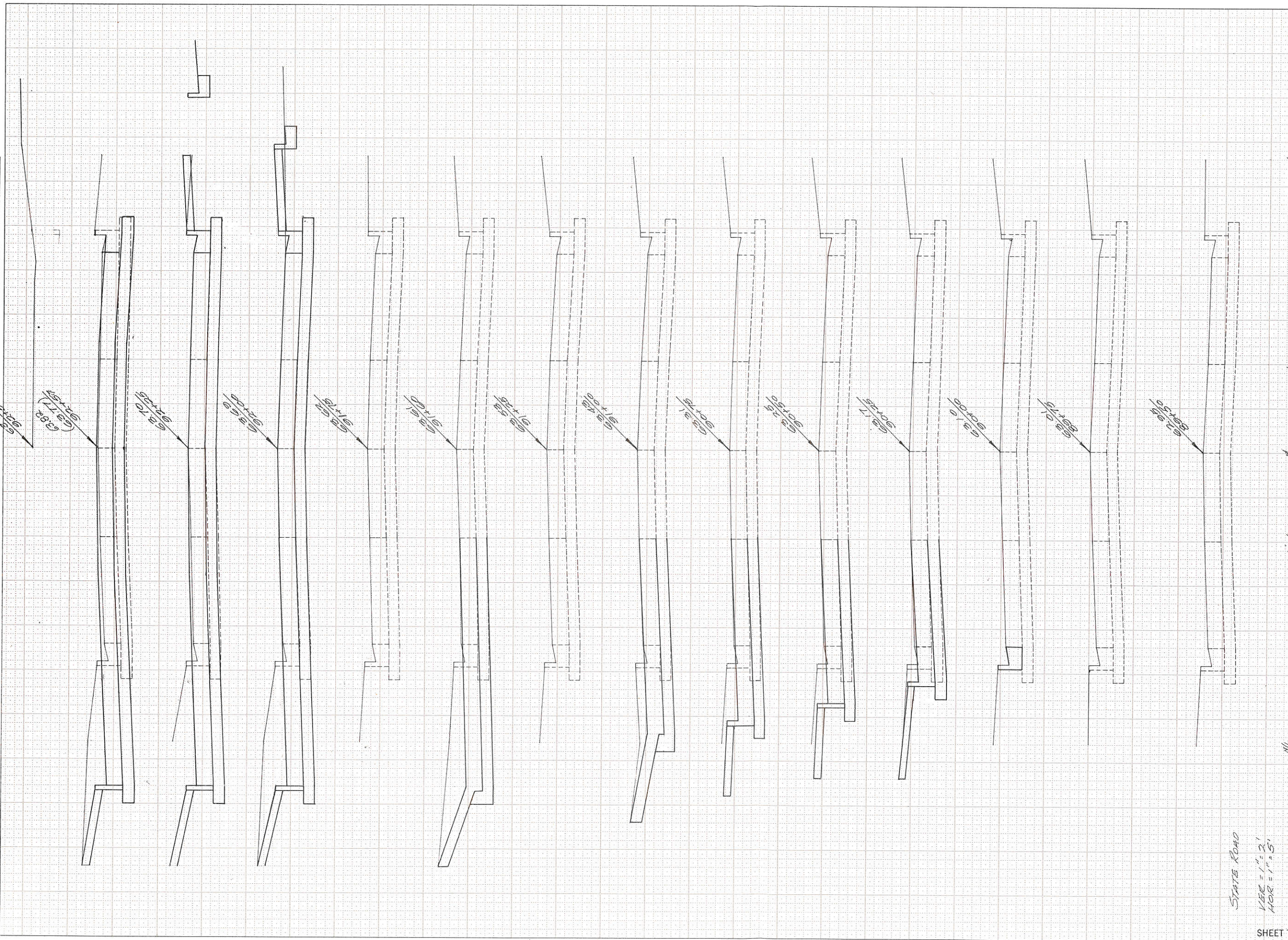
FINAL SURVEY  
 SURVEYED: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_

ORIGINAL SURVEY  
 SURVEYED: \_\_\_\_\_  
 TEMPLATE: \_\_\_\_\_  
 NOTE BOOK: \_\_\_\_\_  
 AREAS CHECKED: \_\_\_\_\_  
 NO. \_\_\_\_\_



| PROJECT DESIGNATION |          | SHEET NUMBER |      |
|---------------------|----------|--------------|------|
| 5991-0-14           |          | 8            |      |
| STATION             | DISTANCE | YARDAGE      |      |
|                     |          | EXCAVATION   | FILL |
| 80 + 48.6           |          |              |      |
| 81 + 00             | 63       |              |      |
| 81 + 30             | 33       |              |      |
| 82 + 01             | 55       |              |      |
| 82 + 50             | 14       |              |      |
| BUS STOPS           | 245      |              |      |
| SHEET TOTAL         | 410      |              |      |



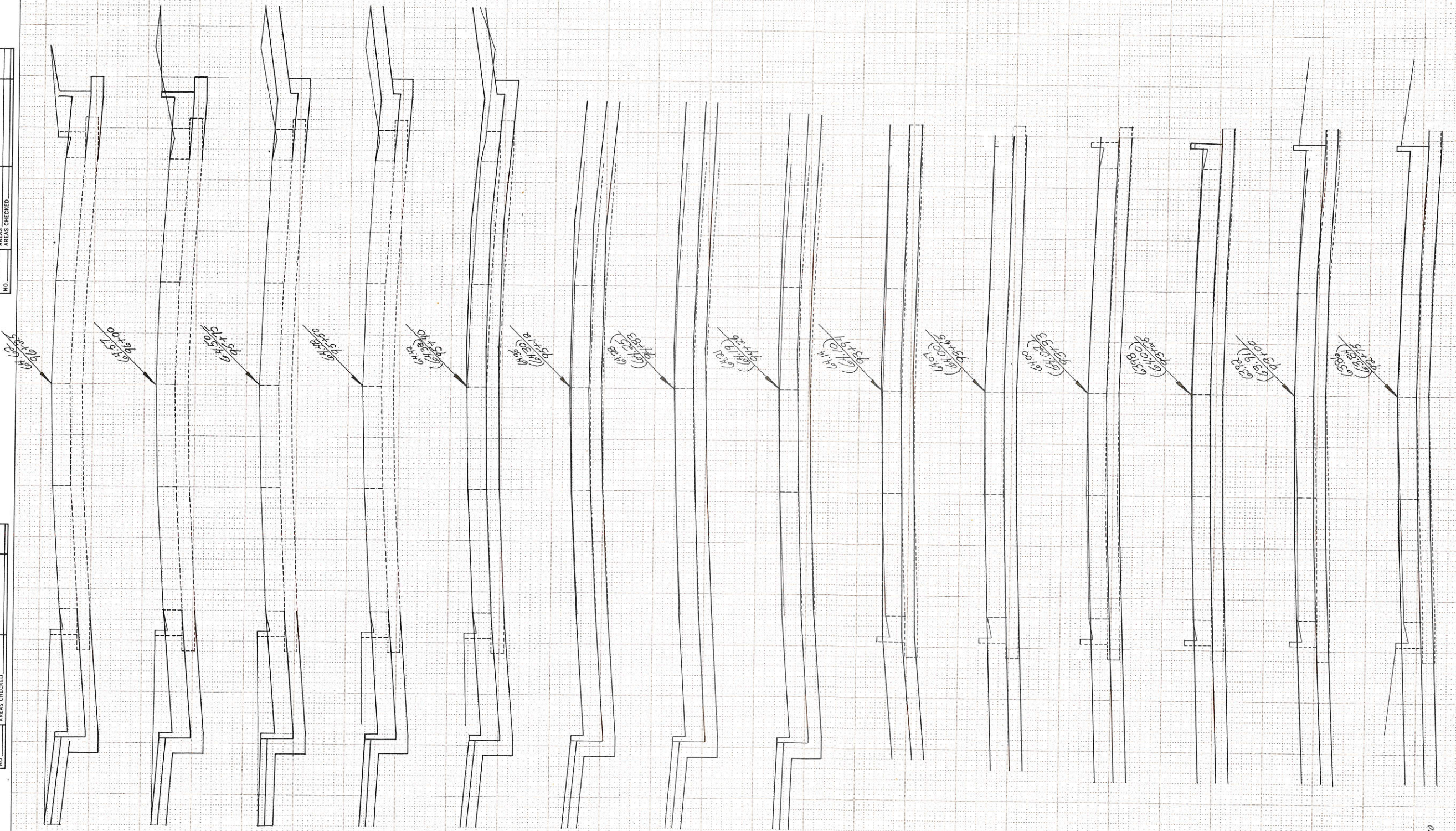


| STATION | DISTANCE | YARDAGE    |    |      |
|---------|----------|------------|----|------|
|         |          | EXCAVATION |    | FILL |
|         |          | UNCL       |    |      |
| 92157   | 320'     | 69         |    |      |
| 92125   | 250'     | 51         |    |      |
| 92100   | 250'     | 76         |    |      |
| 91175   | 150'     |            | 78 |      |
| 91150   | 350'     |            |    | 78   |
| 91125   | 250'     |            |    |      |
| 91100   | 250'     | 24         |    |      |
| 90175   | 250'     |            |    |      |
| 90150   | 250'     |            |    |      |
| 90125   | 250'     | 1          |    |      |
| 90100   | 250'     | 4          |    |      |
| 89175   | 250'     |            |    |      |
| 89150   | 250'     |            |    |      |
| 89125   | 250'     |            |    |      |
| 89100   | 250'     |            |    |      |
| 88175   | 250'     |            |    |      |
| 88150   | 250'     |            |    |      |
| 88125   | 250'     |            |    |      |
| 88100   | 250'     |            |    |      |
| 87175   | 250'     |            |    |      |
| 87150   | 250'     |            |    |      |
| 87125   | 250'     |            |    |      |
| 87100   | 250'     |            |    |      |
| 86175   | 250'     |            |    |      |
| 86150   | 250'     |            |    |      |
| 86125   | 250'     |            |    |      |
| 86100   | 250'     |            |    |      |
| 85175   | 250'     |            |    |      |
| 85150   | 250'     |            |    |      |
| 85125   | 250'     |            |    |      |
| 85100   | 250'     |            |    |      |
| 84175   | 250'     |            |    |      |
| 84150   | 250'     |            |    |      |
| 84125   | 250'     |            |    |      |
| 84100   | 250'     |            |    |      |
| 83175   | 250'     |            |    |      |
| 83150   | 250'     |            |    |      |
| 83125   | 250'     |            |    |      |
| 83100   | 250'     |            |    |      |
| 82175   | 250'     |            |    |      |
| 82150   | 250'     |            |    |      |
| 82125   | 250'     |            |    |      |
| 82100   | 250'     |            |    |      |
| 81175   | 250'     |            |    |      |
| 81150   | 250'     |            |    |      |
| 81125   | 250'     |            |    |      |
| 81100   | 250'     |            |    |      |
| 80175   | 250'     |            |    |      |
| 80150   | 250'     |            |    |      |
| 80125   | 250'     |            |    |      |
| 80100   | 250'     |            |    |      |
| 79175   | 250'     |            |    |      |
| 79150   | 250'     |            |    |      |
| 79125   | 250'     |            |    |      |
| 79100   | 250'     |            |    |      |
| 78175   | 250'     |            |    |      |
| 78150   | 250'     |            |    |      |
| 78125   | 250'     |            |    |      |
| 78100   | 250'     |            |    |      |
| 77175   | 250'     |            |    |      |
| 77150   | 250'     |            |    |      |
| 77125   | 250'     |            |    |      |
| 77100   | 250'     |            |    |      |
| 76175   | 250'     |            |    |      |
| 76150   | 250'     |            |    |      |
| 76125   | 250'     |            |    |      |
| 76100   | 250'     |            |    |      |
| 75175   | 250'     |            |    |      |
| 75150   | 250'     |            |    |      |
| 75125   | 250'     |            |    |      |
| 75100   | 250'     |            |    |      |
| 74175   | 250'     |            |    |      |
| 74150   | 250'     |            |    |      |
| 74125   | 250'     |            |    |      |
| 74100   | 250'     |            |    |      |
| 73175   | 250'     |            |    |      |
| 73150   | 250'     |            |    |      |
| 73125   | 250'     |            |    |      |
| 73100   | 250'     |            |    |      |
| 72175   | 250'     |            |    |      |
| 72150   | 250'     |            |    |      |
| 72125   | 250'     |            |    |      |
| 72100   | 250'     |            |    |      |
| 71175   | 250'     |            |    |      |
| 71150   | 250'     |            |    |      |
| 71125   | 250'     |            |    |      |
| 71100   | 250'     |            |    |      |
| 70175   | 250'     |            |    |      |
| 70150   | 250'     |            |    |      |
| 70125   | 250'     |            |    |      |
| 70100   | 250'     |            |    |      |
| 69175   | 250'     |            |    |      |
| 69150   | 250'     |            |    |      |
| 69125   | 250'     |            |    |      |
| 69100   | 250'     |            |    |      |
| 68175   | 250'     |            |    |      |
| 68150   | 250'     |            |    |      |
| 68125   | 250'     |            |    |      |
| 68100   | 250'     |            |    |      |
| 67175   | 250'     |            |    |      |
| 67150   | 250'     |            |    |      |
| 67125   | 250'     |            |    |      |
| 67100   | 250'     |            |    |      |
| 66175   | 250'     |            |    |      |
| 66150   | 250'     |            |    |      |
| 66125   | 250'     |            |    |      |
| 66100   | 250'     |            |    |      |
| 65175   | 250'     |            |    |      |
| 65150   | 250'     |            |    |      |
| 65125   | 250'     |            |    |      |
| 65100   | 250'     |            |    |      |
| 64175   | 250'     |            |    |      |
| 64150   | 250'     |            |    |      |
| 64125   | 250'     |            |    |      |
| 64100   | 250'     |            |    |      |
| 63175   | 250'     |            |    |      |
| 63150   | 250'     |            |    |      |
| 63125   | 250'     |            |    |      |
| 63100   | 250'     |            |    |      |
| 62175   | 250'     |            |    |      |
| 62150   | 250'     |            |    |      |
| 62125   | 250'     |            |    |      |
| 62100   | 250'     |            |    |      |
| 61175   | 250'     |            |    |      |
| 61150   | 250'     |            |    |      |
| 61125   | 250'     |            |    |      |
| 61100   | 250'     |            |    |      |
| 60175   | 250'     |            |    |      |
| 60150   | 250'     |            |    |      |
| 60125   | 250'     |            |    |      |
| 60100   | 250'     |            |    |      |
| 59175   | 250'     |            |    |      |
| 59150   | 250'     |            |    |      |
| 59125   | 250'     |            |    |      |
| 59100   | 250'     |            |    |      |
| 58175   | 250'     |            |    |      |
| 58150   | 250'     |            |    |      |
| 58125   | 250'     |            |    |      |
| 58100   | 250'     |            |    |      |
| 57175   | 250'     |            |    |      |
| 57150   | 250'     |            |    |      |
| 57125   | 250'     |            |    |      |
| 57100   | 250'     |            |    |      |
| 56175   | 250'     |            |    |      |
| 56150   | 250'     |            |    |      |
| 56125   | 250'     |            |    |      |
| 56100   | 250'     |            |    |      |
| 55175   | 250'     |            |    |      |
| 55150   | 250'     |            |    |      |
| 55125   | 250'     |            |    |      |
| 55100   | 250'     |            |    |      |
| 54175   | 250'     |            |    |      |
| 54150   | 250'     |            |    |      |
| 54125   | 250'     |            |    |      |
| 54100   | 250'     |            |    |      |
| 53175   | 250'     |            |    |      |
| 53150   | 250'     |            |    |      |
| 53125   | 250'     |            |    |      |
| 53100   | 250'     |            |    |      |
| 52175   | 250'     |            |    |      |
| 52150   | 250'     |            |    |      |
| 52125   | 250'     |            |    |      |
| 52100   | 250'     |            |    |      |
| 51175   | 250'     |            |    |      |
| 51150   | 250'     |            |    |      |
| 51125   | 250'     |            |    |      |
| 51100   | 250'     |            |    |      |
| 50175   | 250'     |            |    |      |
| 50150   | 250'     |            |    |      |
| 50125   | 250'     |            |    |      |
| 50100   | 250'     |            |    |      |
| 49175   | 250'     |            |    |      |
| 49150   | 250'     |            |    |      |
| 49125   | 250'     |            |    |      |
| 49100   | 250'     |            |    |      |
| 48175   | 250'     |            |    |      |
| 48150   | 250'     |            |    |      |
| 48125   | 250'     |            |    |      |
| 48100   | 250'     |            |    |      |
| 47175   | 250'     |            |    |      |
| 47150   | 250'     |            |    |      |
| 47125   | 250'     |            |    |      |
| 47100   | 250'     |            |    |      |
| 46175   | 250'     |            |    |      |
| 46150   | 250'     |            |    |      |
| 46125   | 250'     |            |    |      |
| 46100   | 250'     |            |    |      |
| 45175   | 250'     |            |    |      |
| 45150   | 250'     |            |    |      |
| 45125   | 250'     |            |    |      |
| 45100   | 250'     |            |    |      |
| 44175   | 250'     |            |    |      |
| 44150   | 250'     |            |    |      |
| 44125   | 250'     |            |    |      |
| 44100   | 250'     |            |    |      |
| 43175   | 250'     |            |    |      |
| 43150   | 250'     |            |    |      |
| 43125   | 250'     |            |    |      |
| 43100   | 250'     |            |    |      |
| 42175   | 250'     |            |    |      |
| 42150   | 250'     |            |    |      |
| 42125   | 250'     |            |    |      |
| 42100   | 250'     |            |    |      |
| 41175   | 250'     |            |    |      |
| 41150   | 250'     |            |    |      |
| 41125   | 250'     |            |    |      |
| 41100   | 250'     |            |    |      |
| 40175   | 250'     |            |    |      |
| 40150   | 250'     |            |    |      |
| 40125   | 250'     |            |    |      |
| 40100   | 250'     |            |    |      |
| 39175   | 250'     |            |    |      |
| 39150   | 250'     |            |    |      |
| 39125   | 250'     |            |    |      |
| 39100   | 250'     |            |    |      |
| 38175   | 250'     |            |    |      |
| 38150   | 250'     |            |    |      |
| 38125   | 250'     |            |    |      |
| 38100   | 250'     |            |    |      |
| 37175   | 250'     |            |    |      |
| 37150   | 250'     |            |    |      |
| 37125   | 250'     |            |    |      |
| 37100   | 250'     |            |    |      |
| 36175   | 250'     |            |    |      |
| 36150   | 250'     |            |    |      |
| 36125   | 250'     |            |    |      |
| 36100   | 250'     |            |    |      |
| 35175   | 250'     |            |    |      |
| 35150   | 250'     |            |    |      |
| 35125   | 250'     |            |    |      |
| 35100   | 250'     |            |    |      |
| 34175   | 250'     |            |    |      |
| 34150   | 250'     |            |    |      |
| 34125   | 250'     |            |    |      |
| 34100   | 250'     |            |    |      |
| 33175   | 250'     |            |    |      |
| 33150   | 250'     |            |    |      |
| 33125   | 250'     |            |    |      |
| 33100   | 250'     |            |    |      |
| 32175   | 250'     |            |    |      |
| 32150   | 250'     |            |    |      |
| 32125   | 250'     |            |    |      |
| 32100   | 250'     |            |    |      |
| 31175   | 250'     |            |    |      |
| 31150   | 250'     |            |    |      |
| 31125   | 250'     |            |    |      |
| 31100   | 250'     |            |    |      |
| 30175   | 250'     |            |    |      |
| 30150   | 250'     |            |    |      |
| 30125   | 250'     |            |    |      |
| 30100   | 250'     |            |    |      |
| 29175   | 250'     |            |    |      |
| 29150   | 250'     |            |    |      |
| 29125   | 250'     |            |    |      |
| 29100   | 250'     |            |    |      |
| 28175   | 250'     |            |    |      |
| 28150   | 250'     |            |    |      |
| 28125   | 250'     |            |    |      |
| 28100   | 250'     |            |    |      |
| 27175   | 250'     |            |    |      |
| 27150   | 250'     |            |    |      |
| 27125   | 250'     |            |    |      |
| 27100   | 250'     |            |    |      |
| 26175   | 250'     |            |    |      |
| 26150   | 250'     |            |    |      |
| 26125   | 250'     |            |    |      |
| 26100   | 250'     |            |    |      |
| 25175   | 250'     |            |    |      |
| 25150   | 250'     |            |    |      |
| 25125   | 250'     |            |    |      |
| 25100   | 250'     |            |    |      |
| 24175   | 250'     |            |    |      |
| 24150   | 250'     |            |    |      |
| 24125   | 250'     |            |    |      |
| 24100   | 250'     |            |    |      |
| 23175   | 250'     |            |    |      |
| 23150   | 250'     |            |    |      |
| 23125   | 250'     |            |    |      |
| 23100   | 250'     |            |    |      |
| 22175   | 250'     |            |    |      |
| 22150   | 250'     |            |    |      |
| 22125   | 250'     |            |    |      |
| 22100   | 250'     |            |    |      |
| 21175   | 250'     |            |    |      |
| 21150   | 250'     |            |    |      |
| 21125   | 250'     |            |    |      |
| 21100   |          |            |    |      |



|              |    |      |
|--------------|----|------|
| FINAL SURVEY | BY | DATE |
| SURVEYED     |    |      |
| PLOTTED      |    |      |
| NOTE BOOK    |    |      |
| NO.          |    |      |

|                 |    |      |
|-----------------|----|------|
| ORIGINAL SURVEY | BY | DATE |
| SURVEYED        |    |      |
| PLOTTED         |    |      |
| NOTE BOOK       |    |      |
| NO.             |    |      |



| STATION     | DISTANCE | YARDAGE    |  |      |
|-------------|----------|------------|--|------|
|             |          | EXCAVATION |  | FILL |
|             |          | UNCL.      |  |      |
| 92+57       |          |            |  |      |
| 92+75       | 34       |            |  |      |
| 92+100      | 34       |            |  |      |
| 92+25       | 26       |            |  |      |
| 92+33       | 0        |            |  |      |
| 92+65       | 33       |            |  |      |
| 93+94       | 31       |            |  |      |
| 94+26       | 36       |            |  |      |
| 94+71       | 35       |            |  |      |
| 95+12       | 37       |            |  |      |
| 95+40       | 44       |            |  |      |
| 95+50       | 17       |            |  |      |
| 95+75       | 37       |            |  |      |
| 96+00       | 36       |            |  |      |
| 96+25       | 34       |            |  |      |
| STATE ROAD  |          |            |  |      |
| VER 1"=2'   |          |            |  |      |
| HOR 1"=5'   |          |            |  |      |
| SHEET TOTAL |          |            |  | 443  |

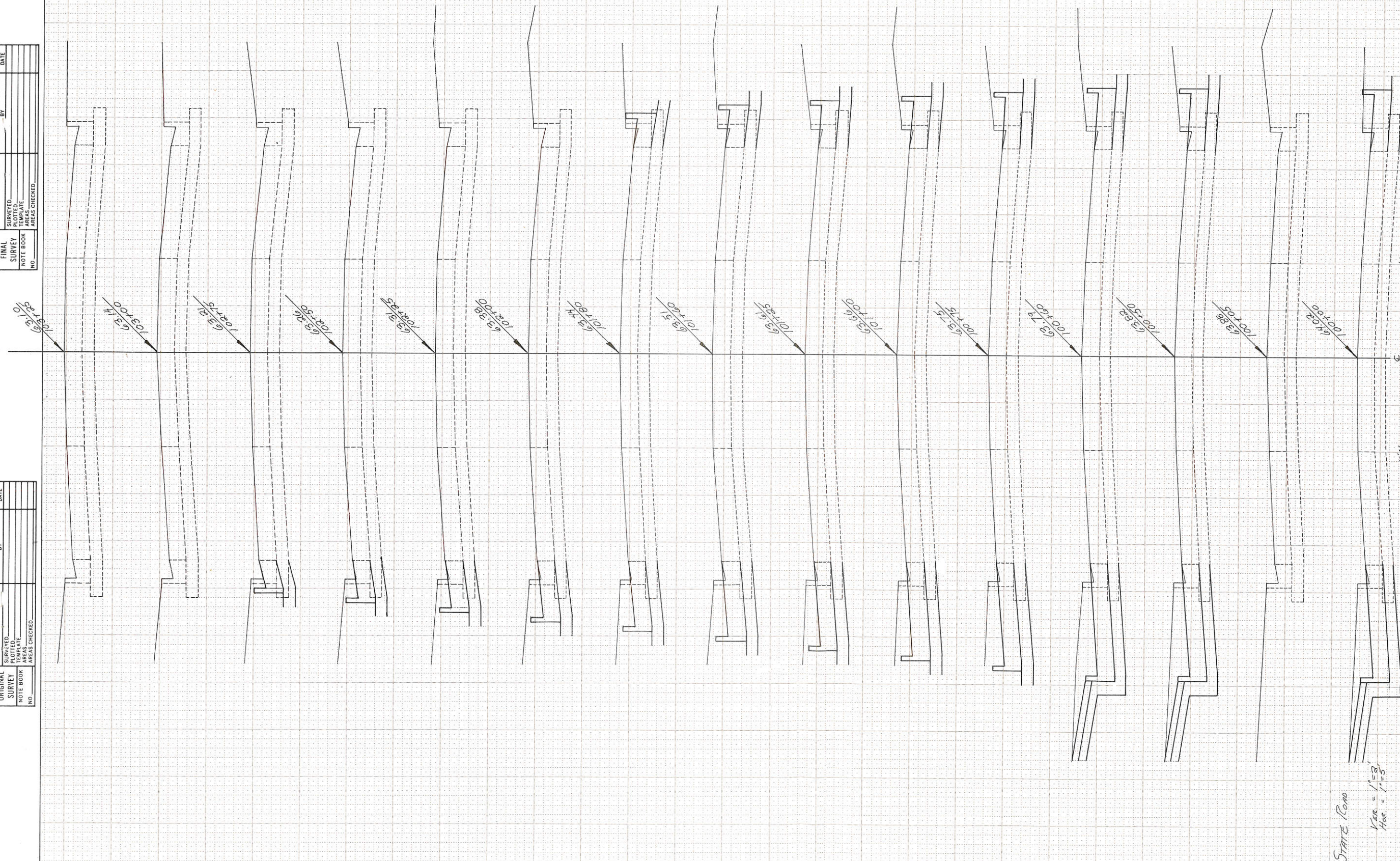






|               |    |      |
|---------------|----|------|
| FINAL SURVEY  | BY | DATE |
| SURVEYED      |    |      |
| PLOTTED       |    |      |
| TEMPLATE      |    |      |
| NO.           |    |      |
| AREAS CHECKED |    |      |

|                 |    |      |
|-----------------|----|------|
| ORIGINAL SURVEY | BY | DATE |
| SURVEYED        |    |      |
| PLOTTED         |    |      |
| TEMPLATE        |    |      |
| NO.             |    |      |
| AREAS CHECKED   |    |      |



| STATION            | DISTANCE   | YARDAGE    |      |
|--------------------|------------|------------|------|
|                    |            | EXCAVATION |      |
|                    |            | UNCL.      | FILL |
| 99+75              | 72         |            |      |
| 100+00             |            |            |      |
| 100+05             |            |            |      |
| 100+50             | 72         |            |      |
| 100+60             | 14         |            |      |
| 100+75             | 19         |            |      |
| 101+00             | 26         |            |      |
| 101+25             |            |            |      |
| 101+60             | 53         |            |      |
| 101+80             | 14         |            |      |
| 102+00             | 11         |            |      |
| 102+25             |            |            |      |
| 102+50             | 17         |            |      |
| 102+75             | 6          |            |      |
| 103+00             | 3          |            |      |
| 103+25             |            |            |      |
| <b>SHEET TOTAL</b> | <b>307</b> |            |      |

STATE ROAD  
 V<sub>ER</sub> = 1" = 3'  
 H<sub>OR</sub> = 1" = 5'



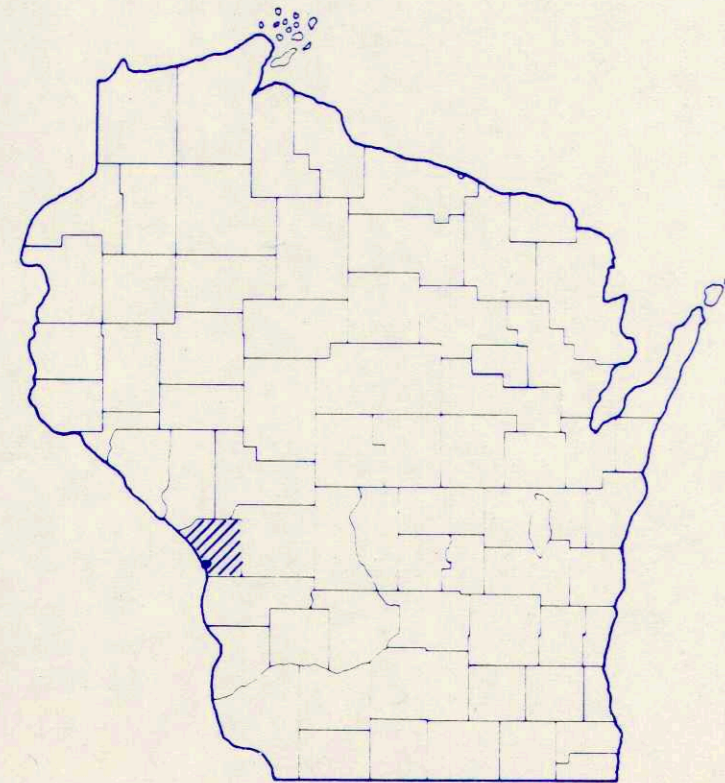
PLAN 229



|              |              |
|--------------|--------------|
| Sheet Number | Total Sheets |
| 4.0          |              |

Index of Sheets

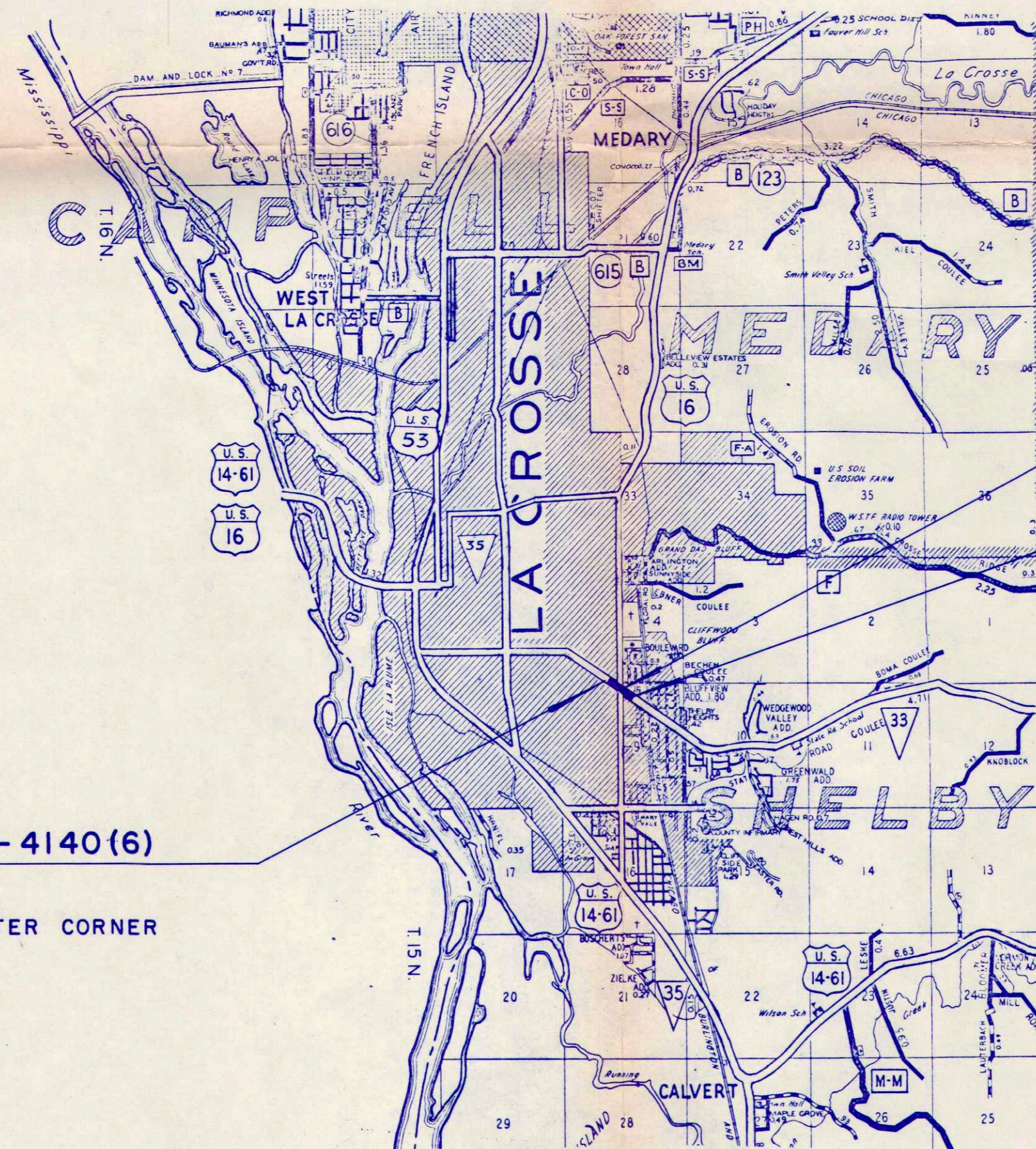
Sheet No. 4.0 TITLE  
 Sheet No. 4.1 PLAT & SCHEDULE  
 Sheet No.



STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS

PLAT OF RIGHT OF WAY REQUIRED FOR  
**LOSEY BLVD - CITY OF LA CROSSE**  
 STATE ROAD INTERSECTION  
 LA CROSSE COUNTY  
 TOPICS

| PROJECT IDENTIFICATION NUMBER | FEDERAL PROJECT DESIGNATION |
|-------------------------------|-----------------------------|
| 5991-0-13                     | EHST-4140(6)                |



EQUATION 94+42.2 BK. =  
 94+71.1 AH.

END RELOCATION ORDER PROJ. 5991-0-13/EHST-4140(6)  
 STATION 102+25  
 ± 870 FEET SOUTH AND ± 605 FEET EAST OF THE NORTH ONE-QUARTER CORNER  
 OF SECTION 9, T15N, R7W.

BEGIN RELOCATION ORDER PROJ. 5991-0-13/EHST-4140(6)  
 STATION 90+00  
 ± 165 FEET SOUTH AND ± 360 FEET WEST OF THE NORTH ONE-QUARTER CORNER  
 OF SECTION 9, T15N, R7W.

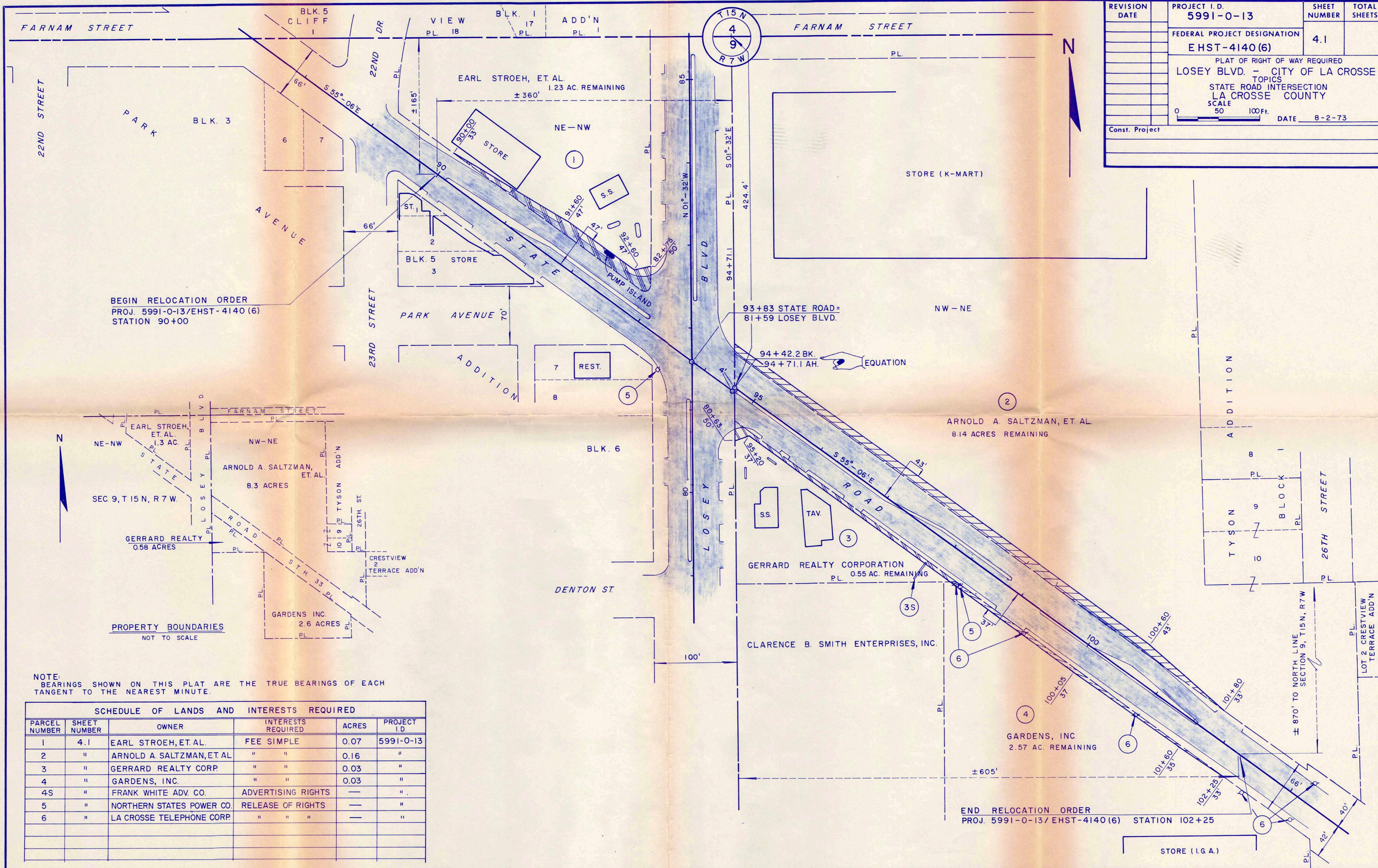
Layout  
 Scale 0 1/2 1 MILE

Total Net Length of Centerline = 0.227 Mi.

PLAN NO. 229

| REVISION DATE | STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>DIVISION OF HIGHWAYS   |
|---------------|--|
| Date 8-2-73   | <i>Barbara D. Millerbach</i><br>CITY ENGINEER  |
| Date _____    | Recommended for Approval:<br>Chief of Facilities Development   |
| Date _____    | Approved: _____<br>Director, Bureau of Right of Way  |
| Date _____    | U.S. DEPARTMENT OF TRANSPORTATION<br>FEDERAL HIGHWAY ADMINISTRATION<br>BUREAU OF PUBLIC ROADS<br>REGION 4 WISCONSIN DIVISION |
| Date _____    | Approved: _____<br>Division Engineer   |





|                |   |                     |              |
|----------------|---|---------------------|--------------|
| REVISION DATE  | PROJECT I.D.<br>5991-0-13   | SHEET NUMBER<br>4.1 | TOTAL SHEETS |
|                | FEDERAL PROJECT DESIGNATION<br>EHST-4140 (6)  |                     |              |
|                | PLAT OF RIGHT OF WAY REQUIRED<br>LOSEY BLVD. - CITY OF LA CROSSE<br>TOPICS<br>STATE ROAD INTERSECTION<br>LA CROSSE COUNTY |                     |              |
|                | SCALE<br>0 50 100 Ft.   | DATE<br>8-2-73      |              |
| Const. Project |   |                     |              |

BEGIN RELOCATION ORDER  
 PROJ. 5991-0-13/EHST-4140 (6)  
 STATION 90+00

END RELOCATION ORDER  
 PROJ. 5991-0-13/EHST-4140 (6) STATION 102+25

PROPERTY BOUNDARIES  
 NOT TO SCALE

NOTE:  
 BEARINGS SHOWN ON THIS PLAT ARE THE TRUE BEARINGS OF EACH  
 TANGENT TO THE NEAREST MINUTE.

| SCHEDULE OF LANDS AND INTERESTS REQUIRED |              |                             |                    |       |              |
|--|--------------|-----------------------------|--------------------|-------|--------------|
| PARCEL NUMBER                            | SHEET NUMBER | OWNER                       | INTERESTS REQUIRED | ACRES | PROJECT I.D. |
| 1  | 4.1          | EARL STROEH, ET. AL.        | FEE SIMPLE         | 0.07  | 5991-0-13    |
| 2  | "            | ARNOLD A. SALTZMAN, ET. AL. | "                  | 0.16  | "            |
| 3  | "            | GERRARD REALTY CORP.        | "                  | 0.03  | "            |
| 4  | "            | GARDENS, INC.               | "                  | 0.03  | "            |
| 4S                                       | "            | FRANK WHITE ADV. CO.        | ADVERTISING RIGHTS | —     | "            |
| 5  | "            | NORTHERN STATES POWER CO.   | RELEASE OF RIGHTS  | —     | "            |
| 6  | "            | LA CROSSE TELEPHONE CORP.   | "                  | —     | "            |