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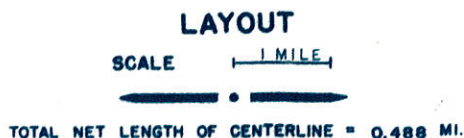
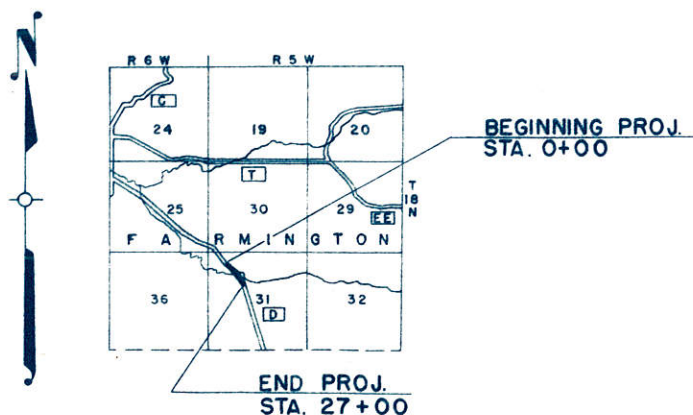
STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

PLAN AND PROFILE OF PROPOSED

S.T.H. 162 - MINDORO (YOUNG BRIDGE)
LA CROSSE COUNTY
PROJECT S 0121(3)

BEGINNING AT A POINT APPROX. 1200 FT. NORTH & 680 FEET WEST OF THE
S.W. CORNER N.E. 1/4 - N.W. 1/4 SEC. 31, T.18 NORTH R.5W THENCE SOUTHEASTERLY
TO A POINT APPROX. 930 FT. S & 780 FT. EAST OF SAID COR. SEC. TOWNSHIP & RANGE

SCALES { PLAN 1 IN. = 100 FT.
PROFILE HOR. 1 IN. = 100 FT. VERT. 1 IN. = 10 FT.
CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
32.6	121.0		11.3	WIS. 5N	1	16

CONVENTIONAL SIGNS

- | | |
|---|--|
| <ul style="list-style-type: none"> STATE LINE COUNTY LINE TOWNSHIP OR RANGE LINE SECTION LINE NEW RIGHT OF WAY LINE PRESENT RIGHT OF WAY LINE WIRE FENCE { WOVEN
BARBED LOT LINE CORPORATE OR CITY LIMITS PROPERTY LINE TRAVELED WAY OR P.E. RAILROADS BASE OR SURVEY LINE | <ul style="list-style-type: none"> CULVERTS IN PLACE CULVERTS REQUIRED DROP INLET POWER POLE TELEPHONE OR TELEGRAPH POLE RIGHT OF WAY MARKERS REFERENCE STAKE FOR HUBS ONLY MARSH HEDGE TREES GROUND ELEVATION GRADE ELEVATION |
|---|--|

STATE HIGHWAY COMMISSION OF WISCONSIN MADISON, WIS.		DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
SURVEYOR <u>W.L.S.</u> NOTE BOOK <u>8747</u>			
DIVISION COMPUTER <u>J.W.J.</u> M. O. CHECKER			
DIVISION CHECKER <u>R.C.J.</u> CORRECT			
CORRECT:			
DATE <u>2/18/51</u>	<u>T. M. Reynolds</u>	RECOMMENDED FOR APPROVAL:	
	DIVISION ENGINEER	DISTRICT ENGINEER	DATE
RECOMMENDED FOR APPROVAL:			
DATE <u>8/14/51</u>	<u>C. C. Blakes</u>		
	CONSTRUCTION ENGINEER	APPROVED	
APPROVED:			
DATE <u>8/14/51</u>	<u>F. C. Rustina</u>		
	STATE HIGHWAY ENGINEER	DIVISION ENGINEER	DATE

S0121(3)

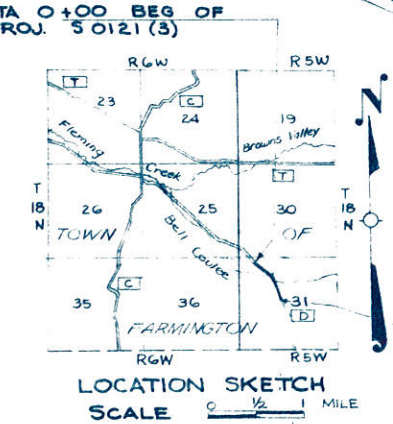
STA. 0+00 BEG. OF PROJECT SO121(3)

ELDOR HORSTMAN

GEORGE F. ROMMEL
 R/W REQD. 0.28 AC.

ALFRED & EDWARD F. YOUNG
 R/W REQD. 1.43 AC.

STA 27+00 END OF PROJECT SO121(3)



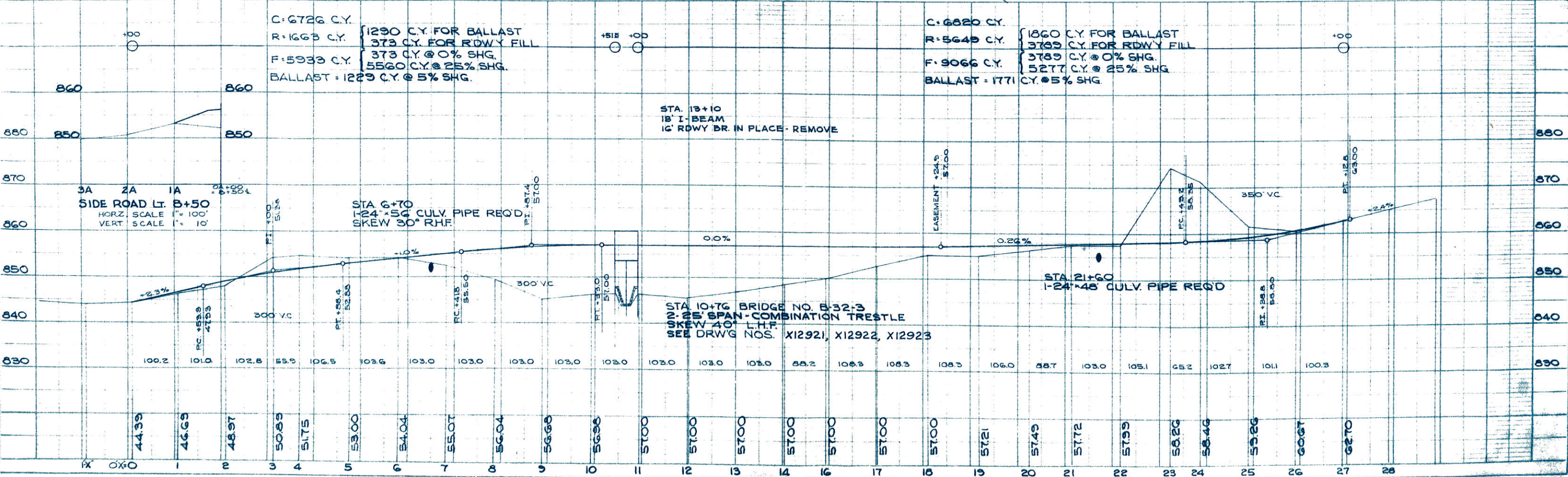
ELDOR HORSTMAN
 R/W REQD. = 2.42 AC.

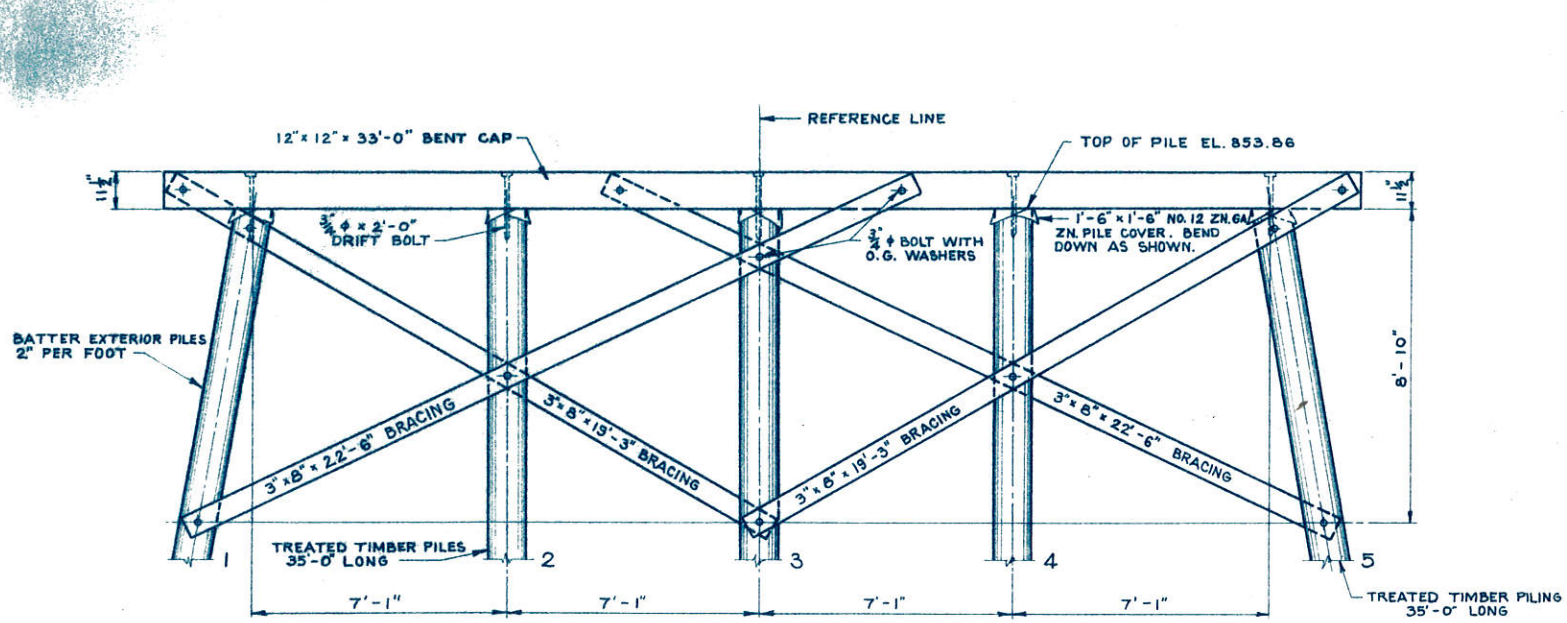
STA 27+00 END OF PROJ. SO121(3)

SW CORNER
 NE 1/4 - NW 1/4
 SEC. 31, T18N, R5W

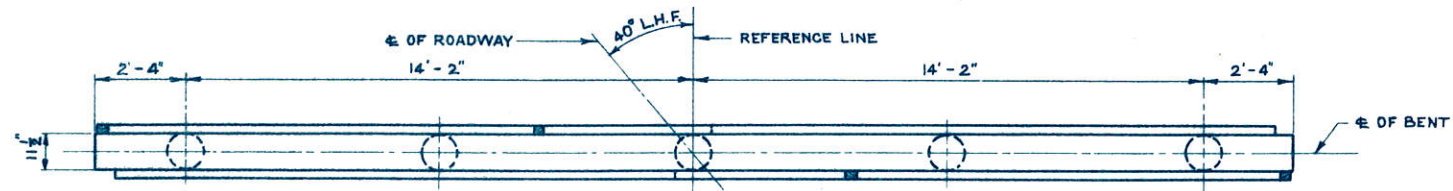
NET LENGTH CENTERLINE STA. 0+00 - 27+00 = 2579.2 LIN. FT.

- BM #1 SPIKE IN PP 1' RT. 3+26 853.49
- BM #2 SPIKE IN PP 2' LT. 12+13 849.39
- BM #4 SPIKE IN TREE 12' LT. 12+13 851.11
- BM #3 SPIKE IN TP 16' LT. 26+81 863.41

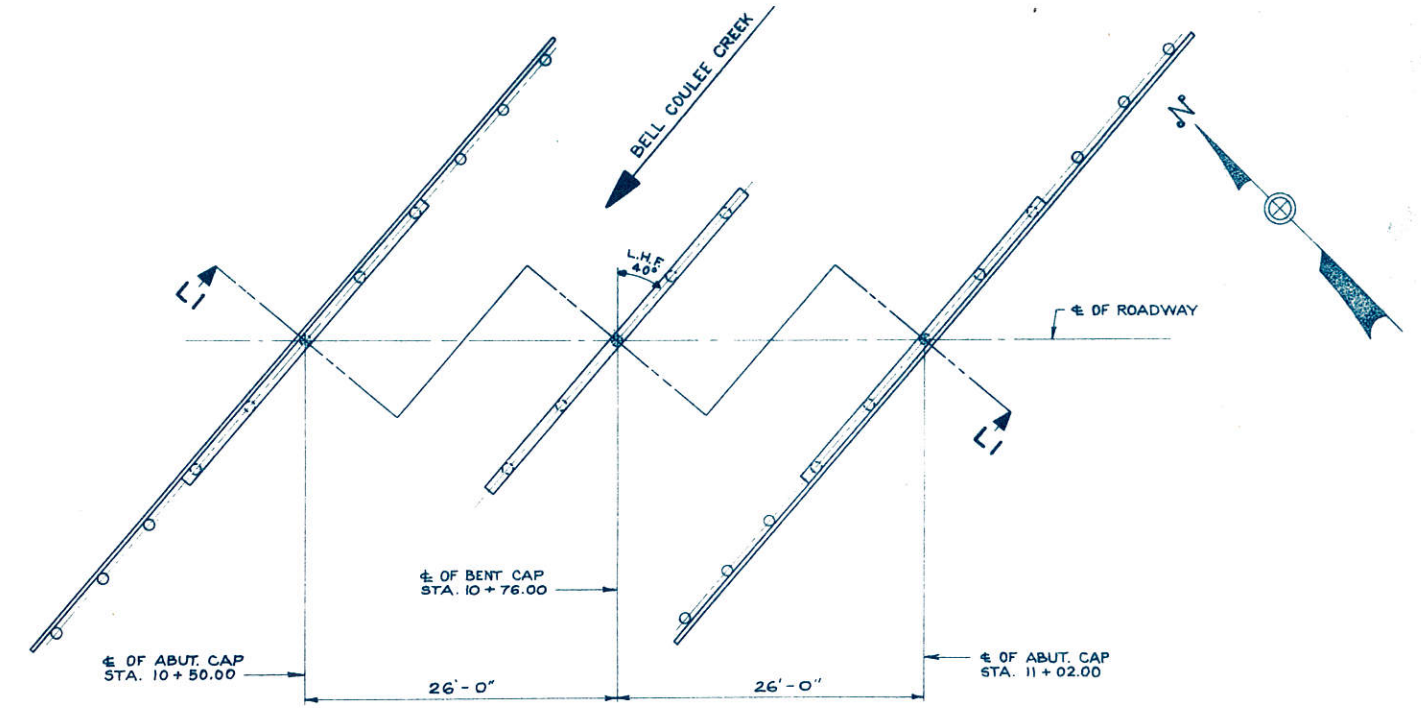




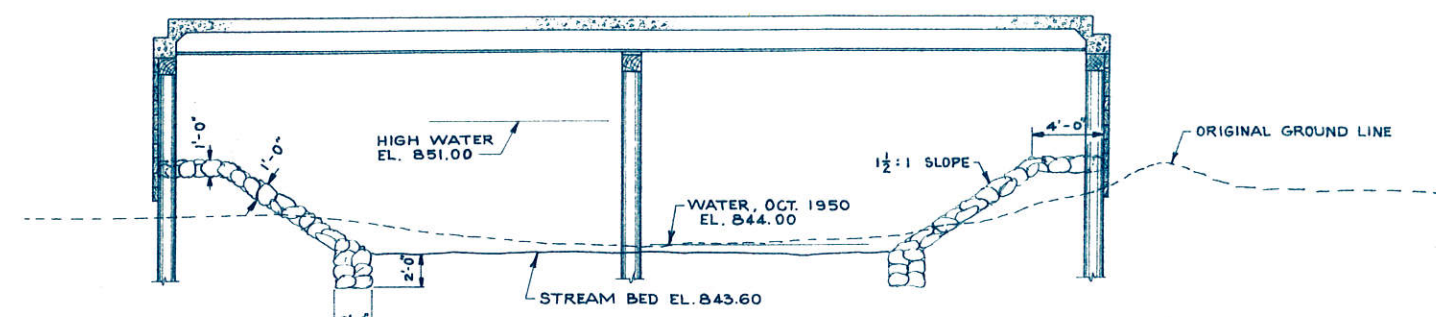
ELEVATION



PLAN



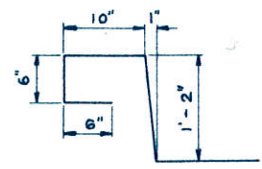
LAYOUT



SECTION LI

BILL OF BARS, SUPERSTRUCTURE

POUR	NO.	MARK	SIZE NO.	LENGTH	SPACING	LOCATION	DET.
	4	S1	8	26-6	SHOWN	LONG. CURBS (DO NOT LAP)	
	16	S2	4	27-0	SHOWN	LONG. CURBS & FLOOR	
	72	S3	4	4-0	1-6	TRANS. " " "	A
	24	S4	6	6-9	SHOWN	TRANS. PAVING SUPPORT	
	24	S5	4	10-0	1-0	LONG. FLOOR, PLACE SYM. ABOUT C OF STRUCTURE	
	32	S6	4	26-0	SHOWN	LONG. FLOOR	
	50	S7	6	26-0	1-0	" " "	
	218	S8	4	25-0	3 1/2	TRANS. FLOOR	
	12	S9	4	24-0	3 1/2	" " "	
	12	S10	4	22-0	3 1/2	" " "	
	12	S11	4	21-0	3 1/2	" " "	
	12	S12	4	20-0	3 1/2	" " "	
	12	S13	4	18-9	3 1/2	" " "	
	12	S14	4	17-9	3 1/2	" " "	
	12	S15	4	16-9	3 1/2	" " "	
	12	S16	4	15-9	3 1/2	" " "	
	12	S17	4	14-9	3 1/2	" " "	
	12	S18	4	13-6	3 1/2	" " "	
	12	S19	4	12-6	3 1/2	" " "	
	12	S20	4	11-6	3 1/2	" " "	
	12	S21	4	10-6	3 1/2	" " "	
	12	S22	4	9-6	3 1/2	" " "	
	12	S23	4	8-6	3 1/2	" " "	
	12	S24	4	7-6	3 1/2	" " "	
	12	S25	4	6-6	3 1/2	" " "	
	12	S26	4	5-3	3 1/2	" " "	
	12	S27	4	4-3	3 1/2	" " "	
	12	S28	4	3-3	3 1/2	" " "	
	12	S29	4	2-3	3 1/2	" " "	
	24	S30	4	1-6	3 1/2	" " "	



DETAIL A

ABOVE DIM. ARE OUT TO OUT

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UNIT	SUPER.	ABUT.	BENT	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.	—	25	—	25
SAND-GRAVEL FILL	C.Y.	—	25	—	25
CONCRETE MASONRY	C.Y.	38.1	—	—	38.1
BAR STEEL REINFORCEMENT	LB.	9470	—	—	9470
STRUCTURAL CARBON STEEL	LB.	18,600	—	—	18,600
SHEET ZINC	LB.	—	23	12	35
TREATED LUMBER & TIMBER	M.B.M.	—	4.68	0.56	5.24
UNTR. TIMBER TEST PILING (2 @ 40'-0")	L.S.	—	—	—	1
TREATED TIMBER PILING DELIVERED	L.F.	—	770	175	945
TREATED TIMBER PILING DRIVEN	L.F.	—	595	125	720
PILE SHOES	EACH	—	22	5	27
FLOOR DRAINS	EACH	2	—	—	2
RIPRAP	C.Y.	—	100	—	100

* DRIVE ONE AT LOCATION OF EACH ABUTMENT

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.
 ALL CONCRETE MASONRY SHALL BE GRADE 'AA'.
 BEVEL EXPOSED EDGES 1" UNLESS OTHERWISE SPECIFIED.
 BAR STEEL REINFORCEMENT IN FLOOR SHALL BE CUT OFF AT FLOOR DRAINS.
 IMBED BAR STEEL REINFORCEMENT 2" CLEAR UNLESS OTHERWISE SHOWN.
 IMBEDMENT DIMENSIONS APPLY TO FACE OF BAR UNLESS OTHERWISE SHOWN.
 FIELD CONNECTIONS OF RAILING POSTS TO CHANNEL BRACKETS SHALL BE 3/4" RIVETS OR TURNED BOLTS. OTHER FIELD CONNECTIONS SHALL BE MADE WITH 3/4" RIVETS OR RIBBED TIGHT-FIT BOLTS IN FULL SIZE PUNCHED HOLES.
 THE 3" x 10" BULKHEAD PLANK IN THE BODY OF THE ABUTMENTS MAY BE SPLICED. SPLICES MUST BE AT C OF PILES AND MUST BE STAGGERED IN ADJACENT PLANKS.
 PILES SHALL BE TREATED TIMBER PILING 35'-0" LONG. PILES FOR THE PIER SHALL BE DRIVEN TO A MINIMUM BEARING VALUE OF 20 TONS PER PILE. PILES FOR THE ABUTMENTS SHALL BE DRIVEN TO A MINIMUM BEARING VALUE OF 15 TONS PER PILE.
 THE FRONT FACES OF THE ABUTMENTS SHALL BE RIPRAPPED AS SHOWN IN "SECTION THROUGH ABUTMENT" AND "SECTION LI". AFTER THE FILL IS IN PLACE, THE SLOPE OF THE FILL AROUND THE ENDS OF THE WINGS SHALL BE RIPRAPPED TO A MINIMUM THICKNESS OF 1'-0" AS DIRECTED BY THE ENGINEER.
 ALL END RAILING POST BRACKET CHANNELS SHALL BE PLACED ON THAT SIDE OF POST TOWARD C OF SPAN IN ORDER TO FACILITATE THE DRILLING OF FIELD HOLES IN POST.

LIST OF DRAWINGS

1-BENT & LAYOUT	X 12921
2-ABUTMENTS	X 12922
3-SUPERSTRUCTURE	X 12923

REVISED	STATE HIGHWAY COMMISSION OF WISCONSIN		
	BENT & LAYOUT		
CO. LA CROSSE	TOWN FARMINGTON	STA. 10 + 76.00	
SECTION 31	TOWN 18 N.	RANGE 5 W.	
DATE 6-11-51	DRWN. G. J. W. P.	CHK. L. W. P.	SPEC. 1951
CORRECT	G. J. W. P. BRIDGE ENGINEER		
APPROVED	STATE HIGHWAY ENGINEER		
STRUCTURE B-32-3		SHEET 1 OF 3	