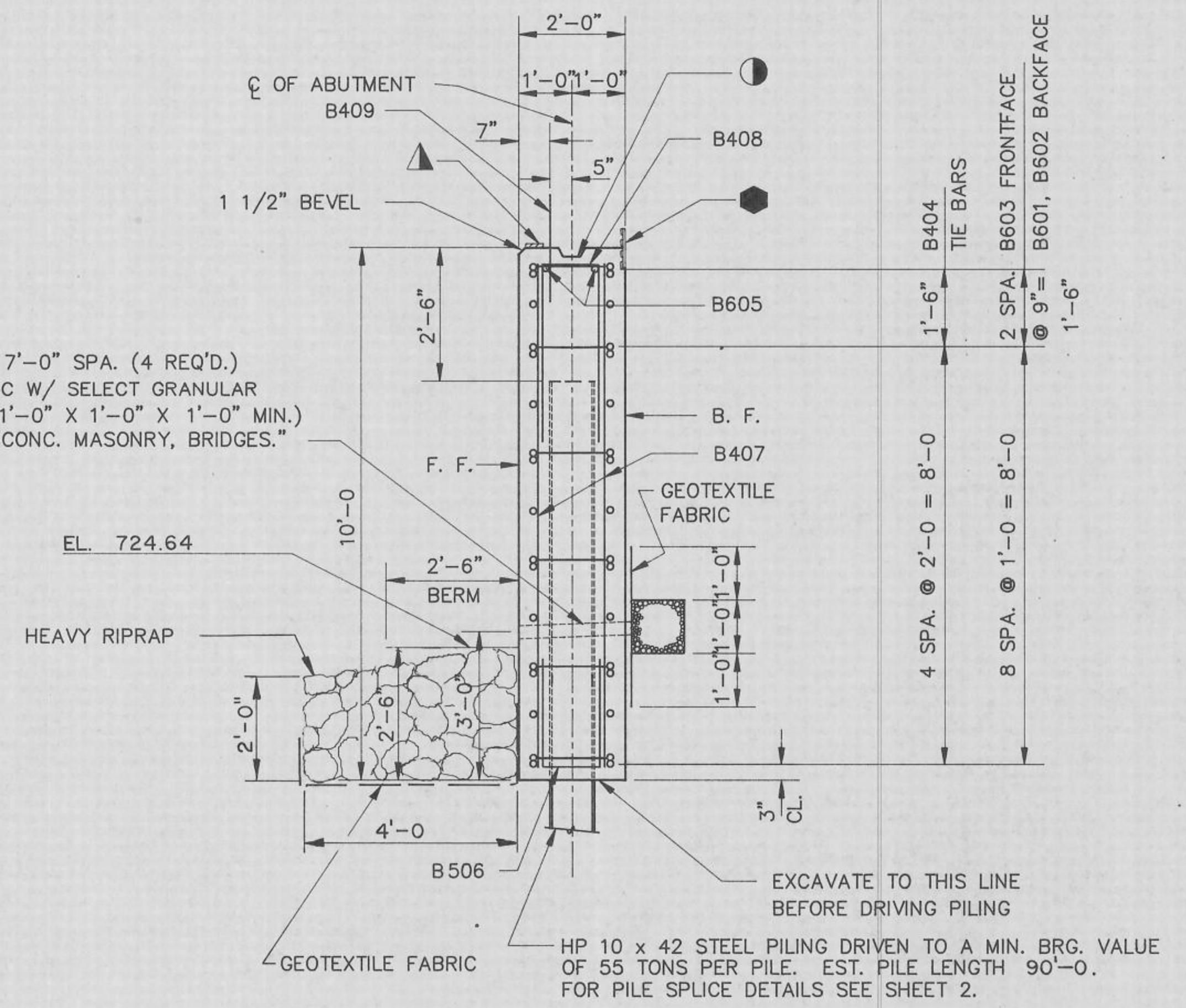
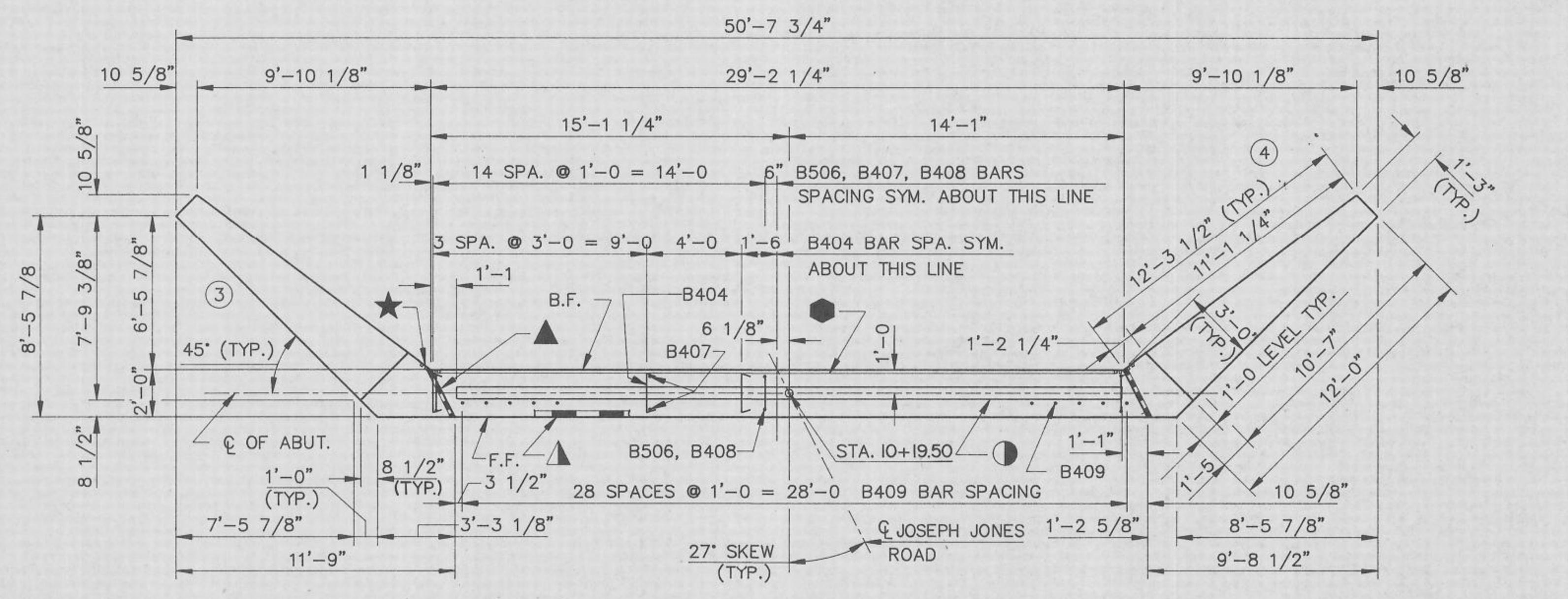
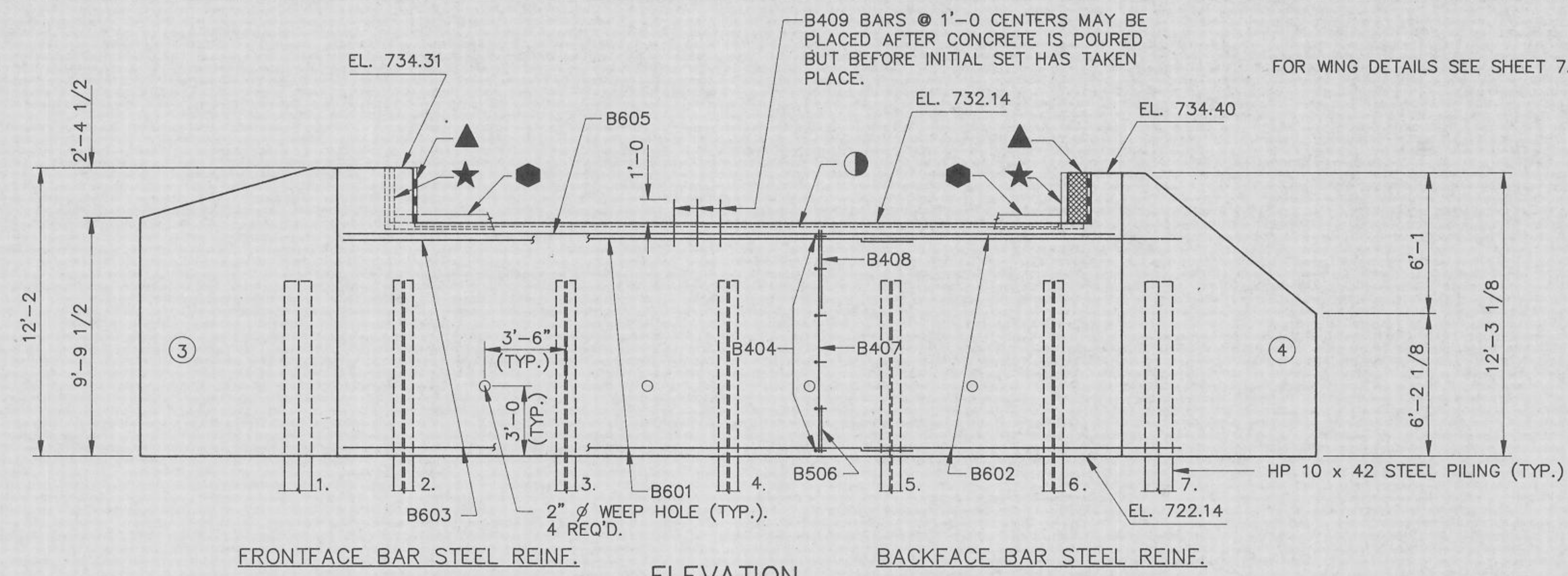
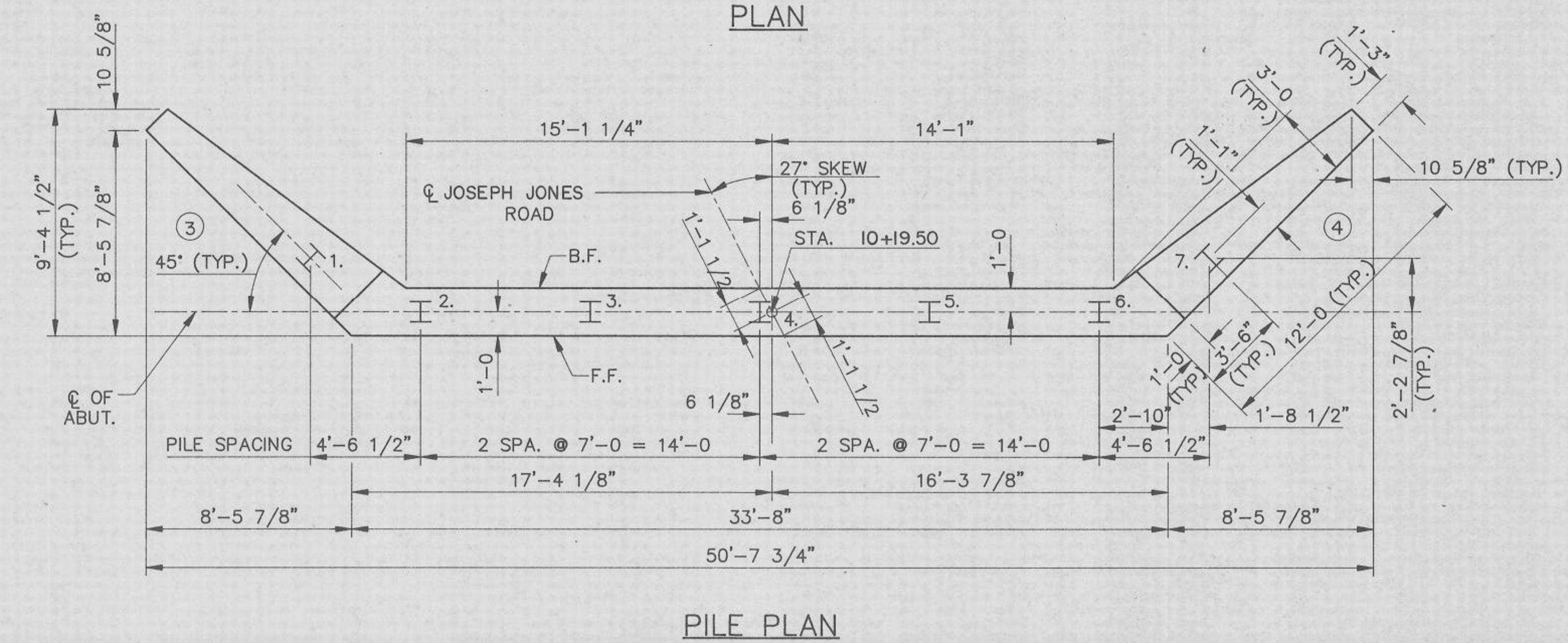


LEGEND

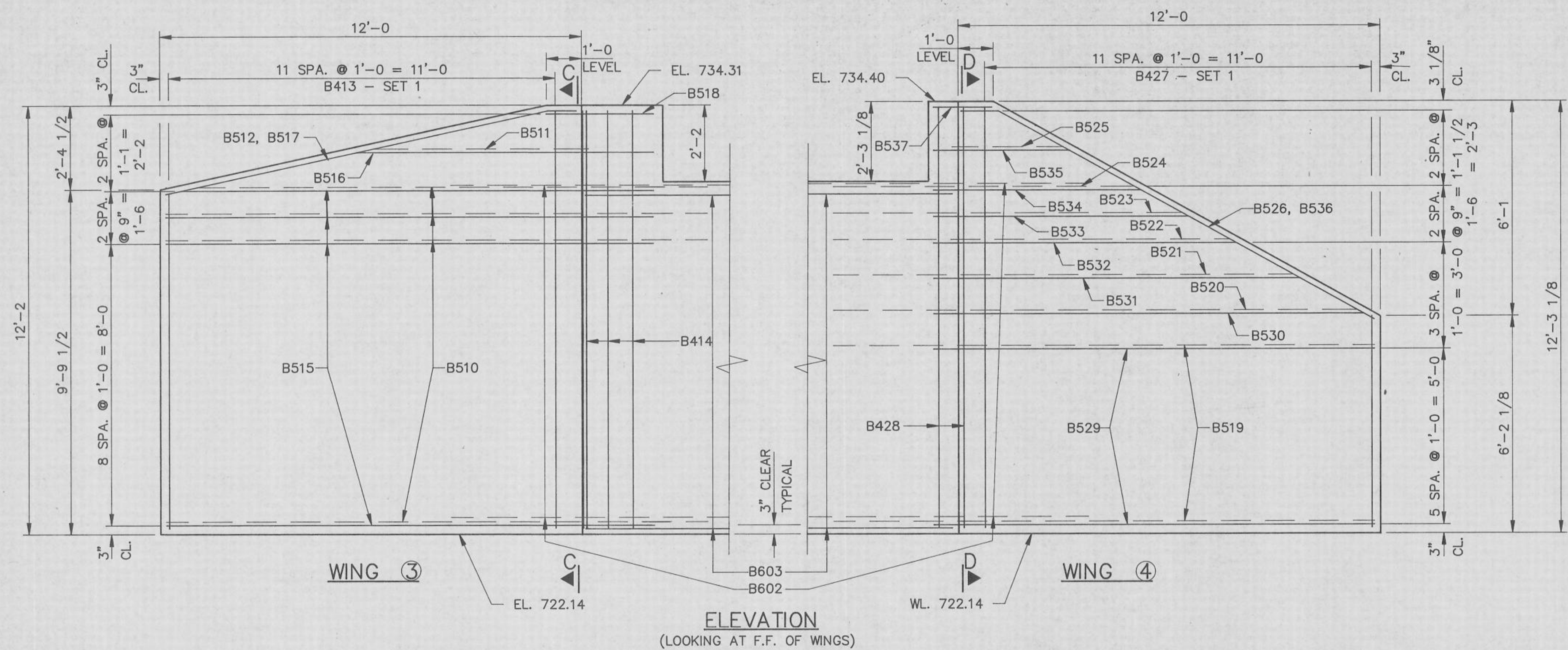
- — KEYED CONSTRUCTION JOINT FORMED BY SURFACED & BEVELED 2 X 6.
 - ★ — VERTICAL P.C.W. EXTEND FROM 5" BELOW BRIDGE SEAT TO TOP OF WING. BUTT-SPlice AT ALL INTERSECTIONS WITH HORIZ. P.C.W. BY USING A HEATED SPLICING IRON. HOLD FLUSH WITH FACE OF CONCRETE. SEAL VERTICAL ENDS OF P.C.W. WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER.
 - — HORIZONTAL P.C.W., EXTEND BETWEEN VERT. P.C.W. AT WINGS. HOLD FLUSH WITH FACE OF CONCRETE.
 - ▲ — 1/2" FILLER EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONCRETE).
 - ▲ — 4" X 3/4" FILLER, EXTEND FULL LENGTH OF ABUTMENT BETWEEN EDGES OF SLAB.
- FOR P.C.W. DETAILS SEE SHEET 2



NOTE: CONTRACTOR SHALL FIELD DRILL EIGHT (8) 1" Ø HOLES IN THE WEB OF PILES #2 AND #6 FOR PLACEMENT OF BACKFACE WING REINFORCEMENT. WING ③ B510, WING ④ B519 THRU B521.

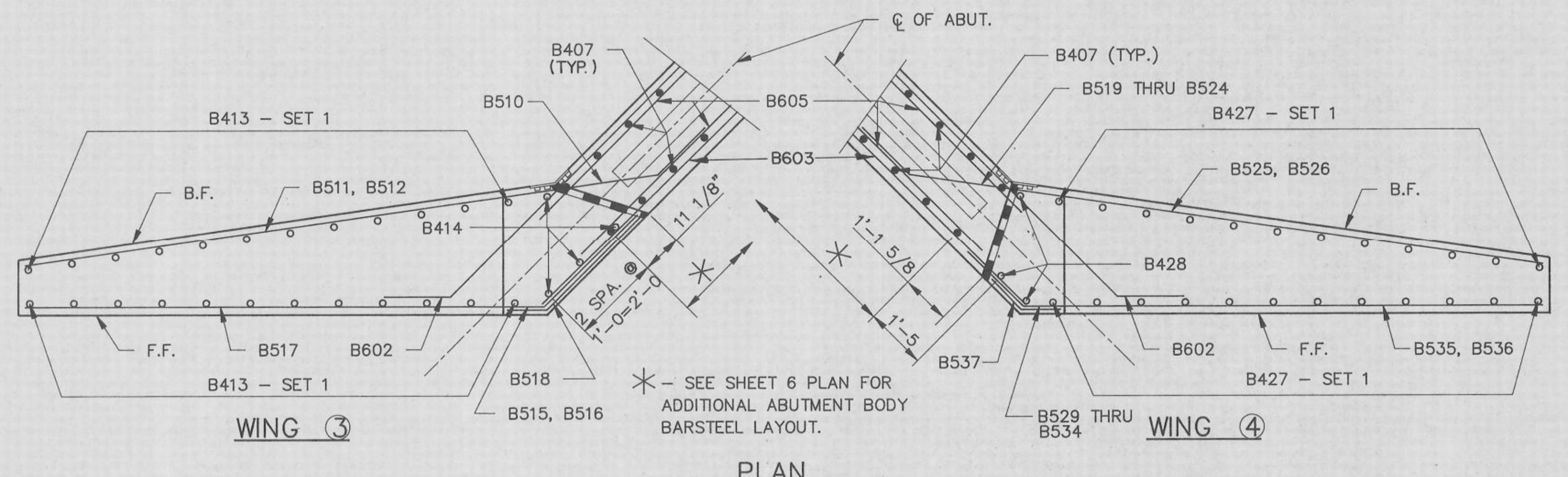


No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI "81"	Drawn By	RLR
		Plans Checked	MSR
EAST ABUTMENT			SHEET 6 OF 9
X80108			

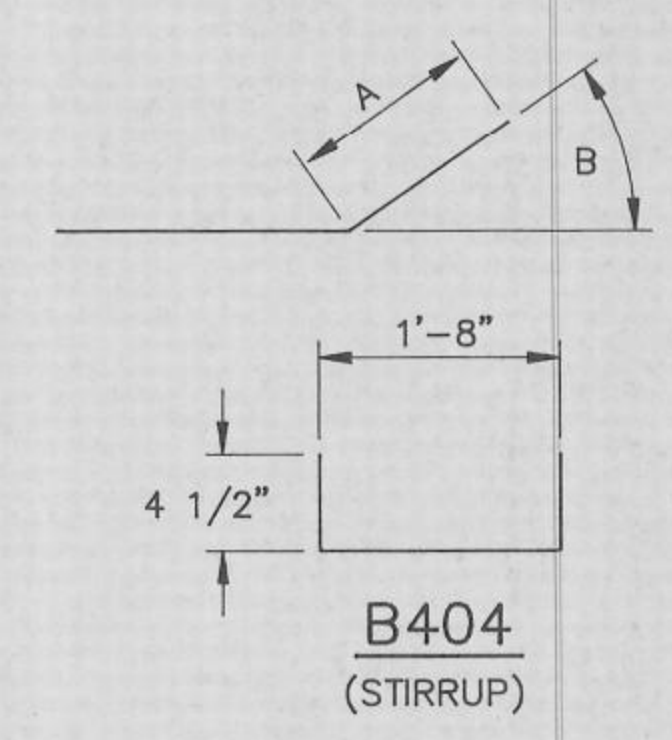
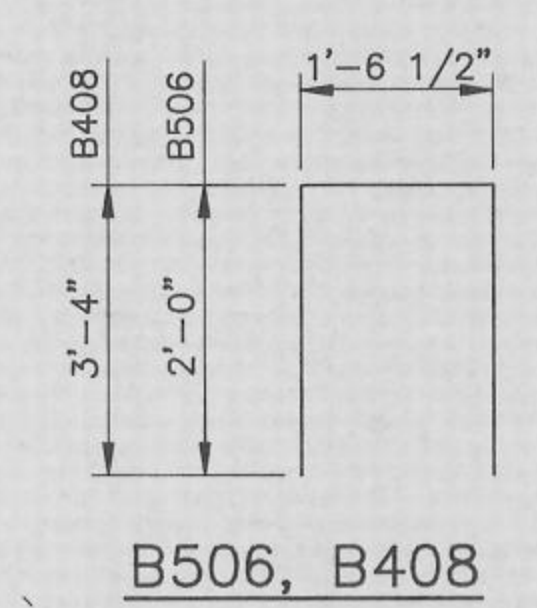


BILL OF BARS

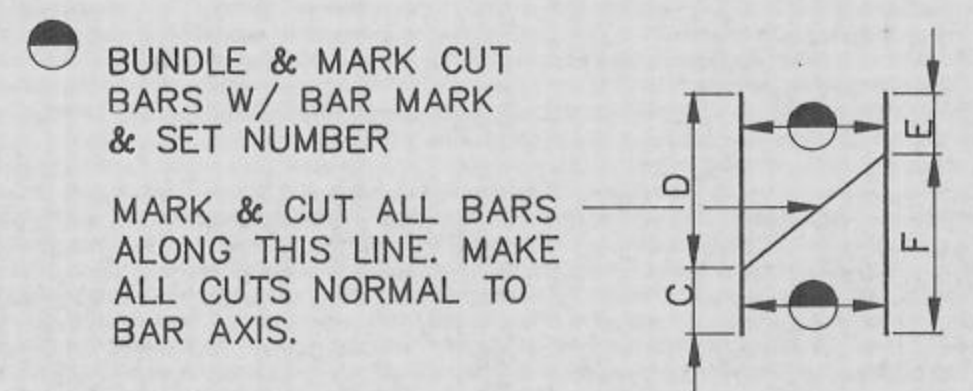
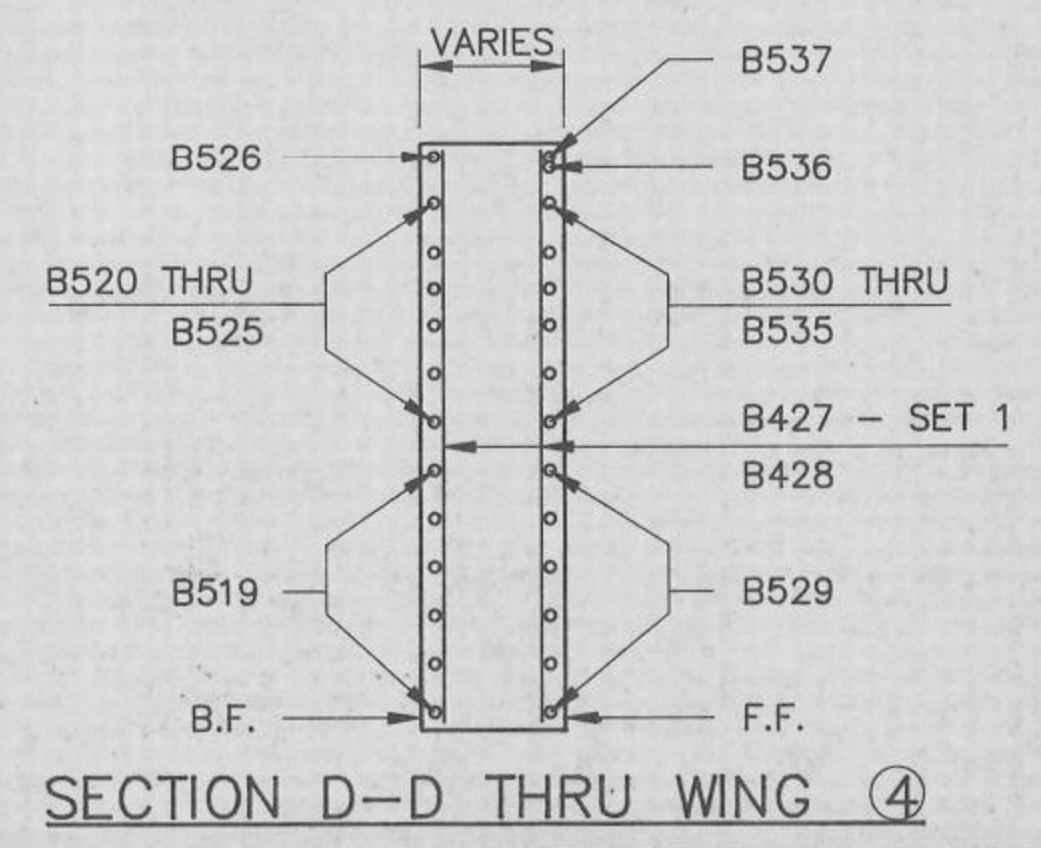
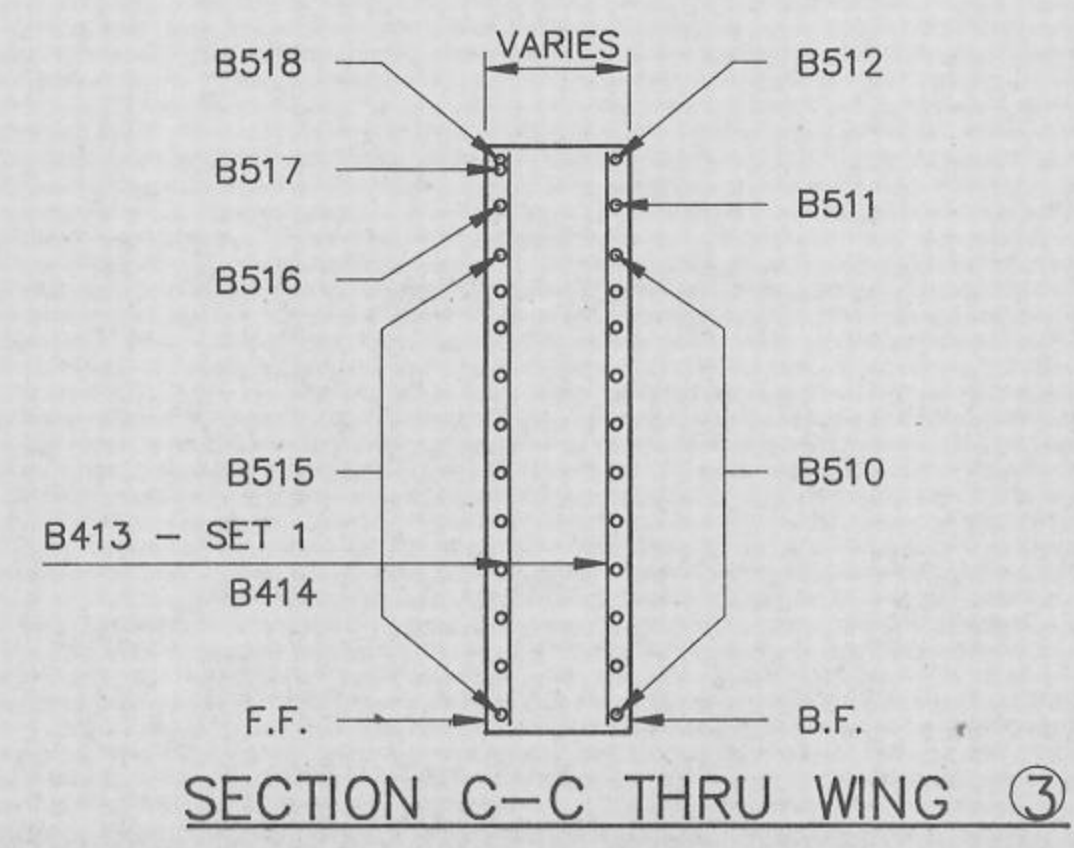
MARK	NO. REQ'D	LENGTH	BENT	CUT. DIAGR.	LOCATION
B601	11	17'-0"			BODY -- B.F. -- HORIZ. CENTER
B602	22	14'-3"	X		" & WING -- " -- "
B603	11	33'-6"			" -- F.F. -- "
B404	60	2'-3"	X		" -- TIES -- "
B605	2	30'-0"			" -- TOP -- "
B506	30	5'-4"	X		" -- BOTTOM -- VERT.
B407	60	9'-8"			" F.F. & B.F. -- "
B408	30	8'-1"	X		" -- TIES TOP -- "
B409	29	2'-0"			" -- DOWELS -- "
B510	11	16'-3"	X		WING (3) -- B.F. -- HORIZ.
B511	1	6'-8"			" -- " -- " -- "
B512	1	12'-1"	X		" -- " -- " -- TOP
B413	12	21'-4"		X	" -- F.F. & " -- VERT.
B414	4	11'-10"			" -- " -- " -- "
B515	11	14'-6"	X		" -- " -- " -- HORIZ.
B516	1	9'-1"	X		" -- " -- " -- "
B517	1	12'-0"	X		" -- " -- " -- TOP
B518	1	3'-10"	X		" -- " -- " -- "
B519	6	16'-3"	X		" (4) -- B.F. -- "
B520	1	16'-0"	X		" -- " -- " -- "
B521	1	14'-2"	X		" -- " -- " -- "
B522	1	12'-4"	X		" -- " -- " -- "
B523	1	11'-0"	X		" -- " -- " -- "
B524	1	9'-8"	X		" -- " -- " -- "
B525	1	3'-4"			" -- " -- " -- "
B526	1	13'-5"	X		" -- " -- " -- TOP
B427	12	17'-11"		X	" -- F.F. & " -- VERT.
B428	3	11'-11"			" -- " -- " -- "
B529	6	13'-4"	X		" -- " -- " -- HORIZ.
B530	1	13'-0"	X		" -- " -- " -- "
B531	1	11'-2"	X		" -- " -- " -- "
B532	1	9'-4"	X		" -- " -- " -- "
B533	1	8'-0"	X		" -- " -- " -- "
B534	1	6'-2"	X		" -- " -- " -- "
B535	1	4'-1"	X		" -- " -- " -- "
B536	1	13'-2"	X		" -- " -- " -- TOP
B537	1	2'-0"	X		" -- " -- " -- "



DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.



MARK	A	B
B602 THRU B534	1'-6"	45°
B510 THRU B524	1'-6"	37°
B512	1'-4"	12°
B515	2'-10"	45°
B517	1'-3"	12°
B518	1'-0"	45°
B535	1'-1"	29°
B537	1'-0"	29°

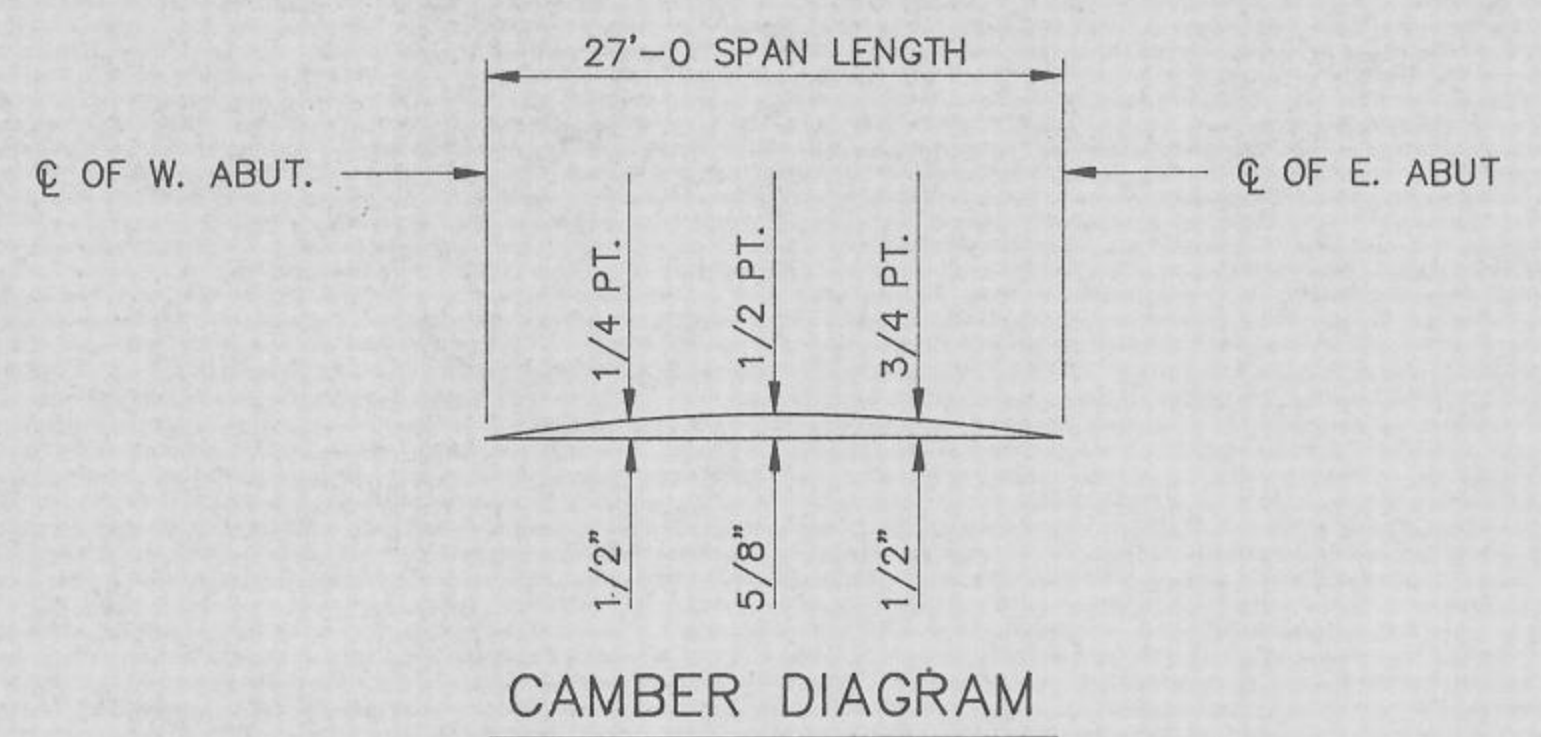
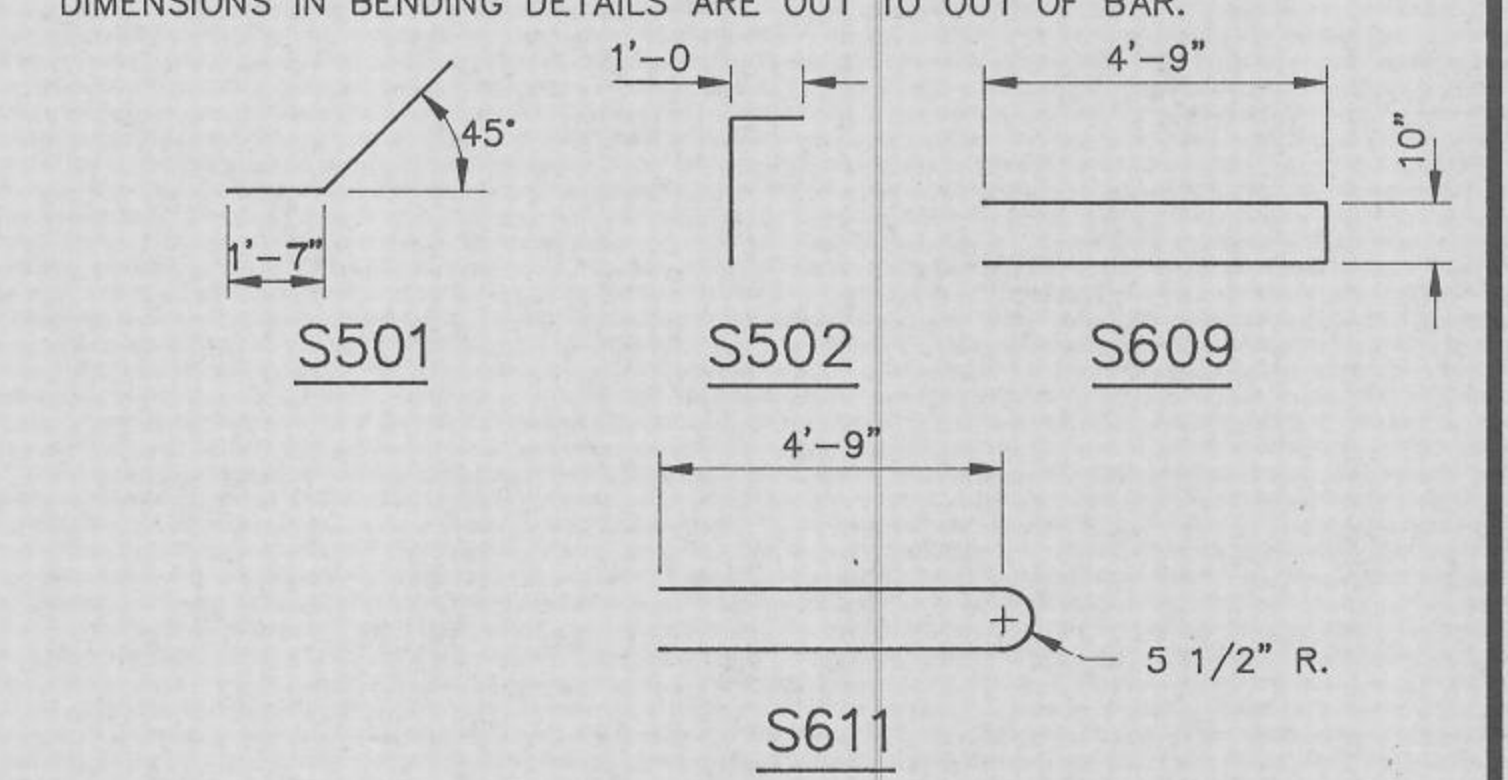


MARK	C	D	E	F	NO. OF BARS/SET	SETS REQ'D
B413 SET 1	9'-6"	11'-10"	9'-6"	11'-10"	12	2
B427 SET 1	6'-0"	11'-11"	6'-0"	11'-11"	12	2

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE		B-32-127	
Const. Spec.	WI "81"	Drawn By	RLR
		Plans Checked	msk
EAST ABUTMENT DETAILS			SHEET 7 OF 9
			X80109

BILL OF BARS COATED 1170 LBS.
UNCOATED 4870 LBS.

MARK	NO. REQ'D.	UN-COATED	LENGTH	BENT	LOCATION
S501	52	52	4'-0"	X	DIAPHRAGM @ ABUTS. - LONGIT.
S502	52	52	2'-10"	X	" " " " - VERT.
S803	28	28	28'-10"		SLAB BOTTOM - LONGIT.
S804	27	27	22'-3"		" " " " - LONGIT.
S505	26	26	28'-9"		" " " " - TRANS.
S406	6	6	28'-9"		@ ABUT. - " "
S407	21	21	28'-9"		TOP " "
S408	18	18	28'-10"		" " " " - LONGIT.
S609	8	8	10'-0"	X	@ RAIL POST, ONE PER POST
S610	20	20	4'-0"		" " " " , TWO " "
S611	2	2	10'-0"	X	" " " " CORNER POSTS AS NOTED



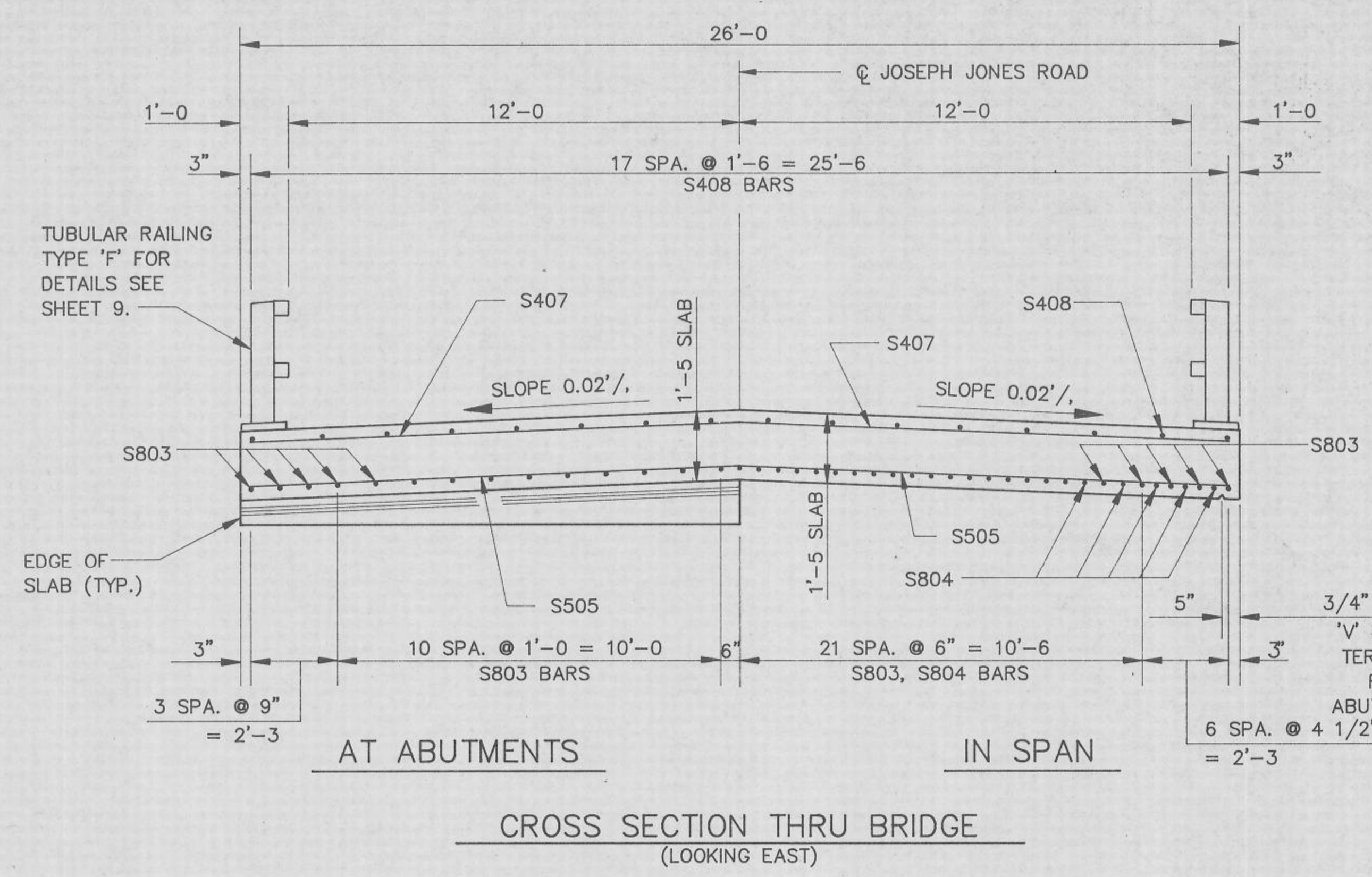
CAMBER SPANS AS SHOWN TO PROVIDE FOR DEADLOAD DEFLECTION AND FUTURE PLASTIC FLOW. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. DEADLOAD DEFLECTION ONLY APPROXIMATELY 1/4 OF CAMBER VALUES SHOWN.

GENERAL NOTES

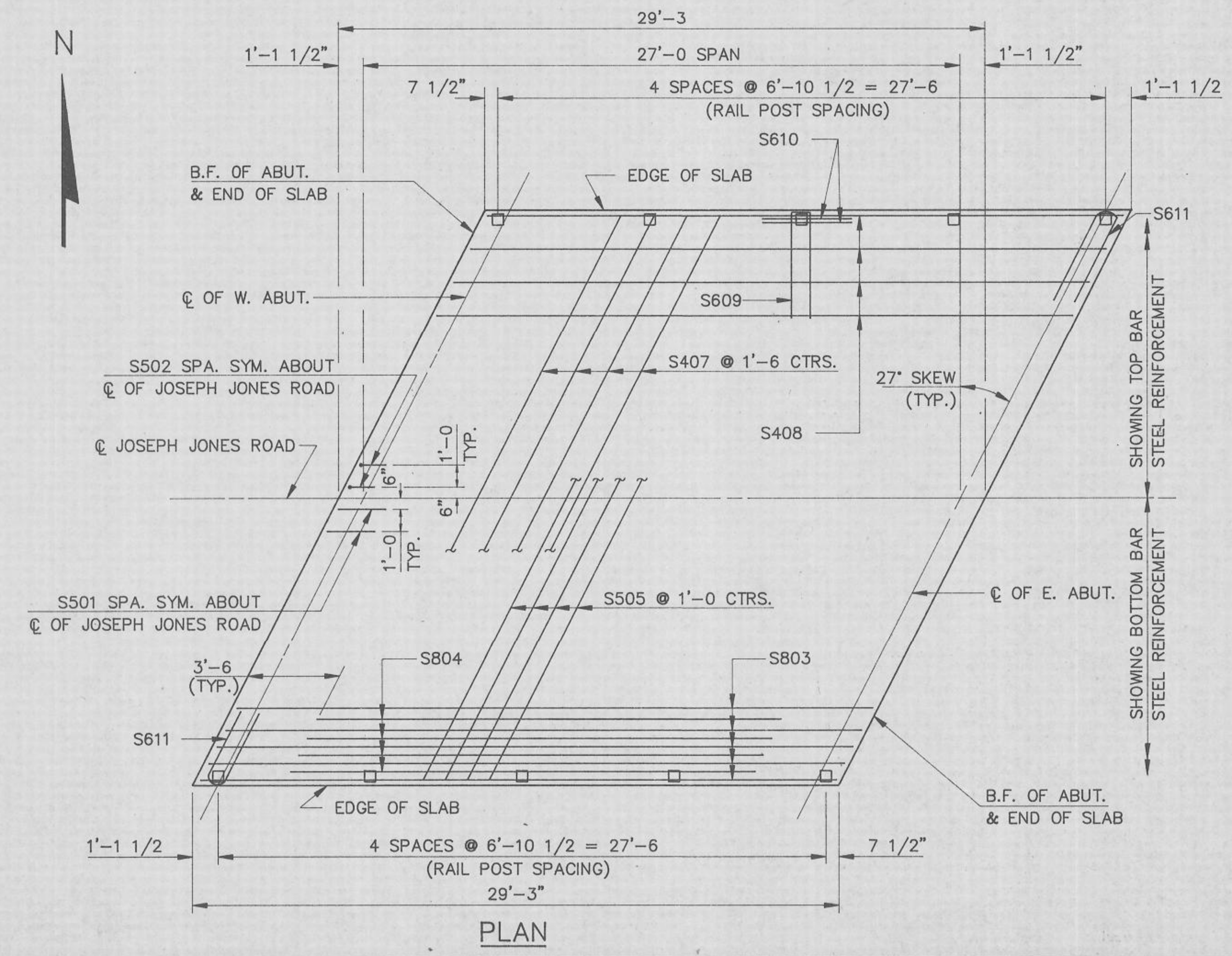
ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

ALTERNATE TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

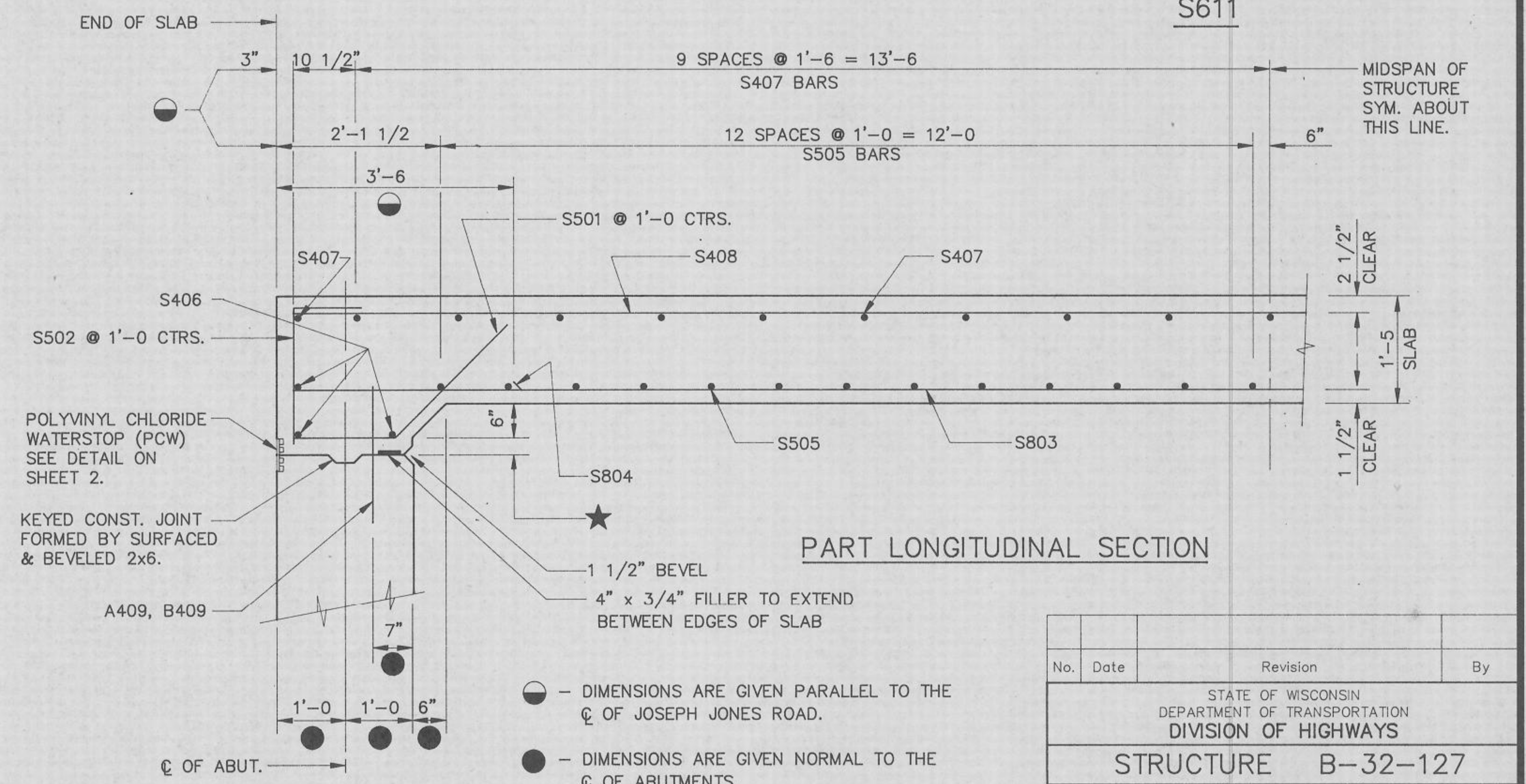
ALL TRANSVERSE BAR STEEL REINFORCEMENT SHALL BE PLACED ON THE SKEW.



CROSS SECTION THRU BRIDGE (LOOKING EAST)



PLAN



PART LONGITUDINAL SECTION

POLYVINYL CHLORIDE WATERSTOP (PCW) SEE DETAIL ON SHEET 2.

KEYED CONST. JOINT FORMED BY SURFACED & BEVELED 2x6.

○ - DIMENSIONS ARE GIVEN PARALLEL TO THE C/L OF JOSEPH JONES ROAD.

● - DIMENSIONS ARE GIVEN NORMAL TO THE C/L OF ABUTMENTS.

★ - VARIES, 7 1/4" @ C/L ROAD @ W. ABUT., 6 5/8" @ C/L ROAD @ E. ABUT., 5 1/8" @ WING (1), 4 1/8" @ WING (4), 3" @ WINGS (2) & (3).

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI "81"	Drawn By	RLR
		Plans Checked	MSR
SUPERSTRUCTURE			SHEET 8 OF 9
			X80110

LEGEND

- ① W 6 X 25 WITH 1/4" DIA. HOLES ON EACH SIDE OF POST FLANGE FOR STUD (6). CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
- ② PLATE 1" X 9 1/2" X 10" WITH 1/16" X 1/2" SLOTTED HOLES FOR ANCHOR BOLTS (3). WELD TO (1) AS SHOWN.
- ③ A449 OR MATERIAL OF EQUIVALENT YIELD STRENGTH AND ELONGATION ANCHOR BOLT 7/8" DIA. X 1'-3" LONG AT END POSTS AND 10" LONG AT ALL OTHER POST LOCATIONS. (MIN. YIELD OF 92 K.S.I. AND ELONGATION OF 14%) WITH A325 NUT AND WASHER. 4 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE (2).
- ④ BAR 3/4" SQ. X 8" LONG. WELD TO ANCHOR BOLTS (3).
- ⑤ TS 4 X 4 X .25 STRUCTURAL TUBING, CONFORMING TO A.S.T.M. DESIGNATION A36. ATTACH TO (1) WITH STUDS (6).
- ⑥ 5/8" DIA. X 1 1/2" LG. SHOP WELDED STUDS, WITH HEX. NUT AND 2 WASHERS. 4 PER POST REQ'D. (2 REQ'D. AT EACH LOCATION).
- ⑦ SQUARE SLEEVE FABRICATED FROM 1/4" PLATE. PROVIDE 'SLIDING FIT' WITH A MINIMUM OUT TO OUT DIMENSION OF 3 13/32".
- ⑧ TS 3 X 3 X .25 X 1'-10" LONG. PROVIDE 1/2" DIA. SURFACE WELDS ON ALL SIDES AS SHOWN. GRIND WELDS TO FIT FREE INTO I.D. OF (5). PROVIDE 3/8" DIA. X 1/2" WELDING STUDS ON TOP AND BOTTOM SURFACES AT CENTERLINE.
- ⑨ PLATE 3/4" X 1'-0" X 1'-6". WELD TO END RAIL POST AS SHOWN IN DETAIL. REQUIRED AT BEAM GUARD ATTACHMENT ONLY.
- ⑩ 1" DIA. HOLES IN PLATE (9) FOR 7/8" DIA. A325 BOLTS W/ HEX NUTS AND WASHERS.

GENERAL NOTES

BID ITEM SHALL BE "TUBULAR RAILING TYPE 'F' " WHICH INCLUDES ALL ITEMS SHOWN.

RAILING SHALL BE FABRICATED IN 2 OR 3 PANEL LENGTHS.

POSTS BASE PLATES (2) SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.

ALL MEMBERS INCLUDING UPPER 4" OF (3) SHALL BE GALVANIZED AFTER FABRICATION.

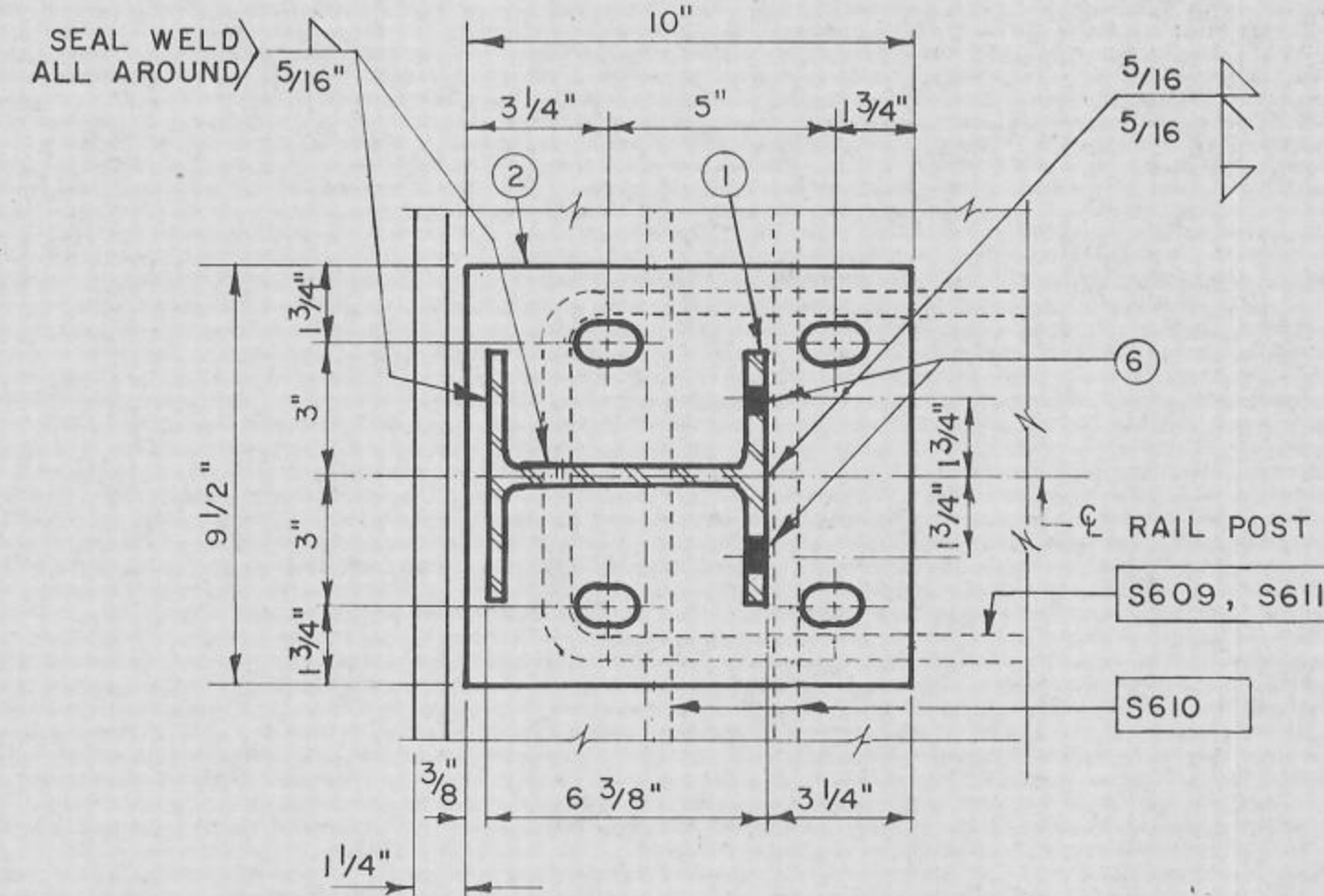
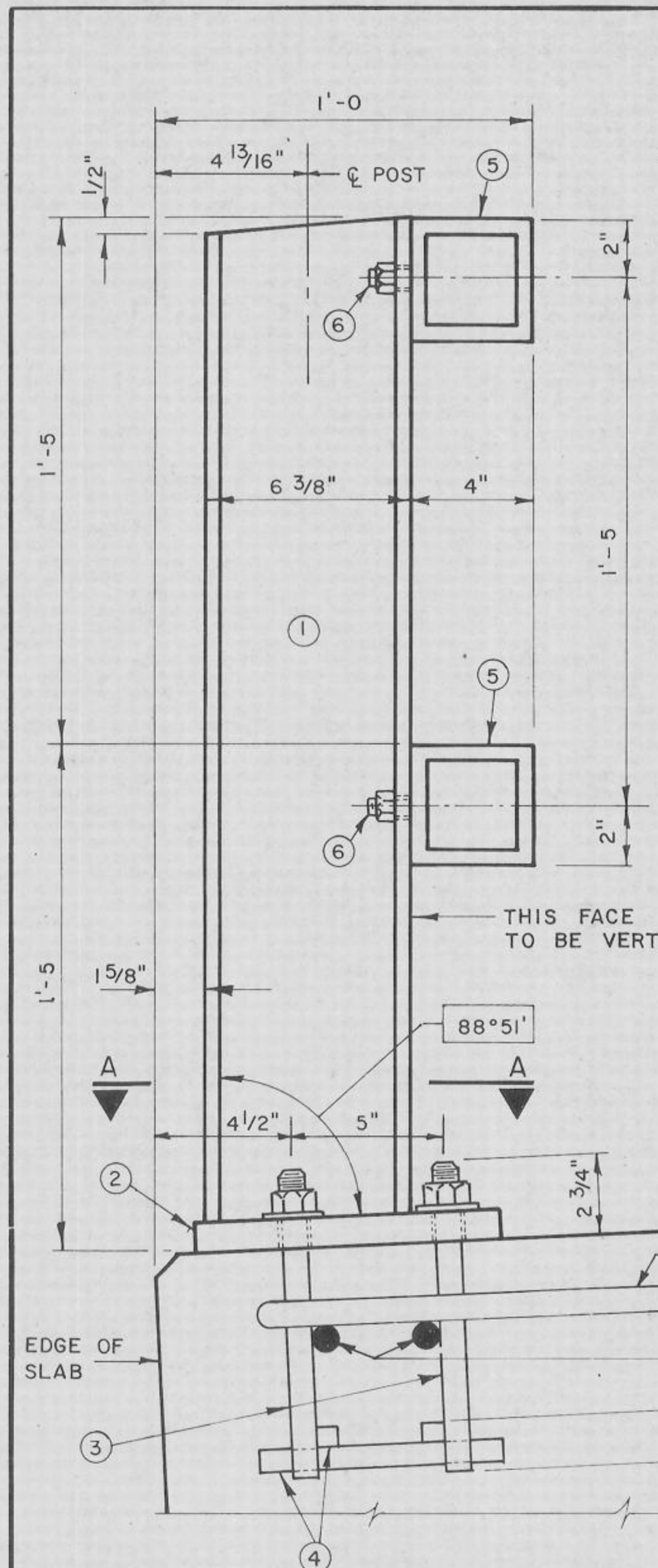
FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE (2) WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. SEAL BOTTOM EDGES OF PLATE (2) TO DECK.

ALL MATERIALS USED IN FABRICATION SHALL BE MADE FROM MATERIALS CONFORMING TO A.S.T.M. DESIGNATION A36 UNLESS NOTED OTHERWISE.

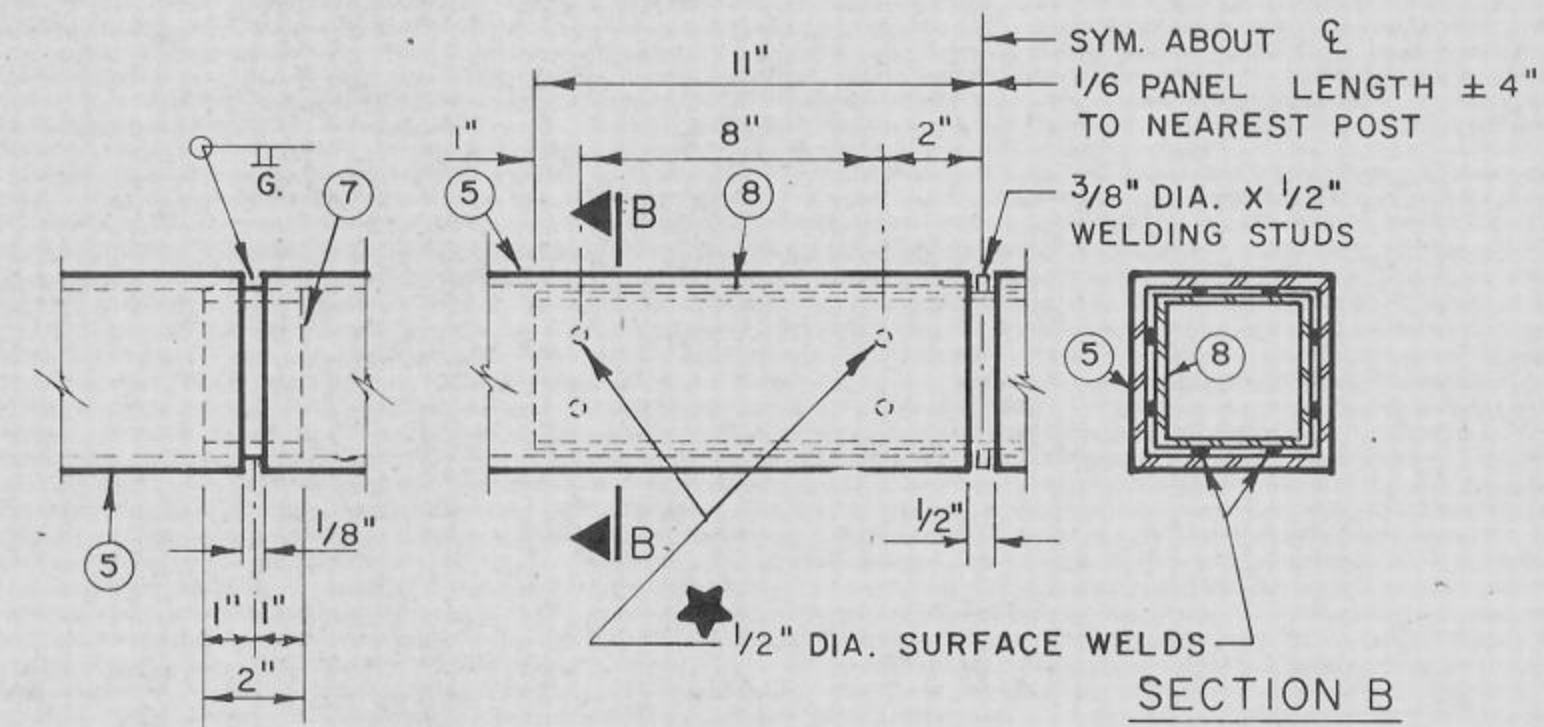
STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.

PRIOR TO GALVANIZING, ALL STEEL RAILING SHALL BE GIVEN A NO.6 COMMERCIAL BLAST CLEANING BY S.S.P.C. SPECIFICATIONS. BLAST CLEANING IS NOT REQUIRED FOR COLD FORMED TUBING (5), EXCEPT TO REMOVE WELDING SLAG AND IMPERVIOUS SUBSTANCES.

ALL POST SPACINGS ARE MEASURED HORIZONTALLY ALONG CENTERLINE OF POST BASE.



SECTION A



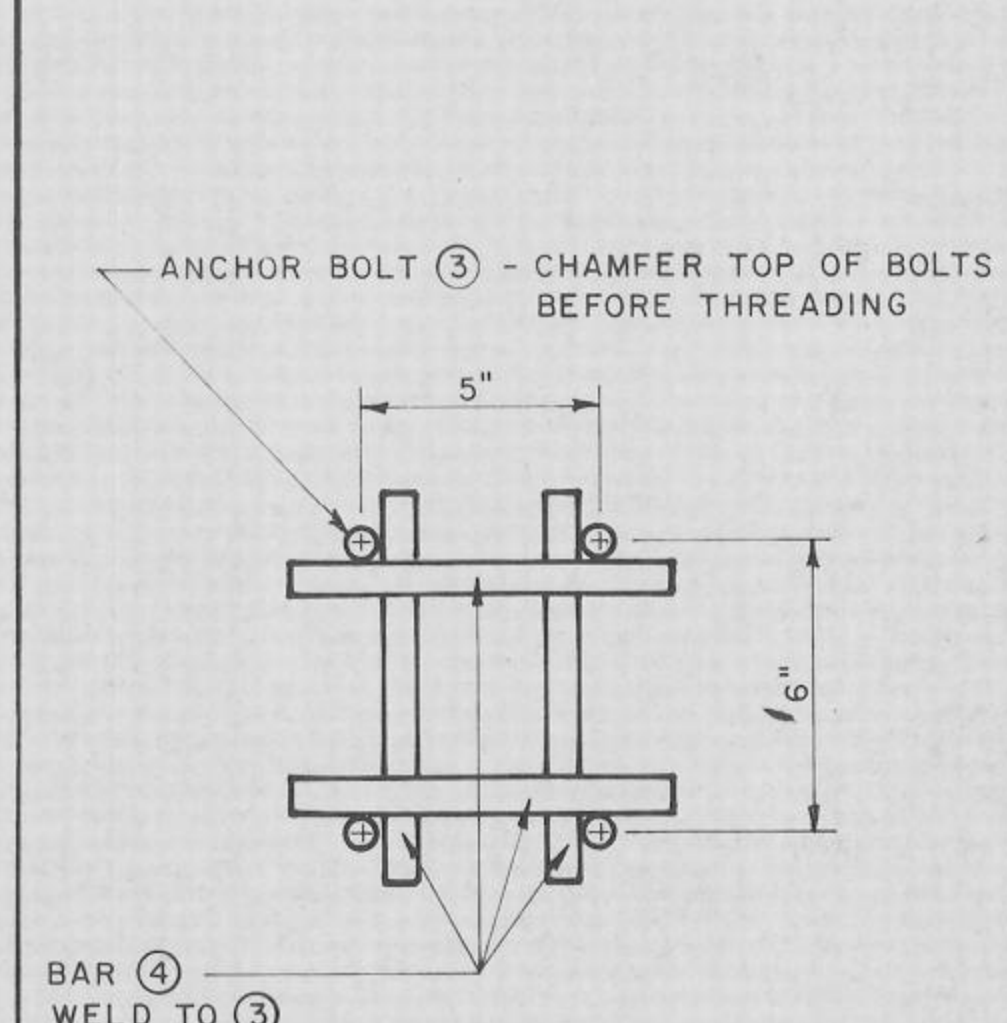
SHOP RAIL SPLICE DETAIL

FIELD ERECTION JOINT DETAIL

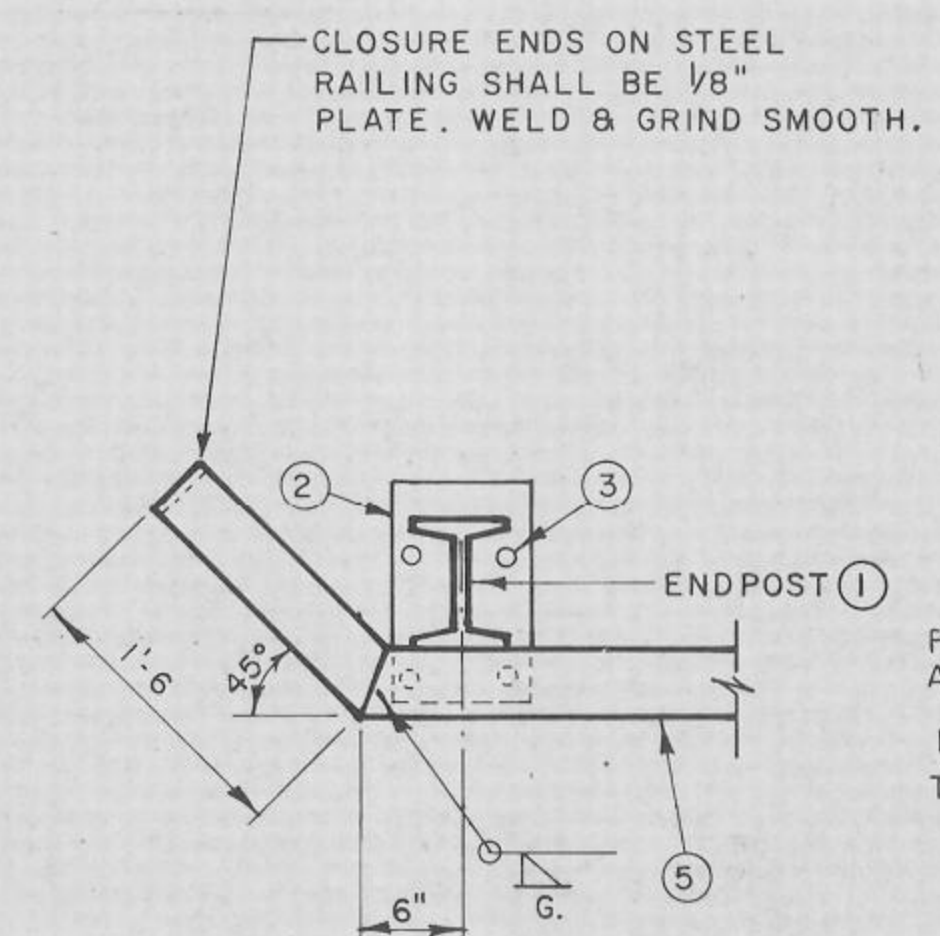
(LOCATION MUST BE SHOWN ON SHOP DRAWINGS)

★ MIN. 5/8" FLAT SURFACE DIA. PUNCHINGS OR STUDS MAY BE USED AS AN ALTERNATE.

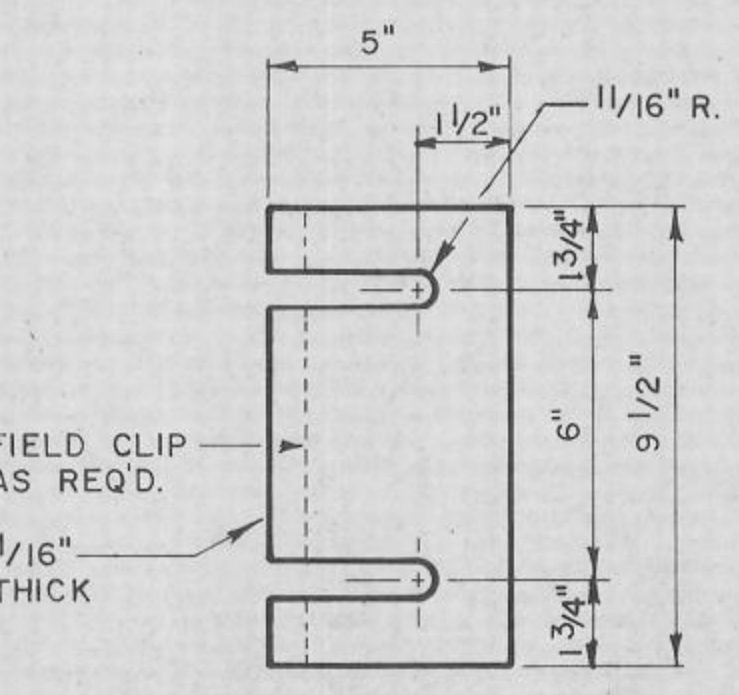
SECTION D THRU RAILING



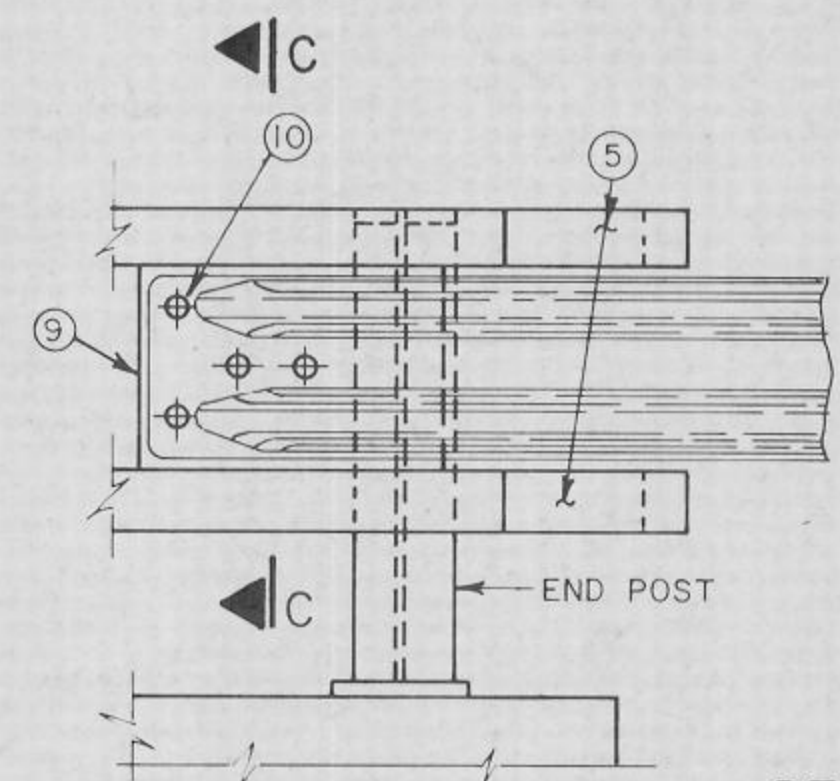
ANCHOR BOLT DETAIL



END DETAIL FOR WINGS

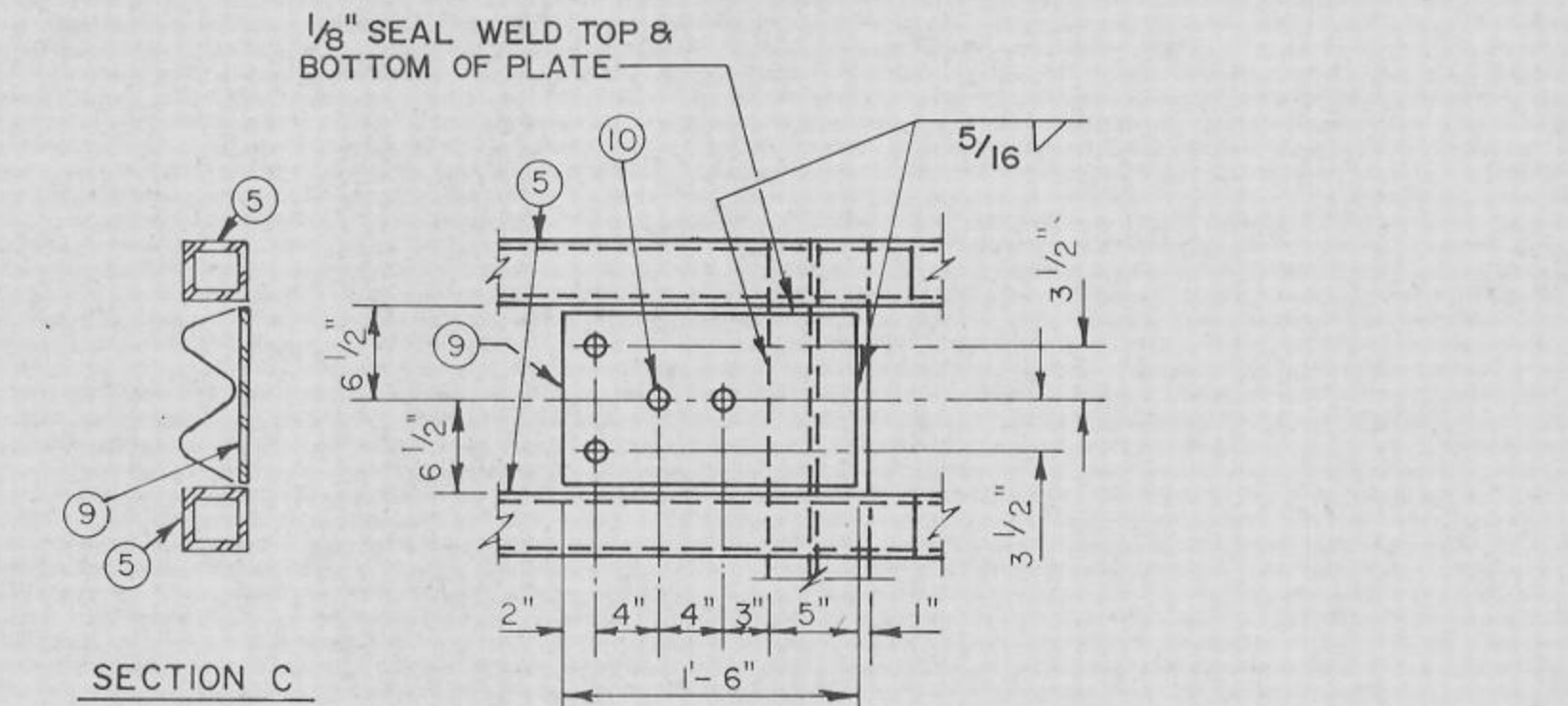


POST SHIM DETAIL
(4 PER POST)

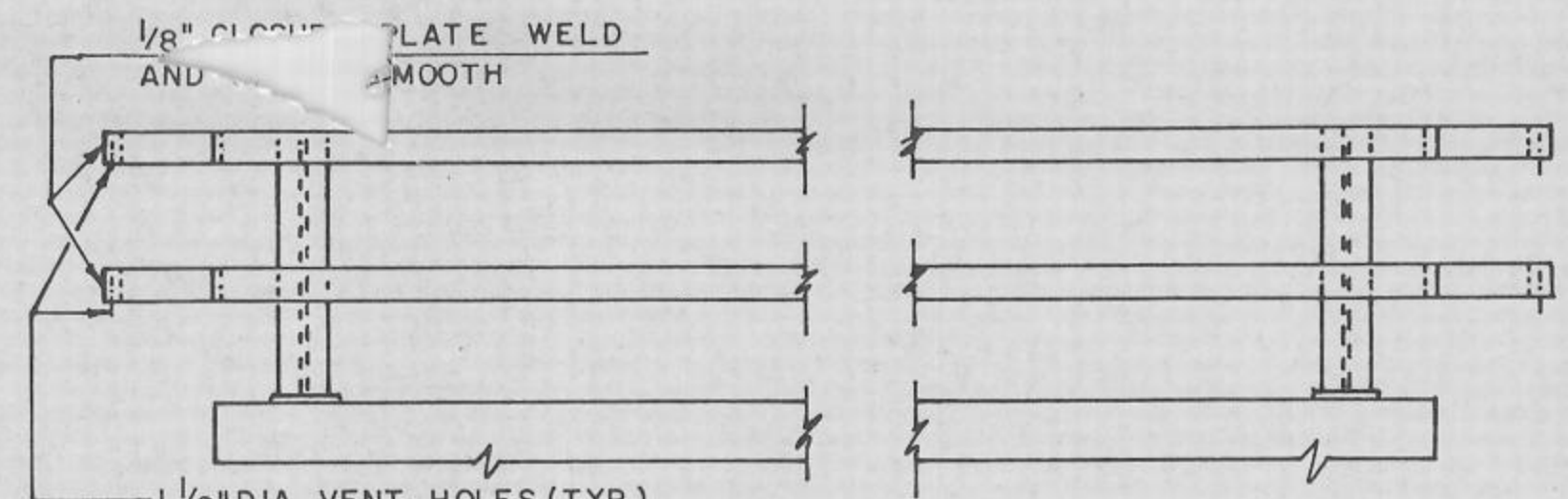


DETAIL AT END POST

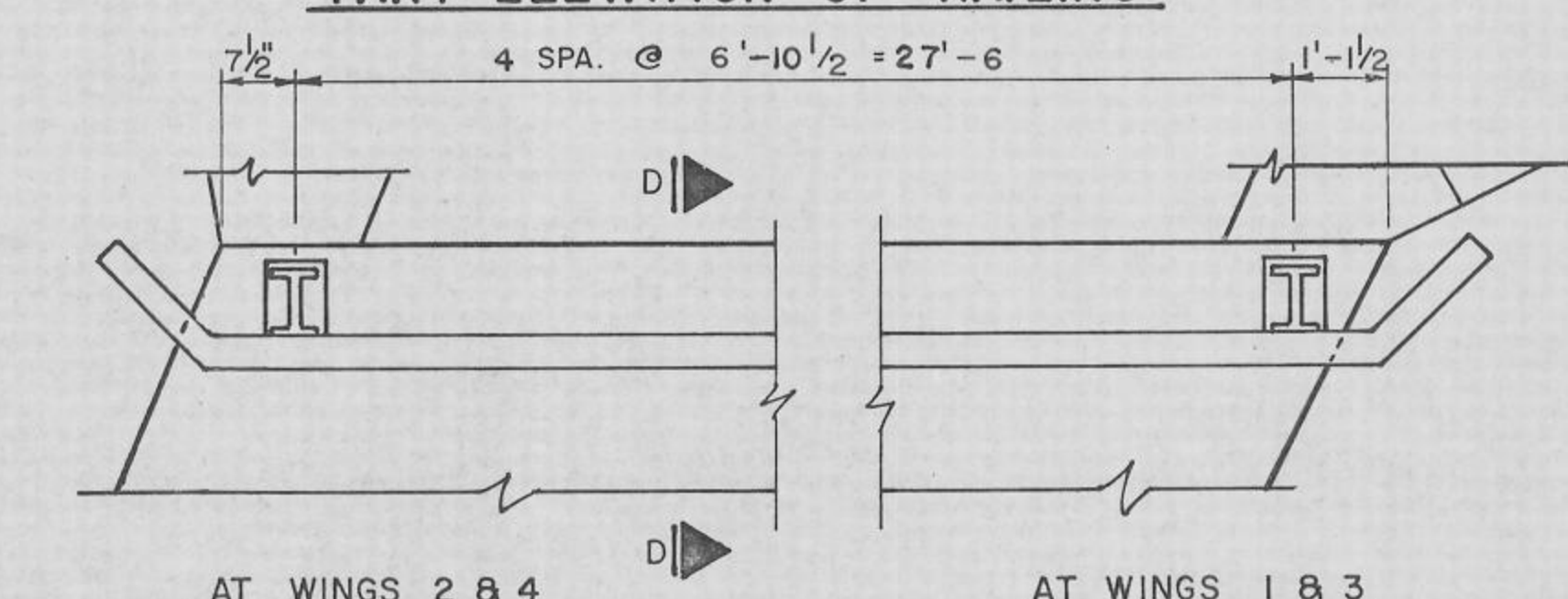
(PROVISION FOR BEAM GUARD RAIL ATTACHMENT ALL WINGS)



SECTION C



PART ELEVATION OF RAILING



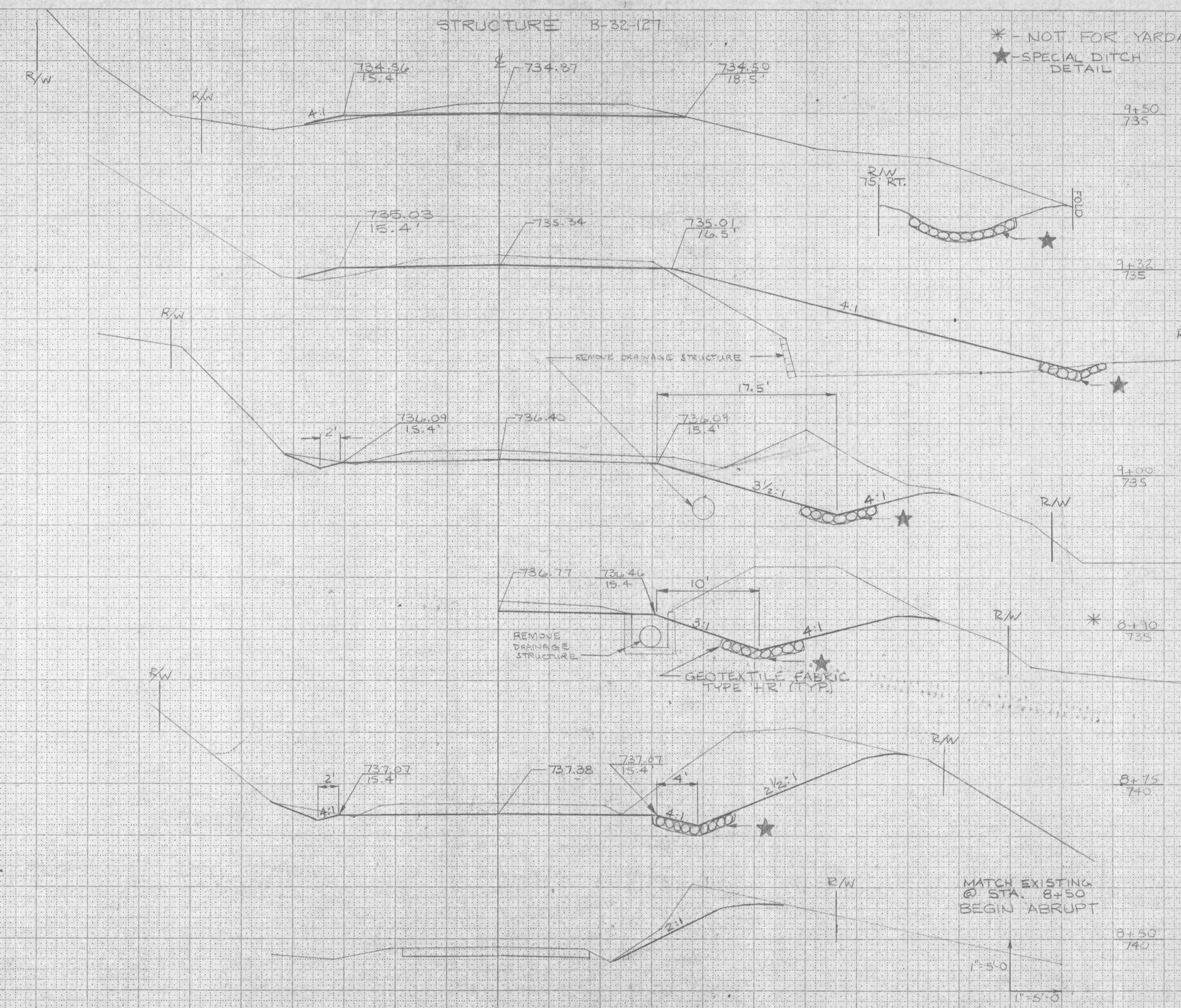
PART PLAN OF RAILING

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI '81	Drawn By R.L.R.	Plans Checked MSR
TUBULAR RAILING	STEEL TYPE 'F'	SHEET 9 OF 9	
		X8011	

STRUCTURE B-32-127

STATE PROJECT NUMBER: 7266-05-71
SHEET NUMBER: 211

* - NOT FOR YARDAGE
★ - SPECIAL DITCH DETAIL

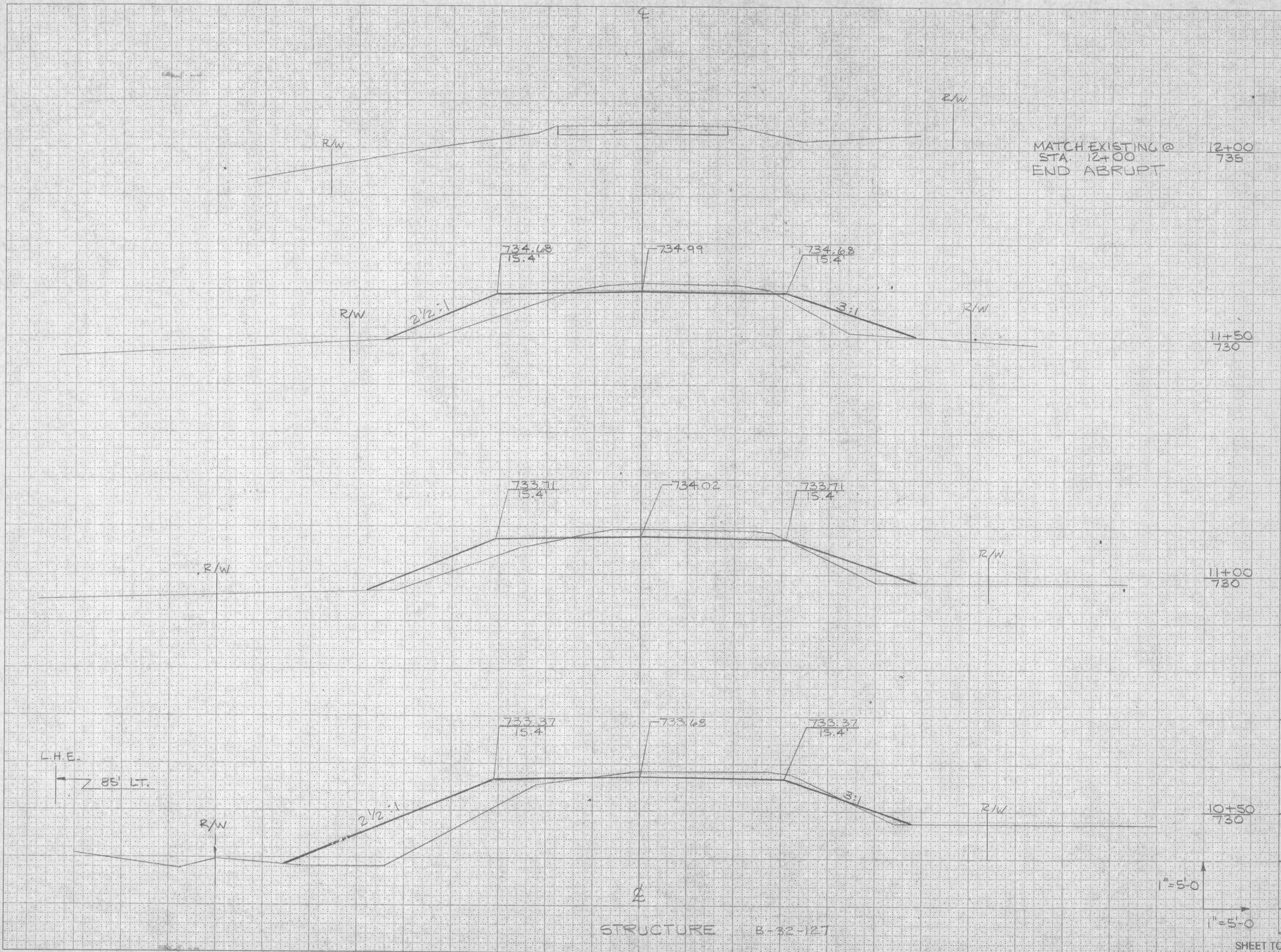


STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
8+50	735		
8+50	25	93	1
8+75	25	137	1
9+00	32	97	79
9+32	18	23	45
9+50	43	39	85
STRUCTURE			
TOTAL		389	211

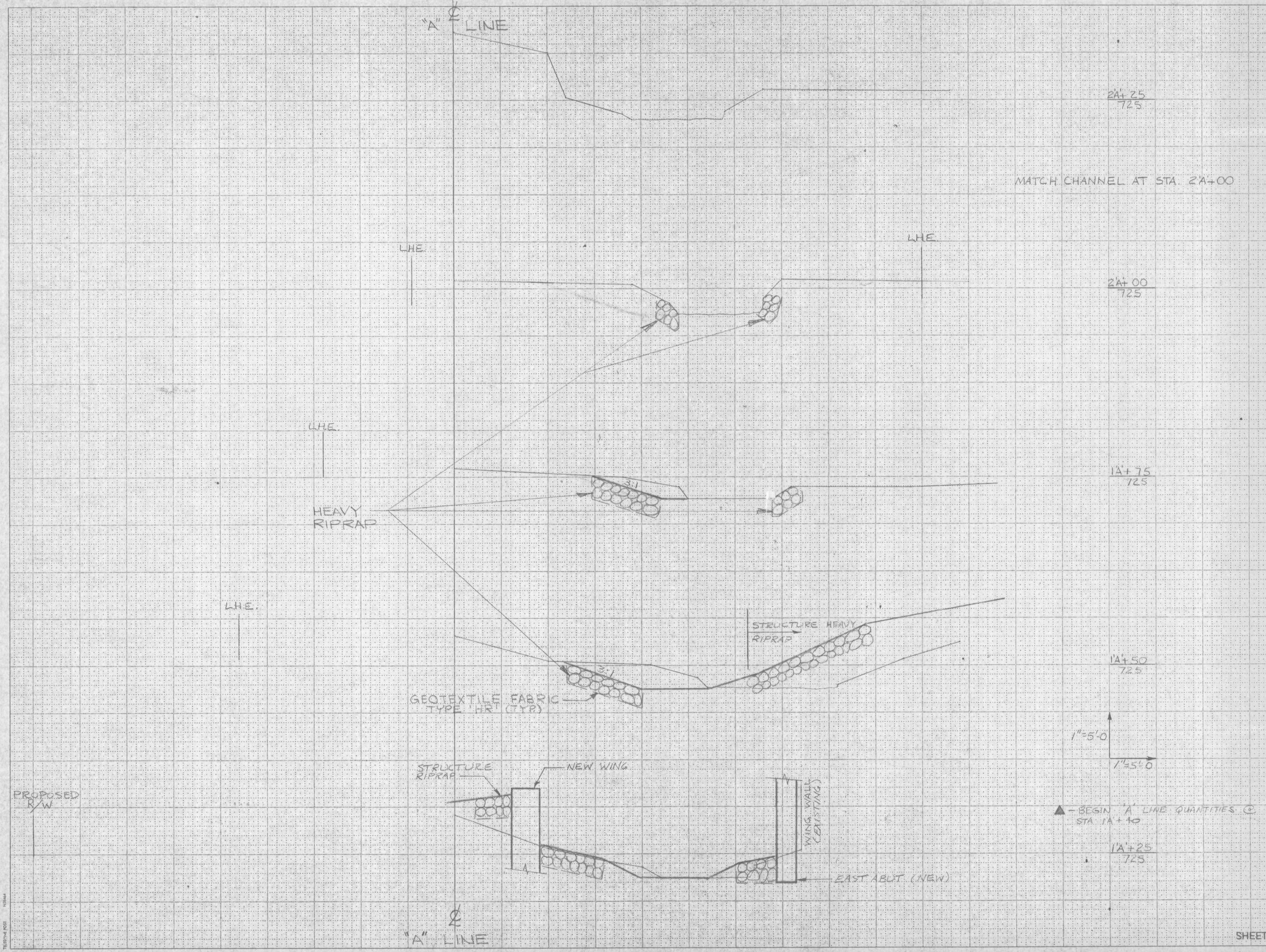
MATCH EXISTING @ STA. 8+50
BEGIN ABRUPT

1"=5'-0"
1"=5'-0"

SHEET TOTAL 389 211



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
		UNCL.	
12+00	735		
11+50	730	31	120
11+50		50	109
11+00	730	50	75
11+50		50	43
11+00	730		
10+50	730		
SHEET TOTAL			
		110	347



STATION	DISTANCE	YARDAGE	
		EXCAVATION	
		UNCL.	FILL
1A+25	▲		
1A+50	10	18	0
1A+75	25	32	0
2A+00	25	17	0
MATCH CHANNEL AT STA. 2A+00			
2A+25/725			
2A+00/725			
1A+75/725			
1A+50/725			
1A+25/725			
1A+40			
1A+25			
1A+00			
0+75			
0+50			
0+25			
0+00			
SHEET TOTAL		67	0

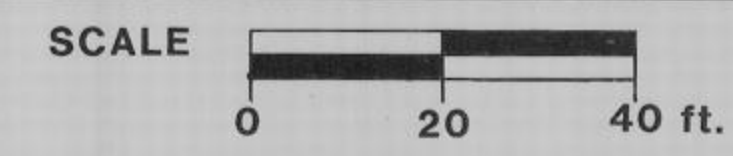
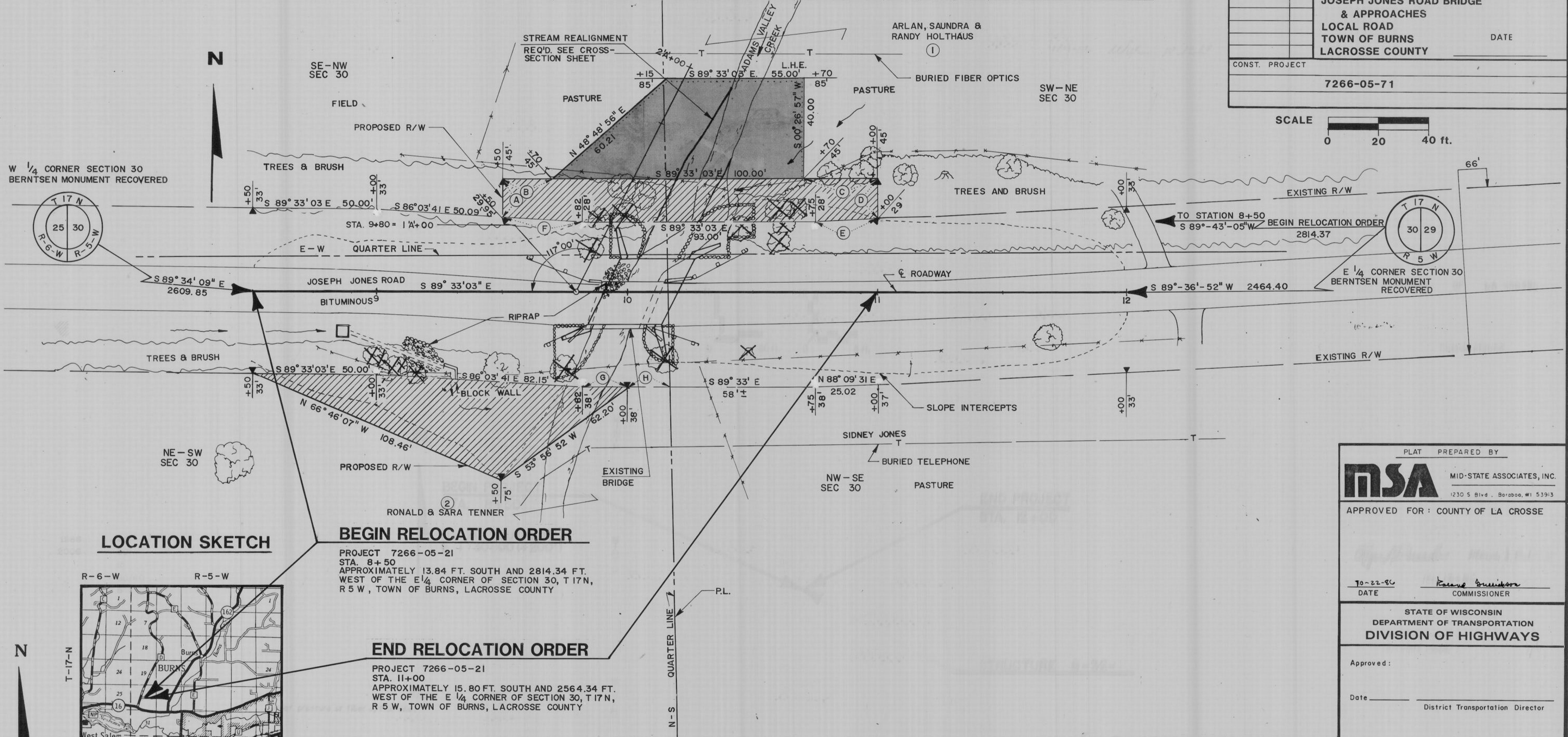
BEARING ORIENTATION

RIGHT-OF-WAY PLAT BEARINGS ARE ORIENTED TO THE EAST-WEST QUARTER LINE OF SECTION 30, T17N, R5W, LACROSSE COUNTY, WHICH HAS AN ASSUMED BEARING OF S 89° 56' 20" E. THE DIFFERENCE BETWEEN PROJECT PLAT BEARING REPRESENTS PLANE ANGLES IN DEGREES, MINUTES, AND SECONDS TO THE NEAREST SECOND.

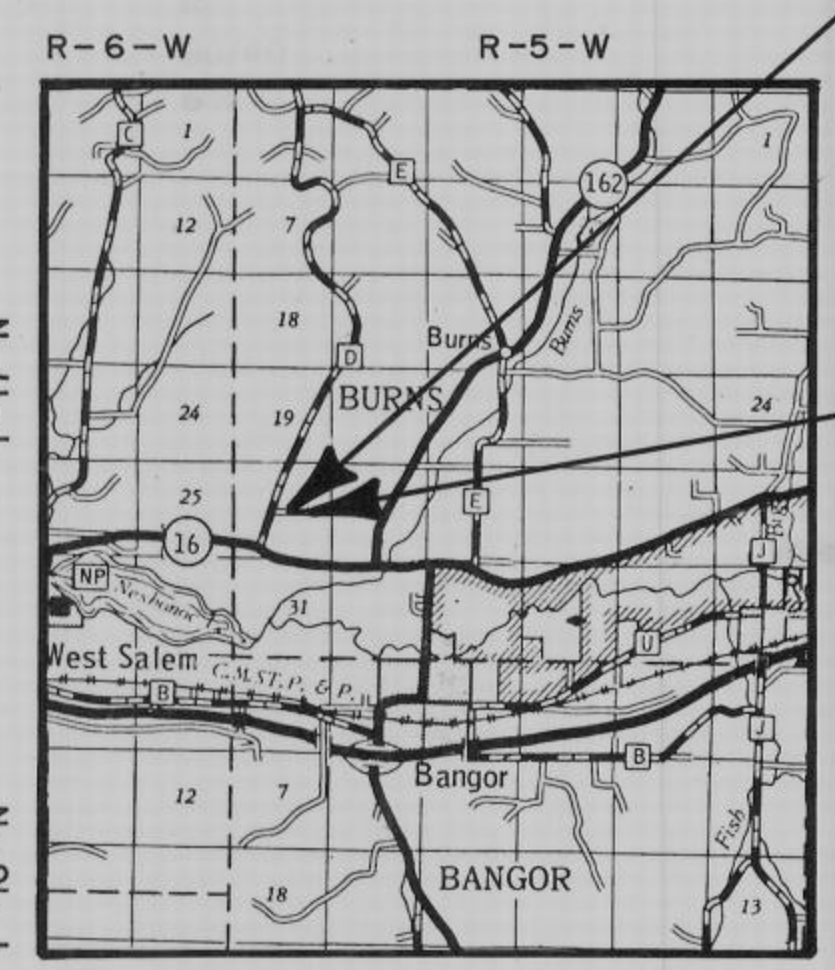
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED	L.H.E. ACRES		R/W ACRES REQ'D.			TOTAL ACRES REM.	OPERATIONS PROJECT I.D.
				TEMP.	PERM.	NEW	EXISTING	TOTAL		
1	4.0	ARLAN, SAUNDRA & RANDY HOLTHAUS	FEE & L.H.E.	0.07		0.06	0.11	0.17	327.71	7266-05-21
2	4.0	RONALD & SARA TENNER	FEE			0.07	0.11	0.18	159.98	7266-05-21

REVISION DATE	R/W PROJECT NUMBER 7266-05-21	SHEET NUMBER	TOTAL SHEETS
	FEDERAL PROJECT NUMBER		
PLAT OF RIGHT OF WAY REQUIRED FOR JOSEPH JONES ROAD BRIDGE & APPROACHES LOCAL ROAD TOWN OF BURNS LACROSSE COUNTY			
DATE			
CONST. PROJECT 7266-05-71			



LOCATION SKETCH



TOTAL NET LENGTH OF CENTERLINE = 0.047 MILES

BEGIN RELOCATION ORDER

PROJECT 7266-05-21
STA. 8+50
APPROXIMATELY 13.84 FT. SOUTH AND 2814.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

END RELOCATION ORDER

PROJECT 7266-05-21
STA. 11+00
APPROXIMATELY 15.80 FT. SOUTH AND 2564.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD. OTHER INFORMATION IS PROVIDED TO SUPPLEMENT THE BASIC PERIMETER DESCRIPTION AND SHALL NOT BE CONSTRUED TO PREVAIL OVER THE PERIMETER DESCRIPTION.

R/W COURSE DATA		
COURSE	BEARING	DISTANCE
A	N 00° 26' 57" E	15.05'
B	S 89° 33' 03" E	20.00'
C	S 89° 33' 03" E	30.00'
D	S 00° 26' 57" W	16.00'
E	S 88° 09' 31" W	25.02'
F	N 86° 03' 41" W	32.06'
G	S 89° 33' 03" E	18.00'
H	S 89° 33' E	17'±

PLAT PREPARED BY
MSA MID-STATE ASSOCIATES, INC.
1230 S Blvd., Baraboo, WI 53913

APPROVED FOR: COUNTY OF LA CROSSE

DATE: 10-22-90
COMMISSIONER: *Kenneth Brundson*

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: _____
Date: _____
District Transportation Director

Approved: _____
Date: _____
Director Bureau of Real Estate

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 6 WISCONSIN DIVISION

Approved: _____
Date: _____
Division Administrator

#629

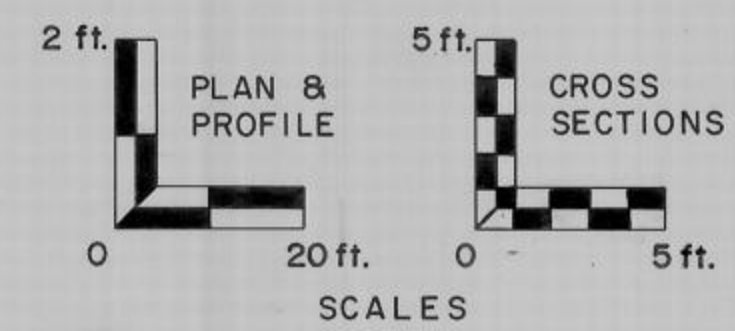
STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7266-05-71	BRZ 3299 (29)	1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT JOSEPH JONES ROAD (ADAMS VALLEY CREEK BRIDGE & APPROACHES)

TOWN ROAD LA CROSSE COUNTY

STATE PROJECT NUMBER
7266-05-71



Index of Sheets

Sheet No. 1	Title
Sheet No. 2	Typical Sections and Details
Sheet No. 3 - 3.1	Estimate of Quantities
Sheet No. 2	Miscellaneous Quantities
Sheet No. 4	Right of Way Plat
Sheet No. 5	Plan and Profile
Sheet No. 6 - 6.6	Standard Detail Drawings
Sheet No. -	Standard Sign Plates
Sheet No. 8 - 8.8	Structure Plans
Sheet No. -	Computer Earthwork Data
Sheet No. 9 - 9.2	Cross Sections

TOTAL SHEETS = 25



LACROSSE

Design Designation

A.D.T.	1986	=	40
A.D.T.	2006	=	80
D.H.V.	2006	=	15
D.		=	60/40
T.		=	6%

Conventional Signs

County Line	-----	Caution Symbol (Combustible fluids under pressure or fiber optics line)	
Township or Range Line	-----	Railroads	-----
Section Line	-----	Fence	-----
Corporate or City Limits	-----	Culverts in Place	-----
Property line	-----	Culverts Required	-----
Lot Line	-----	Power Pole	-----
Existing Right of Way Line	-----	Telephone or Telegraph Pole	-----
New Right of Way Line	-----	Right of Way Markers	-----
Base or Survey Line	-----	Marsh	-----
Slope Intercept	-----	Wooded Area	-----
Existing Roadway or Private Entrance	-----	Grade Elevation	-----

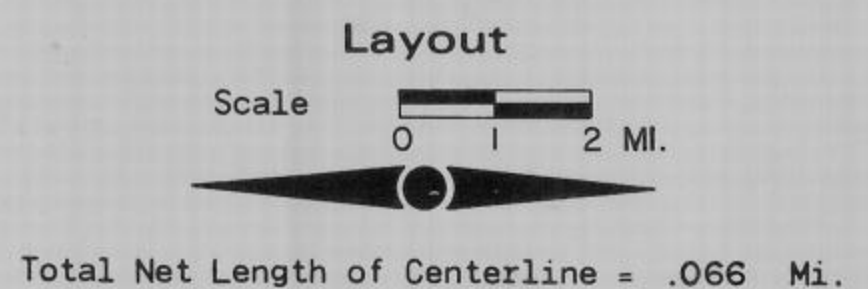
**BEGIN PROJECT
STA. 8+50**

N. 701 700 (±200')
E. 1 730 800 (±200')

**END PROJECT
STA. 12+00**



STRUCTURE B-32-127



COORDINATES ARE SCALED FROM THE U.S.G.S. TOPOGRAPHIC MAP, WEST SALEM, WISCONSIN, QUADRANGLE FOR IDENTIFICATION ONLY.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO USH '16' BURNS CREEK BRIDGE U.S.G.S. DATUM. CHISELED SQUARE IN N.E. CURB, ELEV. 741.26

APPROVED FOR
COUNTY OF LA CROSSE

DATE 10-22-86 Ronald Breichorn
COMMISSIONER

ORIGINAL PLANS
PREPARED BY

NSA MID-STATE ASSOCIATES, INC.
1230 S. Blvd., Baraboo, WI. 53913

ROGER L. DOESCHER
E-19812
BARABOO, WIS.
PROFESSIONAL ENGINEER

MARVIN S. RUHLAND
E-15125
BARABOO, WIS.
PROFESSIONAL ENGINEER

Roger L. Doescher Marvin S. Ruhland
ENGINEER
DATE 10-13-86

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Surveyor MID-STATE ASSOC. District Checker A.L.P.
Designer MID-STATE ASSOC. C. O. Checker for
District Supervisor G.W.P. C. O. Coordinator C.E.R.

Approved:
Date 2-5-87 James D. Brundler for
District Transportation Director

Approved:
Date 3/24/87 James Smith
State Design Engineer for Hwys.

Approved:
Date 3/27/87 E.J. Byrskit
Director of Engineering Development

U. S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 5 WISCONSIN DIVISION

Approved:
Date _____
Division Administrator

BRZ
NET 7266-5-71
W/7265-3-71

GENERAL NOTES

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE FERTILIZED, SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

THE BITUMINOUS SURFACE SHALL TAPER FROM 26 FEET WIDE AT THE END OF BRIDGE TO 18 FEET WIDE, 50 FEET FROM THE END OF THE BRIDGE AT CENTERLINE OF ROADWAY.

SILT FENCE TO BE PLACED AS DIRECTED BY THE ENGINEER.

THE CONTROL SURVEY FOR THIS PROJECT WAS CONDUCTED AND MET SPECIFICATIONS FOR THIRD ORDER CONTROL SURVEYS.

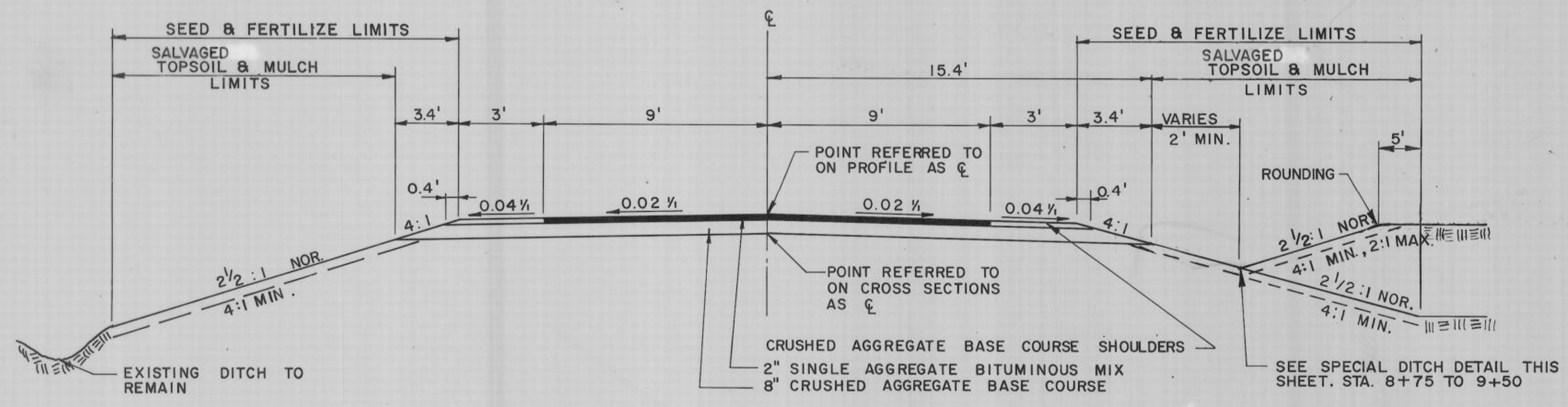
WHEN THE QUANTITY OF ITEMS OF BASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

BEARINGS SHOWN ON THIS PLAN ARE ORIENTED TO THE EAST-WEST QUARTER LINE OF SECTION 30, T17N, R5W, WITH AN ASSUMED BEARING OF S 89° 56' 20" E. SHOWN TO THE NEAREST SECOND.

DELINEATORS AND POSTS SHALL BE STEEL.

APPLICABLE STANDARD DETAIL DRAWINGS

8E9-2	SILT FENCE
12A3-4	NAME PLATE (STRUCTURES)
15A1-5	MARKER POST FOR RIGHT-OF-WAY
15C1-7	CONSTRUCTION BARRICADES AND STANDARD SIGNS
15C6-1	TRAFFIC CONTROL DEVICES FOR TWO LANE BRIDGES
15A2-2	DELINEATOR POSTS, MARKER POSTS & DELINEATORS
15C2-1	TRAFFIC CONTROL TO CLOSE HIGHWAY UNDER CONSTRUCTION



TYPICAL SECTION

SUMMARY OF MISCELLANEOUS QUANTITIES

CLEARING & GRUBBING

STATION TO STATION	LOCATION	CLEARING STA.	GRUBBING STA.
9+00	12+00 LT. & RT.	3	3

CRUSHED AGGREGATE BASE COURSE

STATION TO STATION	BASE C.Y.	SHOULDER C.Y.
8+50	130	10
10+20.5	160	10

HEAVY RIPRAP

STATION TO STATION	C.Y.
8±75	45
1"A"±40	45

MARKER POSTS FOR RIGHT-OF-WAY

STATION	LOCATION	EACH
8+50	33' LT. & RT.	2
9+50	75' RT., 29.95' LT., 45' LT.	3
10+00	38' RT.	1
11+00	29' LT., 45' LT.	2
12+00	33' LT. & RT.	2

EROSION CONTROL

STATION TO STATION	SALVAGED TOPSOIL S.Y.	MULCHING S.Y.	TYPE B FERT. CWT.	SEED LB.
8+50	430	430	0.4	15
10+19	550	550	0.45	19
BORROW PIT			0.15	6

REMOVING DRAINAGE STRUCTURE

STATION	LOCATION	L.S.
8+80	9+35 RT.	1

SILT FENCE

LOCATION	L.F.
WEST BANK	130
EAST BANK	130
1"A"±35	15
1"A"±80	15

GEOTEXTILE FABRIC, TYPE 'HR'

STATION TO STATION	S.Y.
8±75	85
1"A"±40	75

SINGLE AGGREGATE BITUMINOUS MIX

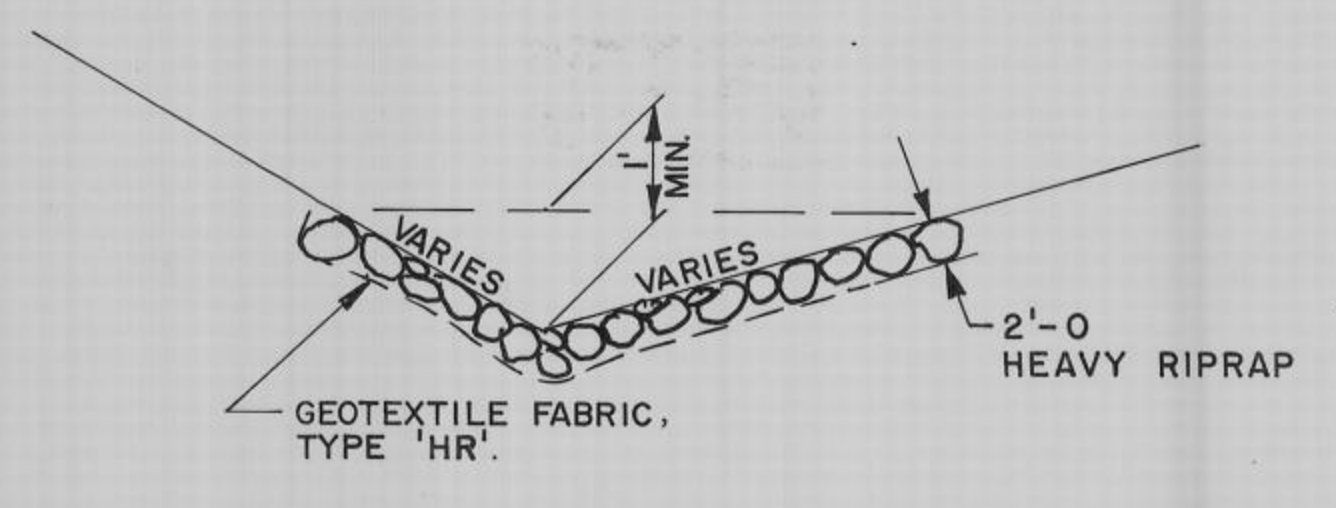
STATION TO STATION	TON
8+50	33
BRIDGE TAPERS	6
10+20.5	41

DELINEATORS

LOCATION	EACH	POSTS EACH
9+12 RT.	2	1
9+21 LT.	2	1
9+37 RT.	2	1
9+46 LT.	2	1
9+61 RT.	2	1
9+71 LT.	2	1
10+40 RT.	2	1
10+50 LT.	2	1
10+65 RT.	2	1
10+75 LT.	2	1
10+90 RT.	2	1
11+00 LT.	2	1

STANDARD ABBREVIATIONS

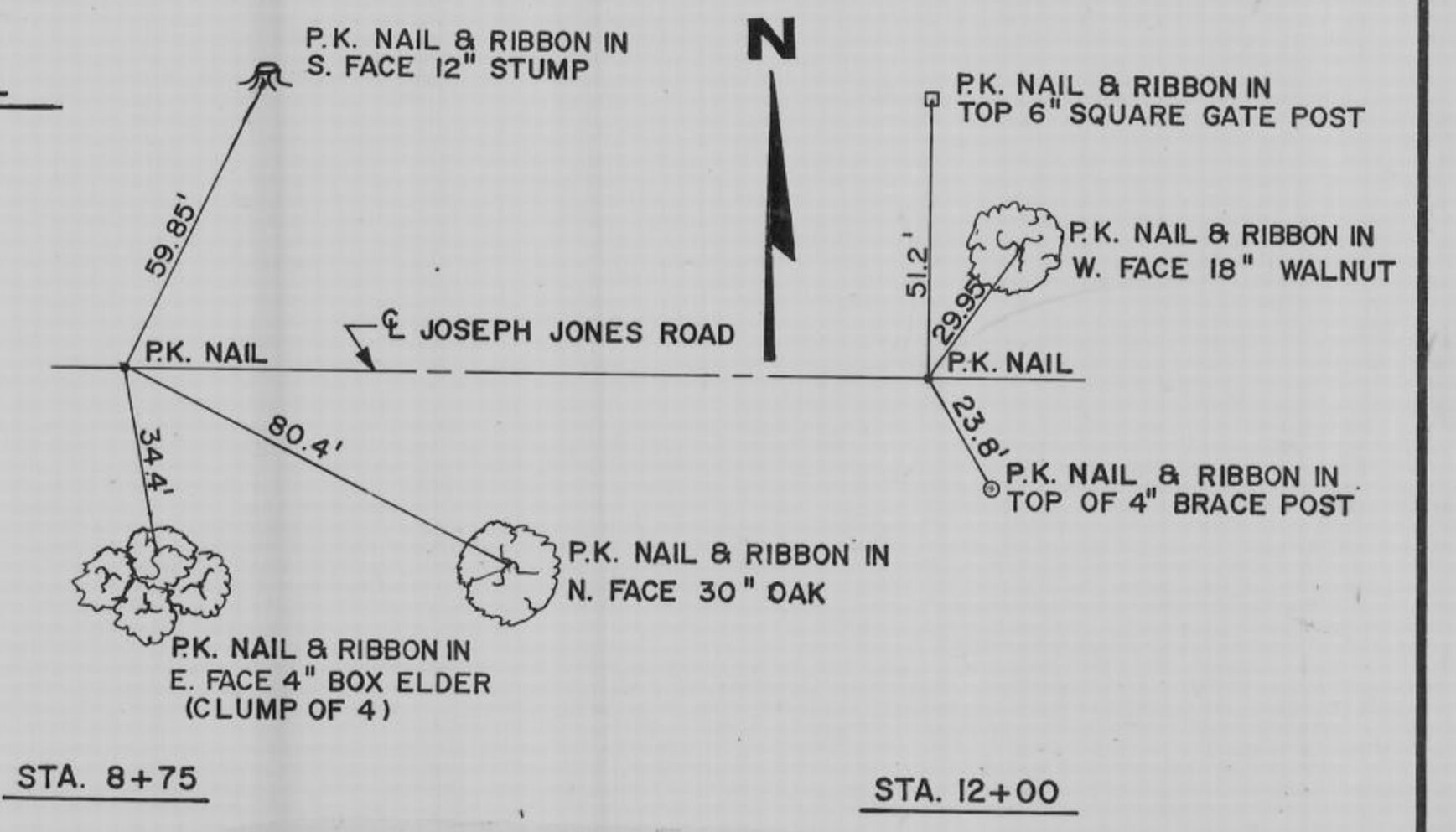
ABUT.	ABUTMENT
A.D.T.	AVERAGE DAILY TRAFFIC
B.F.	BACKFACE
BRG.	BEARING
B.M.	BENCH MARK
BIT.	BITUMINOUS
CL.	CLEAR
C, C, C/L	CENTERLINE
C.Y. OR CU.YD.	CUBIC YARD
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
D.	DIRECTIONAL DISTRIBUTION
E.L. OR ELEV.	ELEVATION
EXC.	EXCAVATION
EXIST.	EXISTING
FERT.	FERTILIZER
FT.	FOOT
F.F.	FRONT FACE
GAL.	GALLON
H.W.	HIGHWATER
HORIZ.	HORIZONTAL
CWT.	HUNDREDWEIGHT
I.D.	INSIDE DIAMETER OR DIMENSION
L.H.F.	LEFT-HAND FORWARD
L.H.E.	LIMITED HIGHWAY EASEMENT
L.F. OR LIN.FT.	LINEAR FOOT
L.S.	LUMP SUM
LT.	LEFT
NO.	NUMBER
OBS.	OBSERVED
O.D.	OUTSIDE DIAMETER
P.L.	PROPERTY LINE
P.C.	POINT OF CURVATURE
P.I.	POINT OF INTERSECTION
P.T.	POINT OF TANGENCY
P.C.W.	POLYVINYL CHLORIDE WATERSTOP
LB.	POUND
R.	RADIUS
R.R.	RAILROAD
R.	RANGE
REQ'D	REQUIRED
RT.	RIGHT
R/W	RIGHT-OF-WAY
SHLDR.	SHOULDER
SPA.	SPACING
S.Y. OR SQ.YD.	SQUARE YARD
STA.	STATION
SYM.	SYMMETRIC
TRANS.	TRANSVERSE
T.	TRUCKS (PERCENT OF)
TYP.	TYPICAL
UNCL.	UNCLASSIFIED
V.	VELOCITY OR DESIGN SPEED
VERT.	VERTICAL
V.C.	VERTICAL CURVE
W.	WEST
WT.	WEIGHT



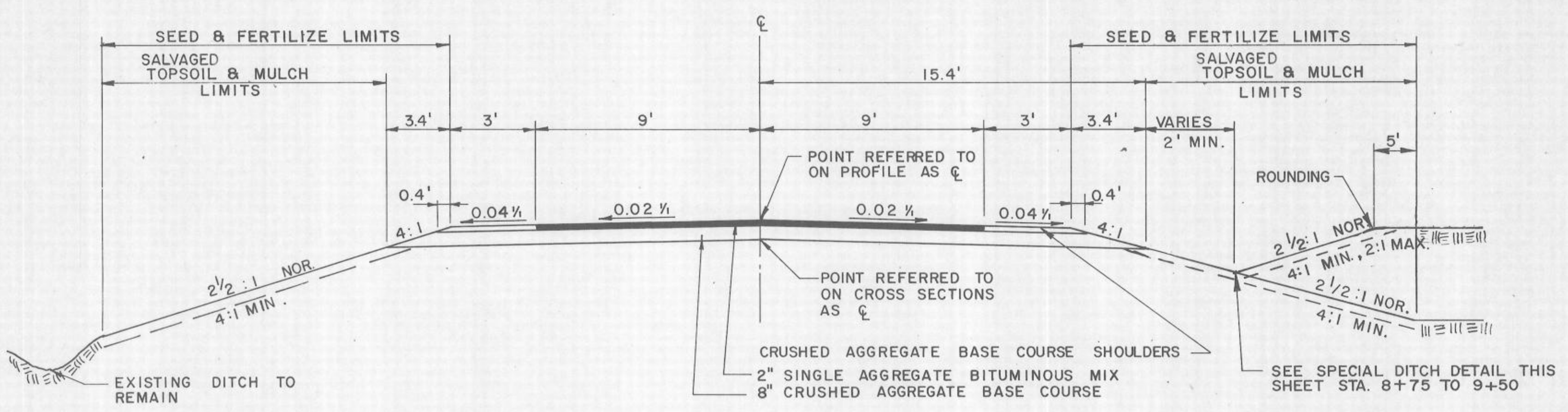
SPECIAL DITCH DETAIL

UTILITIES

FIBRE OPTICS LINE & TELEPHONE
 GENERAL TELEPHONE CO. OF WISCONSIN
 43 N. FIRST STREET
 BLACK RIVER FALLS, WI 54615
 ATTN.: BRUCE LAMBERT
 715-284-4376



CENTERLINE TIES



TYPICAL SECTION

STANDARD ABBREVIATIONS

- ABUT. ABUTMENT
- A.D.T. AVERAGE DAILY TRAFFIC
- B.F. BACKFACE
- BRG. BEARING
- B.M. BENCH MARK
- BIT. BITUMINOUS
- CL. CLEAR
- CL. CENTERLINE
- C, C, C/L CUBIC YARD, OR CUBIC YARD
- D.H.V. DESIGN HOUR VOLUME
- DIA. DIAMETER
- D. DIRECTIONAL DISTRIBUTION
- ELEV. OR ELEV. ELEVATION
- EXC. EXCAVATION
- EXIST. EXISTING
- FERT. FERTILIZER
- FT. FOOT
- F.F. FRONT FACE
- GAL. GALLON
- H.W. HIGHWATER
- HORIZ. HORIZONTAL
- CWT. HUNDREDWEIGHT
- I.D. INSIDE DIAMETER OR DIMENSION
- L.H.F. LEFT-HAND FORWARD
- L.H.E. LIMITED HIGHWAY EASEMENT
- L.F. OR LIN.FT. LINEAR FOOT
- L.S. LUMP SUM
- LT. LEFT
- NO. NUMBER
- OBS. OBSERVED
- O.D. OUTSIDE DIAMETER
- P.L. PROPERTY LINE
- P.C. POINT OF CURVATURE
- P.I. POINT OF INTERSECTION
- P.T. POINT OF TANGENCY
- P.C.W. POLYVINYL CHLORIDE WATERSTOP
- LB. POUND
- R. RADIUS
- R.R. RAILROAD
- R. RANGE
- REQ'D. REQUIRED
- RT. RIGHT
- R/W RIGHT-OF-WAY
- SHLDR. SHOULDER
- SPA. SPACING
- S.Y. OR SQ.YD. SQUARE YARD
- STA. STATION
- SYM. SYMMETRIC
- TRANS. TRANSVERSE
- T. TRUCKS (PERCENT OF)
- TYP. TYPICAL
- UNCL. UNCLASSIFIED
- V. VELOCITY OR DESIGN SPEED
- VERT. VERTICAL CURVE
- V.C. VERTICAL CURVE
- W. WEST
- WT. WEIGHT

STATE PROJECT NUMBER	SHEET NO.
7266-05-71	
TYPICAL SECTION, DETAILS & MISCELLANEOUS QUANTITIES	

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APPLICABLE STANDARD DETAIL DRAWINGS

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12A3-4	NAME PLATE (STRUCTURES)
15A1-5	MARKER POST FOR RIGHT-OF-WAY
15C1-7	CONSTRUCTION BARRICADES AND STANDARD SIGNS
15C6-1	TRAFFIC CONTROL DEVICES FOR TWO LANE BRIDGES

SUMMARY OF MISCELLANEOUS QUANTITIES

CLEARING & GRUBBING

STATION TO STATION	LOCATION	CLEARING STA.	GRUBBING STA.
9+00	12+00 LT. & RT.	3	3

REMOVING DRAINAGE STRUCTURE

STATION	LOCATION	L.S.
8+80	9+35 RT.	1

CRUSHED AGGREGATE BASE COURSE

STATION TO STATION	BASE C.Y.	SHOULDER C.Y.
8+50	9+91.5	130
10+20.5	12+00	160

SILT FENCE

LOCATION	L.F.
WEST BANK	130
EAST BANK	130
1"A" ± 35	15
1"A" ± 80	15

HEAVY RIPRAP

STATION TO STATION	C.Y.
8 ± 75	9 ± 50 RT.
1"A" ± 40	2"A" ± 00 RT.

GEOTEXTILE FABRIC, TYPE 'HR'

STATION TO STATION	S.Y.
8 ± 75	9 ± 50 RT.
1"A" ± 40	2"A" ± 00 RT.

MARKER POSTS FOR RIGHT-OF-WAY

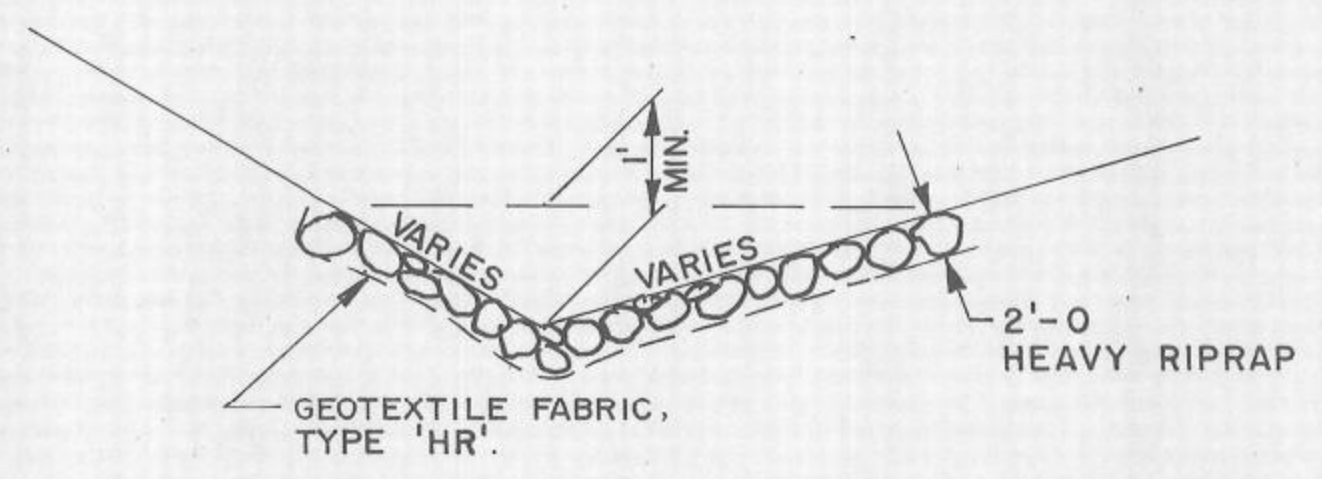
STATION	LOCATION	EACH
8+50	33' LT. & RT.	2
9+50	75' RT., 29.95' LT., 45' LT.	3
10+00	38' RT.	1
11+00	29' LT., 45' LT.	2
12+00	33' LT. & RT.	2

SINGLE AGGREGATE BITUMINOUS MIX

STATION TO STATION	TON
8+50	9+91.5
BRIDGE TAPERS	6
10+20.5	12+00
	41

EROSION CONTROL

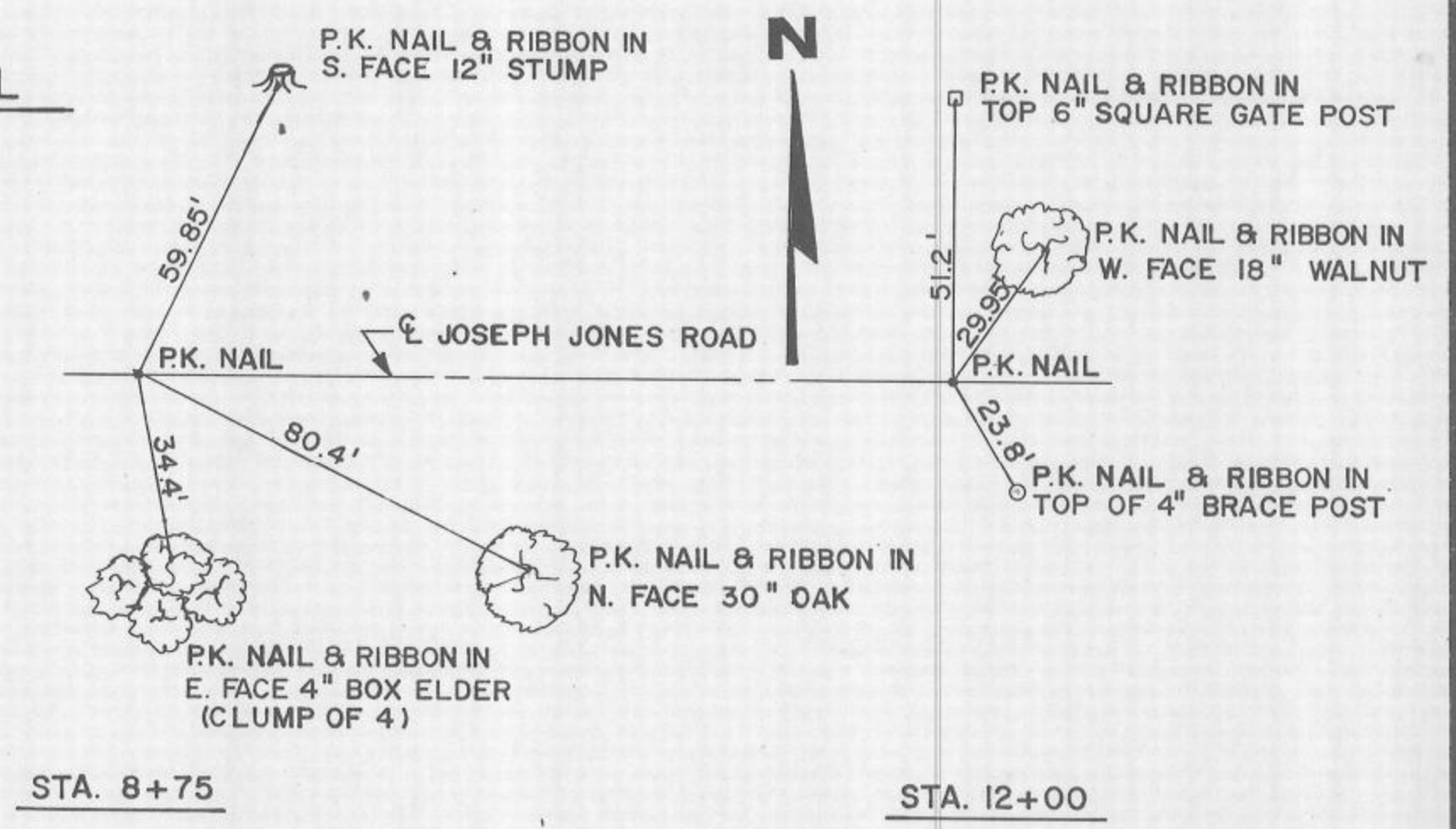
STATION TO STATION	SALVAGED TOPSOIL S.Y.	MULCHING S.Y.	TYPE B FERT. CWT.	SEED LB.
8+50	9+93	430	0.4	15
10+19	12+00	550	0.45	19
BORROW PIT			0.15	6



SPECIAL DITCH DETAIL

UTILITIES

FIBRE OPTICS LINE & TELEPHONE
 GENERAL TELEPHONE CO. OF WISCONSIN
 43 N. FIRST STREET
 BLACK RIVER FALLS, WI 54615
 ATTN.: BRUCE LAMBERT
 715-284-4376



CENTERLINE TIES

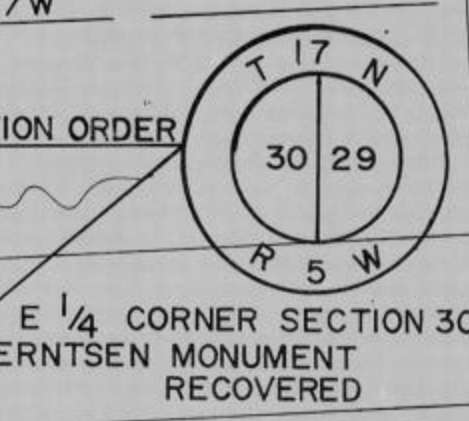
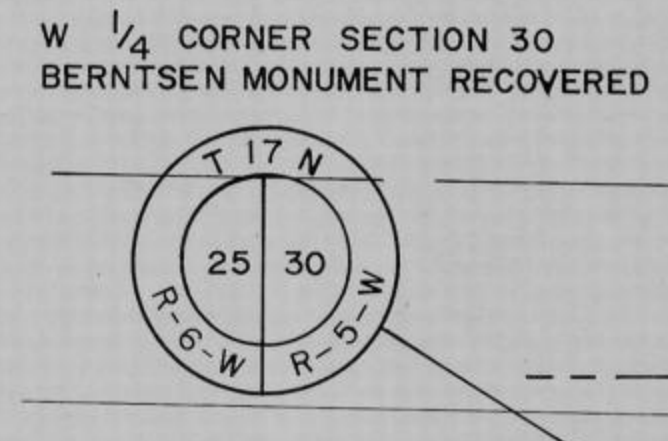
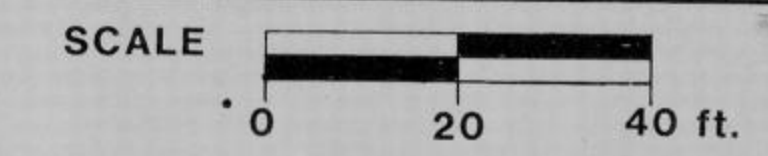
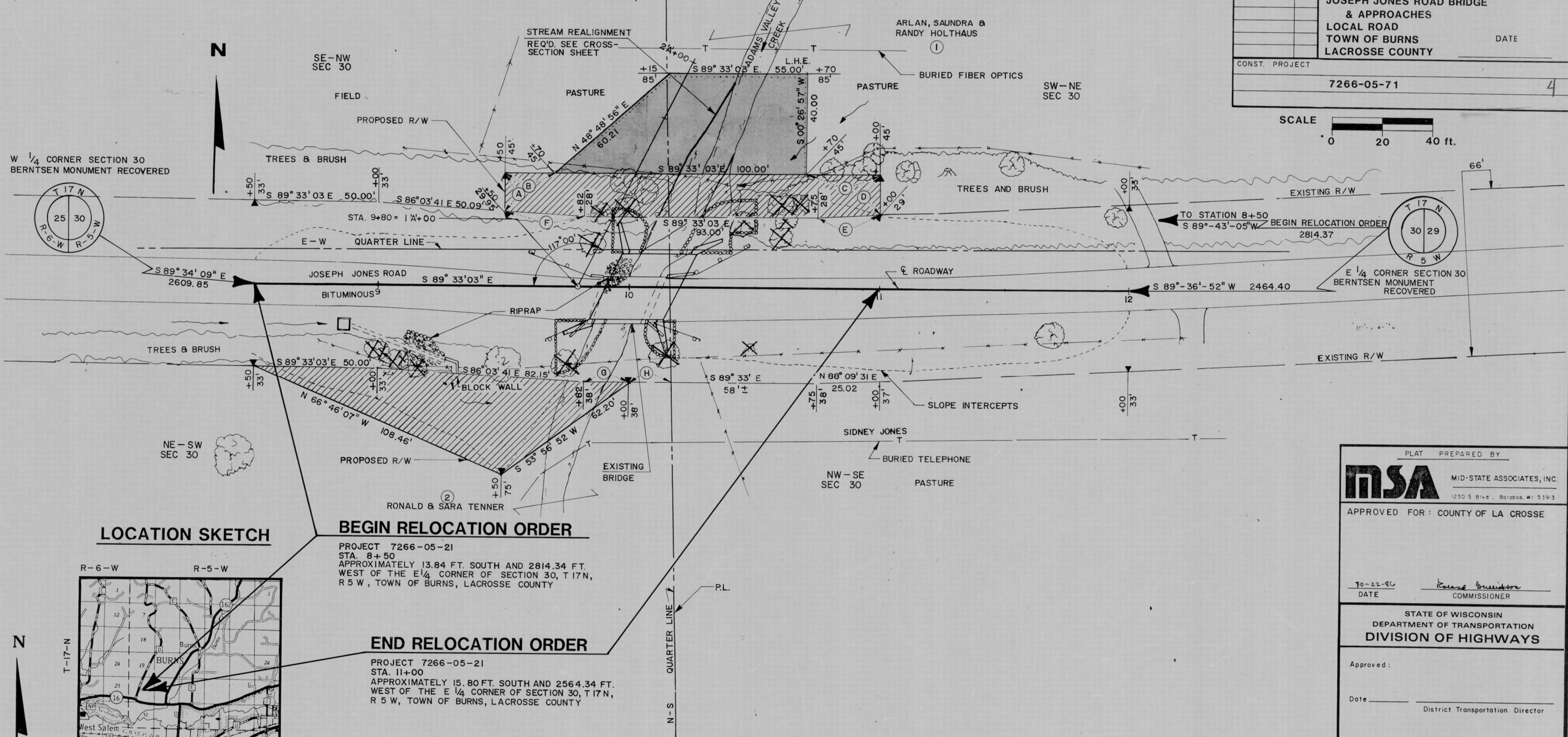
BEARING ORIENTATION

RIGHT-OF-WAY PLAT BEARINGS ARE ORIENTED TO THE EAST-WEST QUARTER LINE OF SECTION 30, T17N, R5W, LACROSSE COUNTY, WHICH HAS AN ASSUMED BEARING OF S 89° 56' 20" E. THE DIFFERENCE BETWEEN PROJECT PLAT BEARING REPRESENTS PLANE ANGLES IN DEGREES, MINUTES, AND SECONDS TO THE NEAREST SECOND.

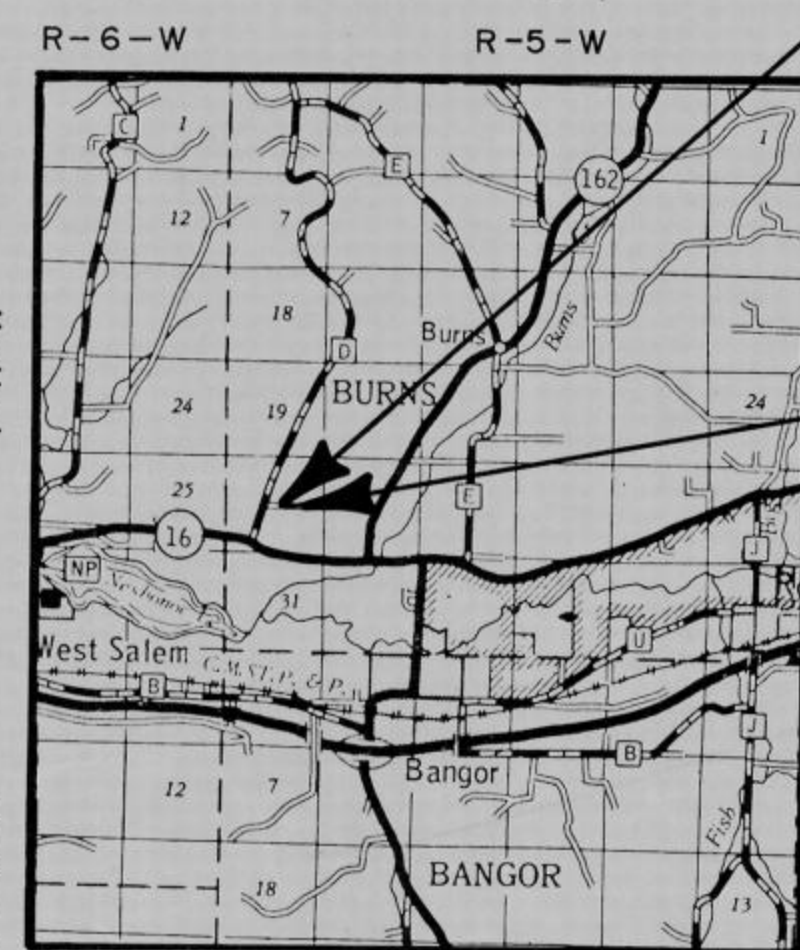
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED	L.H.E. ACRES		R/W ACRES REQ'D.			TOTAL ACRES REM.	OPERATIONS PROJECT I.D.
				TEMP.	PERM.	NEW	EXISTING	TOTAL		
1	4.0	ARLAN, SAUNDRA & RANDY HOLTHAUS	FEE & L.H.E.	0.07		0.06	0.11	0.17	327.71	7266-05-21
2	4.0	RONALD & SARA TENNER	FEE			0.07	0.11	0.18	159.98	7266-05-21

REVISION DATE	R/W PROJECT NUMBER 7266-05-21	SHEET NUMBER	TOTAL SHEETS
	FEDERAL PROJECT NUMBER		
PLAT OF RIGHT OF WAY REQUIRED FOR			
JOSEPH JONES ROAD BRIDGE & APPROACHES			
LOCAL ROAD TOWN OF BURNS LACROSSE COUNTY			
DATE			
CONST. PROJECT			
7266-05-71			
4			



LOCATION SKETCH



BEGIN RELOCATION ORDER

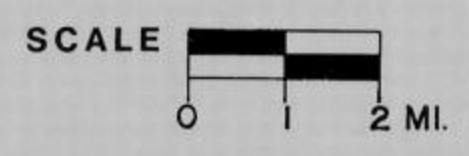
PROJECT 7266-05-21
STA. 8+50
APPROXIMATELY 13.84 FT. SOUTH AND 2814.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

END RELOCATION ORDER

PROJECT 7266-05-21
STA. 11+00
APPROXIMATELY 15.80 FT. SOUTH AND 2564.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD. OTHER INFORMATION IS PROVIDED TO SUPPLEMENT THE BASIC PERIMETER DESCRIPTION AND SHALL NOT BE CONSTRUED TO PREVAIL OVER THE PERIMETER DESCRIPTION.

R/W COURSE DATA		
COURSE	BEARING	DISTANCE
A	N 00° 26' 57" E	15.05'
B	S 89° 33' 03" E	20.00'
C	S 89° 33' 03" E	30.00'
D	S 00° 26' 57" W	16.00'
E	S 88° 09' 31" W	25.02'
F	N 86° 03' 41" W	32.06'
G	S 89° 33' 03" E	18.00'
H	S 89° 33' E	17'±



TOTAL NET LENGTH OF CENTERLINE = 0.047 MILES

PLAT PREPARED BY

MSA MID-STATE ASSOCIATES, INC.
1230 S Blvd., Baraboo, WI 53913

APPROVED FOR: COUNTY OF LA CROSSE

10-22-86 DATE *Karen Brunkson* COMMISSIONER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

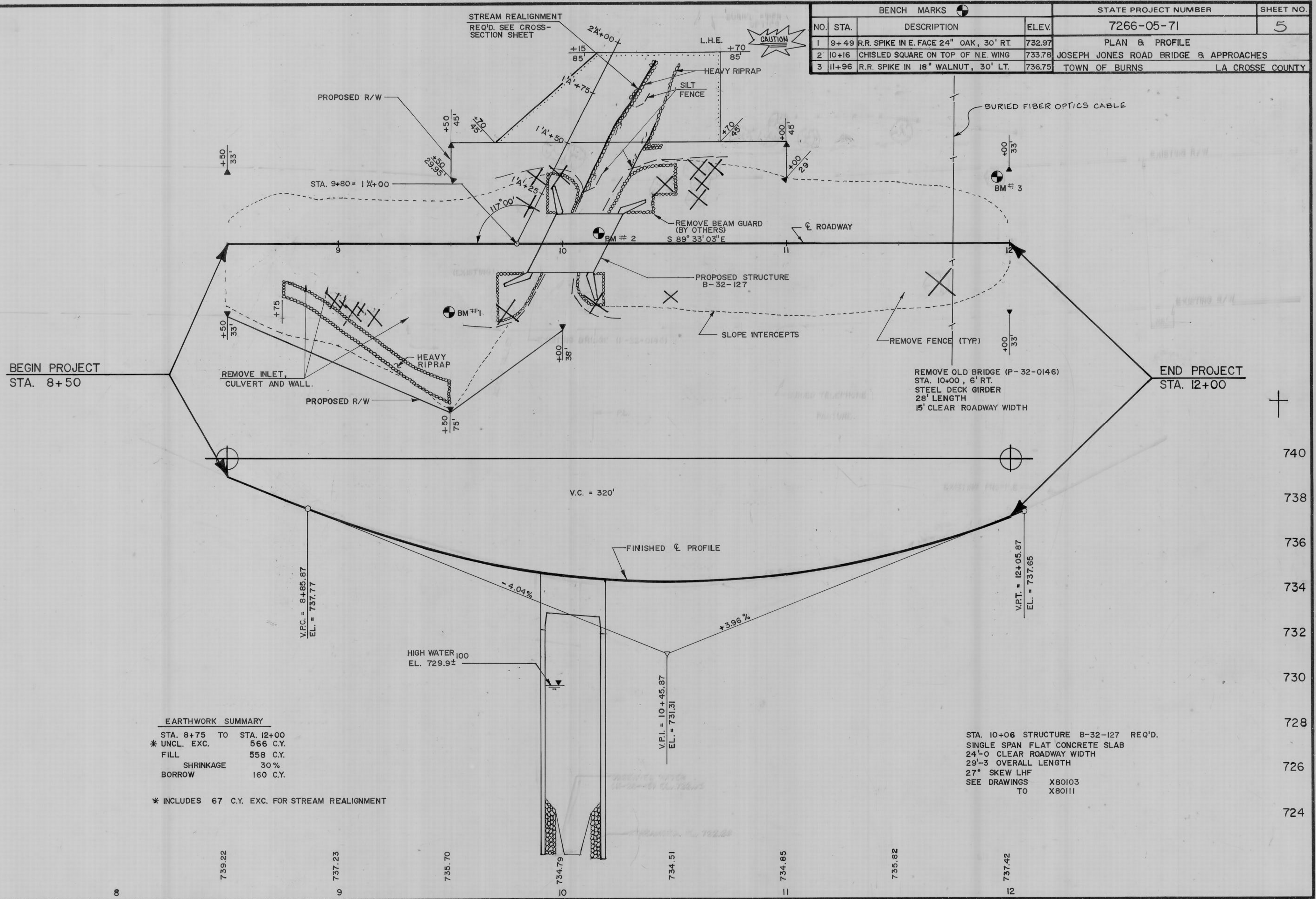
Approved: _____
Date _____ District Transportation Director

Approved: _____
Date _____ Director Bureau of Real Estate

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 5 WISCONSIN DIVISION

Approved: _____
Date _____ Division Administrator

BENCH MARKS			STATE PROJECT NUMBER	SHEET NO.
NO.	STA.	DESCRIPTION	7266-05-71	5
1	9+49	R.R. SPIKE IN E. FACE 24" OAK, 30' RT.	PLAN & PROFILE JOSEPH JONES ROAD BRIDGE & APPROACHES TOWN OF BURNS LA CROSSE COUNTY	
2	10+16	CHISELED SQUARE ON TOP OF N.E. WING		
3	11+96	R.R. SPIKE IN 18" WALNUT, 30' LT.		



BEGIN PROJECT
STA. 8+50

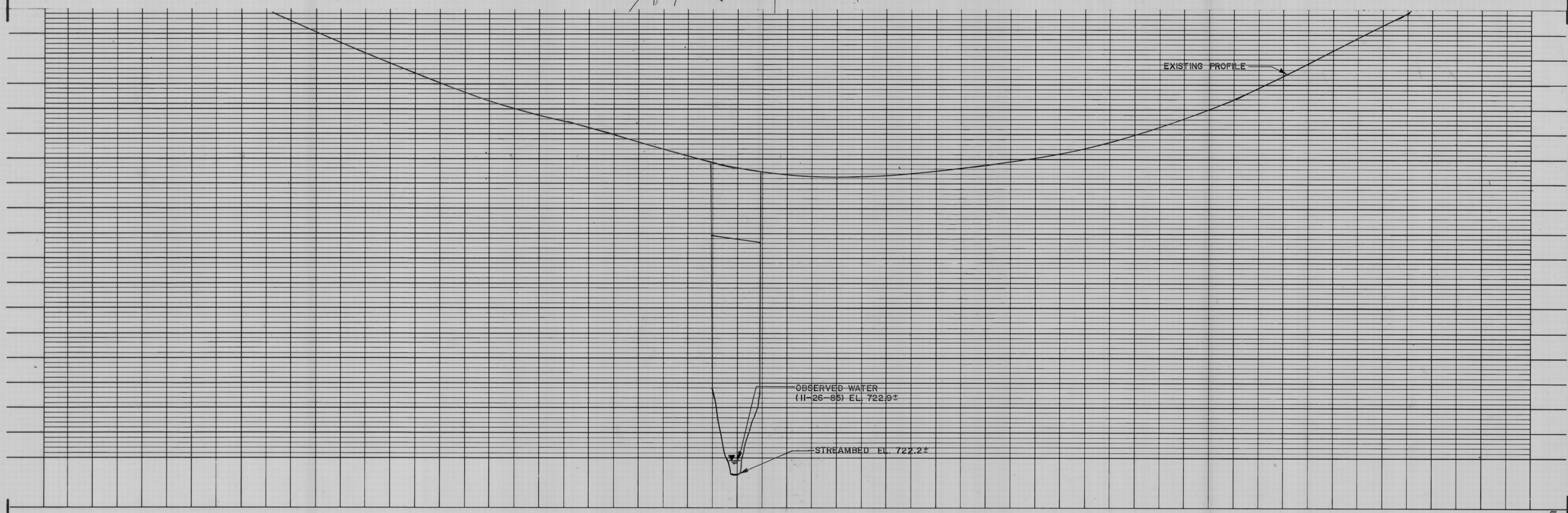
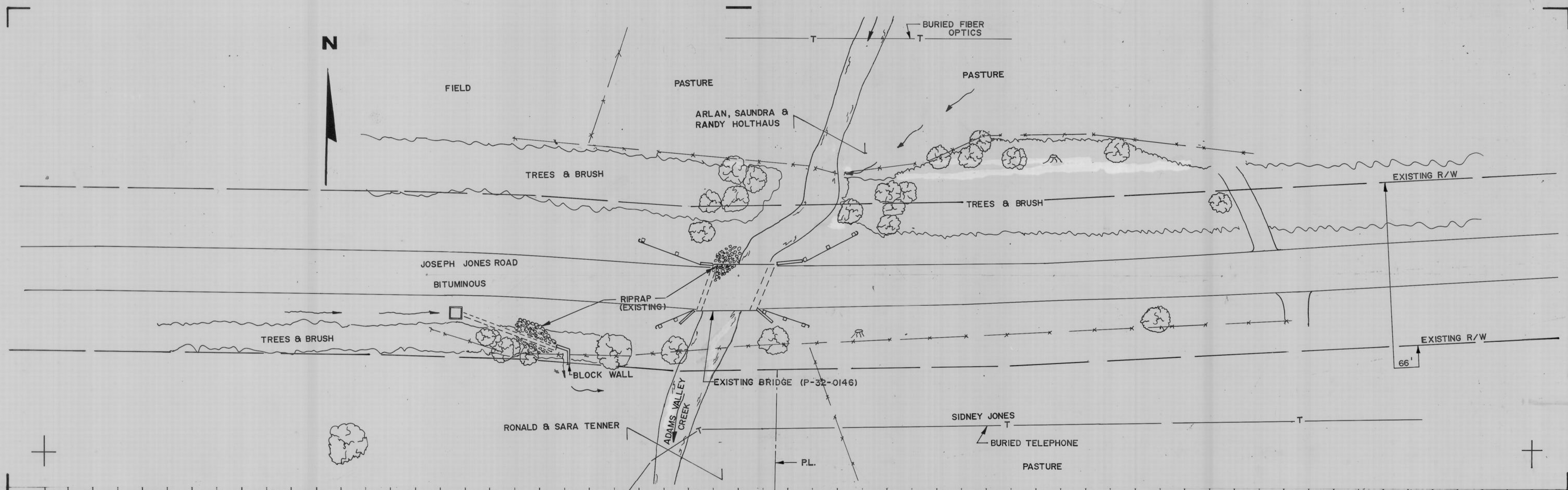
END PROJECT
STA. 12+00

EARTHWORK SUMMARY

STA. 8+75 TO	STA. 12+00	
* UNCL. EXC.	566 C.Y.	
FILL	558 C.Y.	
SHRINKAGE	30%	
BORROW	160 C.Y.	

* INCLUDES 67 C.Y. EXC. FOR STREAM REALIGNMENT

STA. 10+06 STRUCTURE B-32-127 REQ'D.
SINGLE SPAN FLAT CONCRETE SLAB
24'-0" CLEAR ROADWAY WIDTH
29'-3" OVERALL LENGTH
27° SKEW LHF
SEE DRAWINGS
TO X80103
X80111



STRUCTURE B-32-127

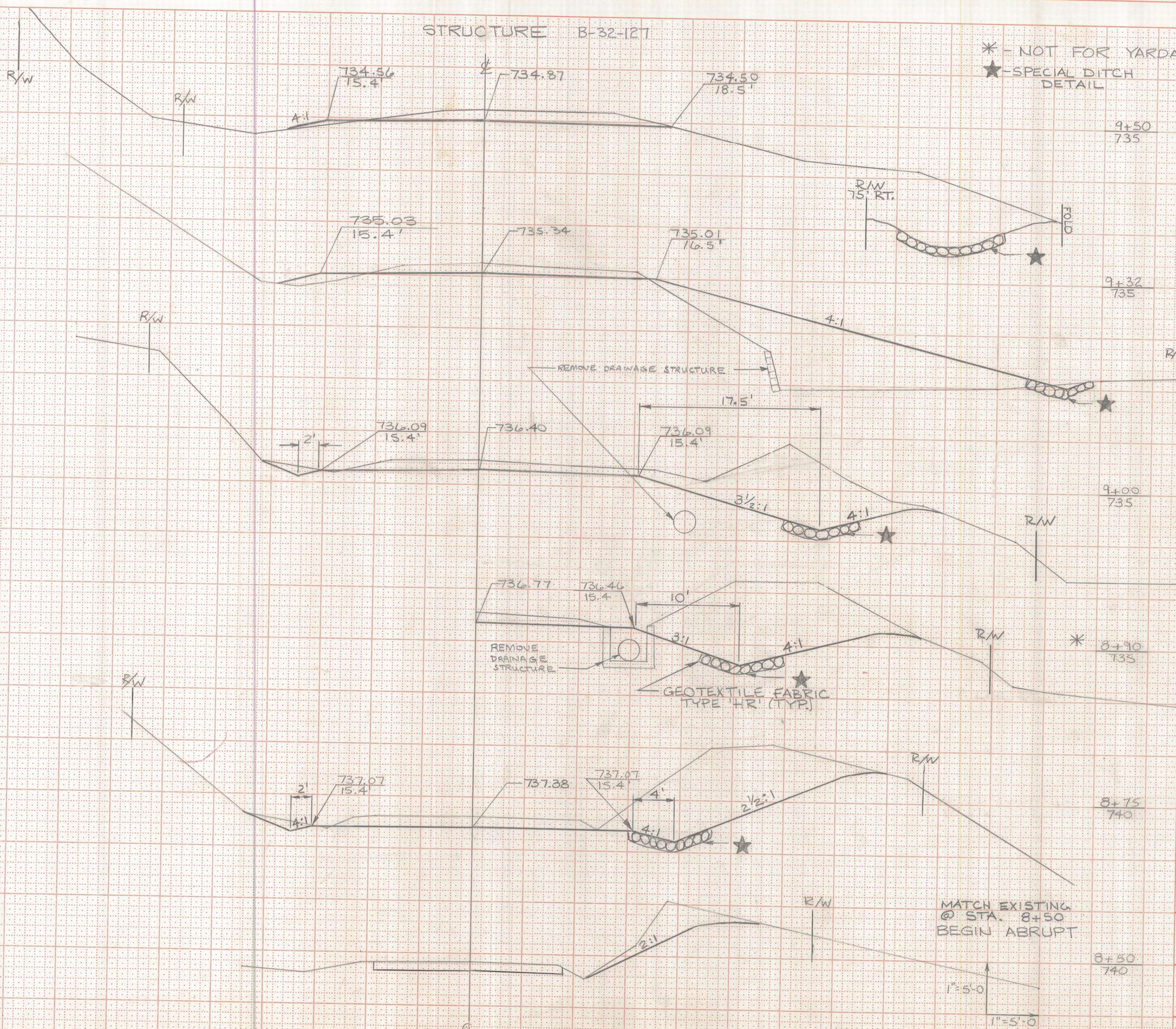
* - NOT FOR YARDAGE
 * - SPECIAL DITCH
 DETAIL

STATE PROJECT NUMBER

7266-05-71

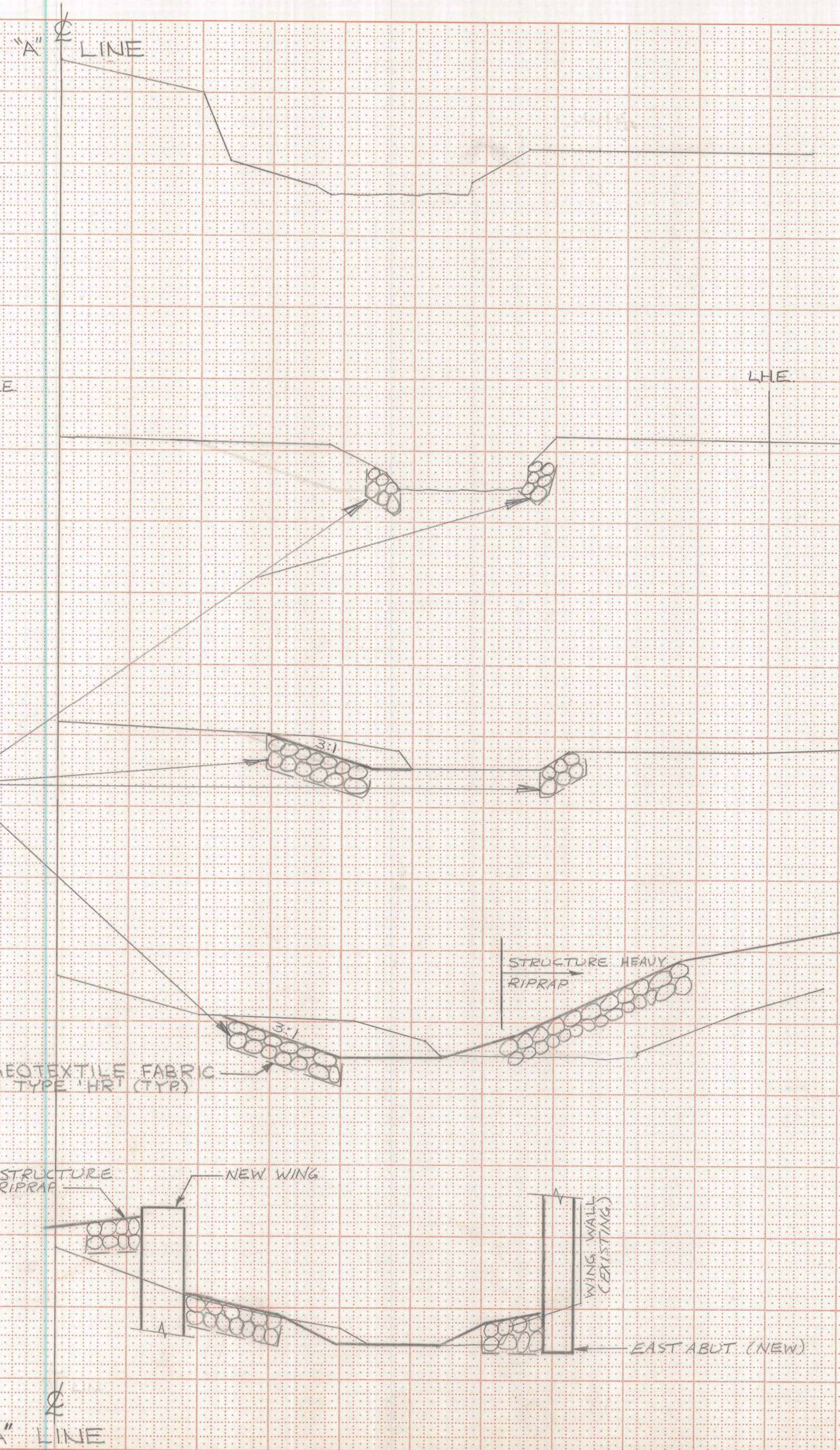
SHEET NUMBER

9



STATION	DISTANCE	YARDAGE	
		EXCAVATION	FILL
8+50	735		
8+50	75	93	1
8+75	735	137	1
9+00	735	97	79
9+32	735	23	45
9+50	735	39	85
STRUCTURE			
TOTAL			
		389	211

SHEET TOTAL 389 211



MATCH CHANNEL AT STA. 2A+00

STATION	DISTANCE	YARDAGE	
		UNCL.	FILL
1A+25	▲		
1A+50	10	18	0
1A+75	25	32	0
2A+00	25	17	0
SHEET TOTAL			
		67	0

▲ - BEGIN 'A' LINE QUANTITIES @ STA 1A+40

1"=5'-0"
1"=5'-0"

PROPOSED R/W

TELETYPE POST

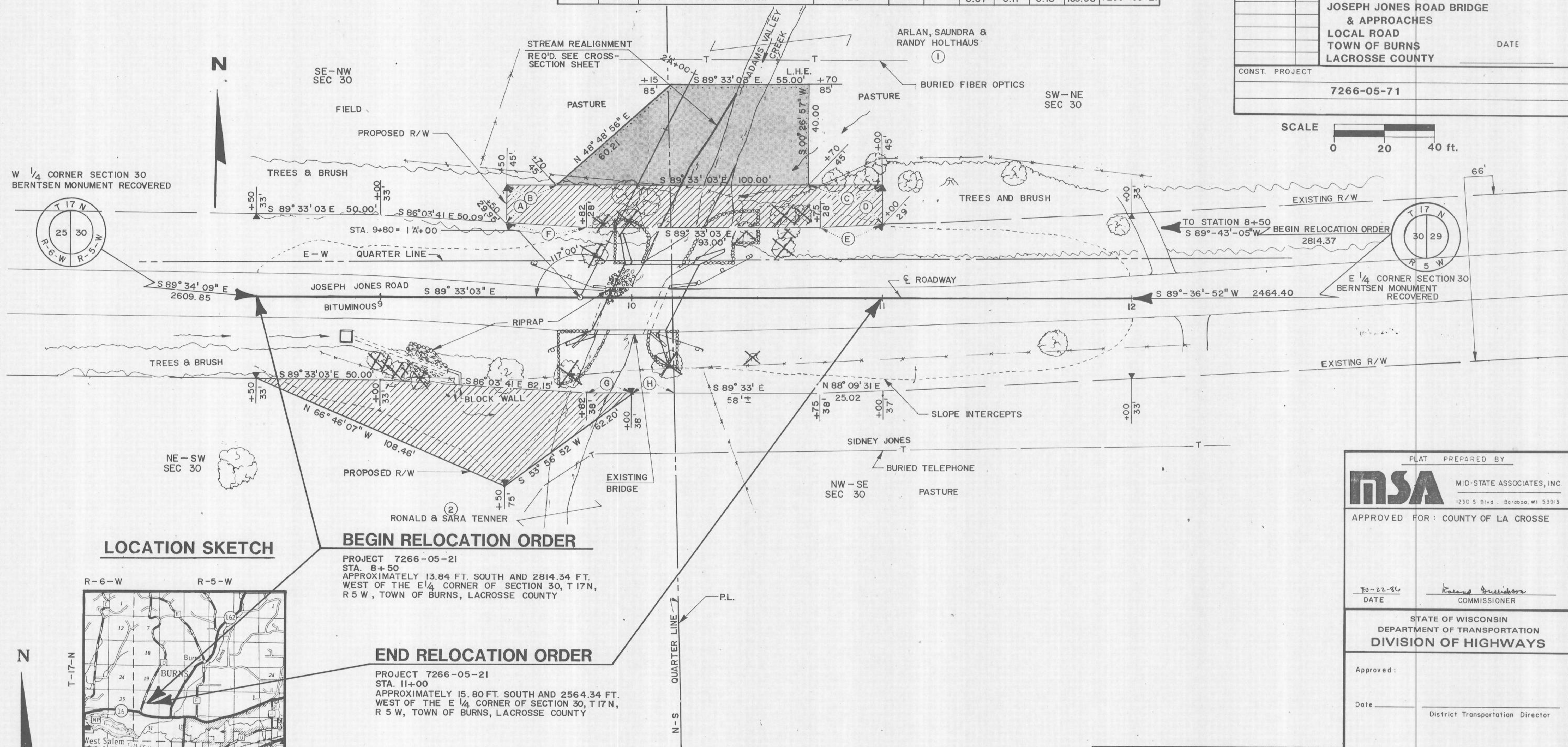
BEARING ORIENTATION

RIGHT-OF-WAY PLAT BEARINGS ARE ORIENTED TO THE EAST-WEST QUARTER LINE OF SECTION 30, T17N, R5W, LACROSSE COUNTY, WHICH HAS AN ASSUMED BEARING OF S 89° 56' 20" E. THE DIFFERENCE BETWEEN PROJECT PLAT BEARING REPRESENTS PLANE ANGLES IN DEGREES, MINUTES, AND SECONDS TO THE NEAREST SECOND.

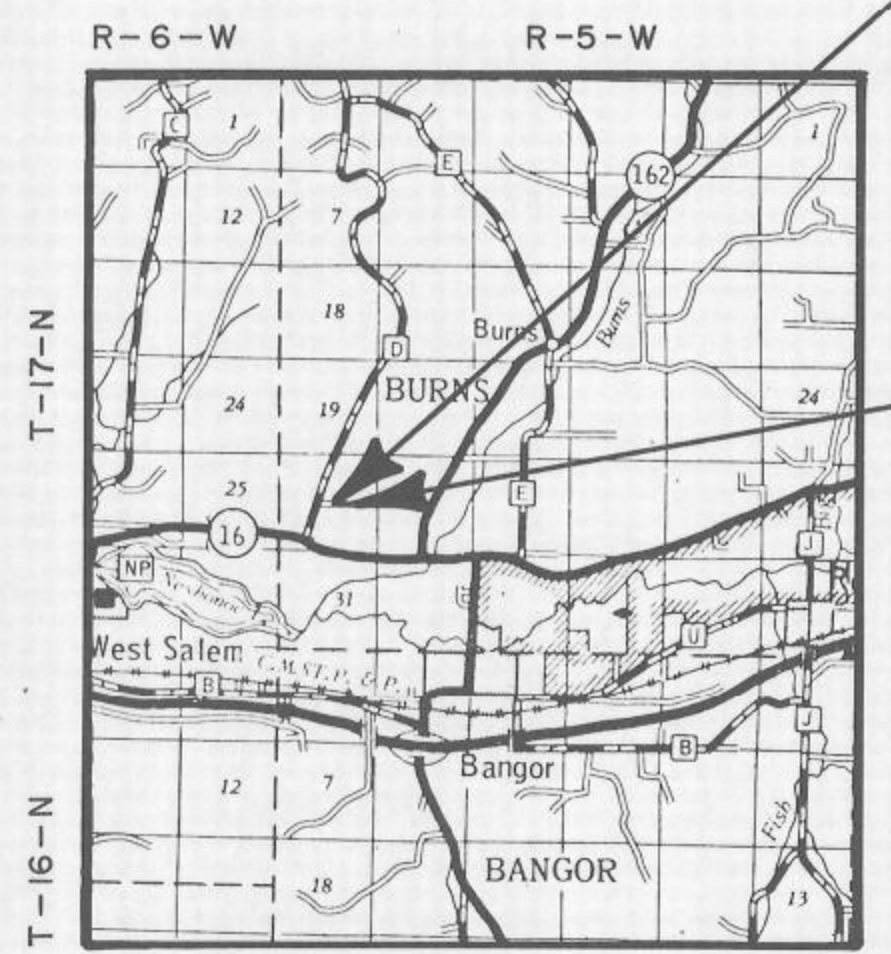
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED	L.H.E. ACRES		R/W ACRES REQ'D.			TOTAL ACRES REM.	OPERATIONS PROJECT I.D.
				TEMP.	PERM.	NEW	EXISTING	TOTAL		
1	4.0	ARLAN, SAUNDRA & RANDY HOLTHAUS	FEE & L.H.E.	0.07		0.06	0.11	0.17	327.71	7266-05-21
2	4.0	RONALD & SARA TENNER	FEE			0.07	0.11	0.18	159.98	7266-05-21

REVISION DATE	R/W PROJECT NUMBER 7266-05-21	SHEET NUMBER	TOTAL SHEETS
	FEDERAL PROJECT NUMBER		
PLAT OF RIGHT OF WAY REQUIRED FOR			
JOSEPH JONES ROAD BRIDGE & APPROACHES			
LOCAL ROAD TOWN OF BURNS LACROSSE COUNTY			
DATE			
CONST. PROJECT 7266-05-71			



LOCATION SKETCH



BEGIN RELOCATION ORDER

PROJECT 7266-05-21
STA. 8+50
APPROXIMATELY 13.84 FT. SOUTH AND 2814.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

END RELOCATION ORDER

PROJECT 7266-05-21
STA. 11+00
APPROXIMATELY 15.80 FT. SOUTH AND 2564.34 FT. WEST OF THE E 1/4 CORNER OF SECTION 30, T17N, R5W, TOWN OF BURNS, LACROSSE COUNTY

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD. OTHER INFORMATION IS PROVIDED TO SUPPLEMENT THE BASIC PERIMETER DESCRIPTION AND SHALL NOT BE CONSTRUED TO PREVAIL OVER THE PERIMETER DESCRIPTION.

R/W COURSE DATA		
COURSE	BEARING	DISTANCE
A	N 00° 26' 57" E	15.05'
B	S 89° 33' 03" E	20.00'
C	S 89° 33' 03" E	30.00'
D	S 00° 26' 57" W	16.00'
E	S 88° 09' 31" W	25.02'
F	N 86° 03' 41" W	32.06'
G	S 89° 33' 03" E	18.00'
H	S 89° 33' E	17'±

PLAT PREPARED BY
MSA MID-STATE ASSOCIATES, INC.
1230 S Blvd., Baraboo, WI 53913

APPROVED FOR: COUNTY OF LA CROSSE

DATE: 7-22-80
COMMISSIONER: *Kenneth Brueckner*

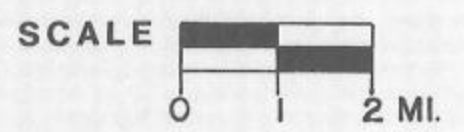
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: _____
Date: _____
District Transportation Director

Approved: _____
Date: _____
Director Bureau of Real Estate

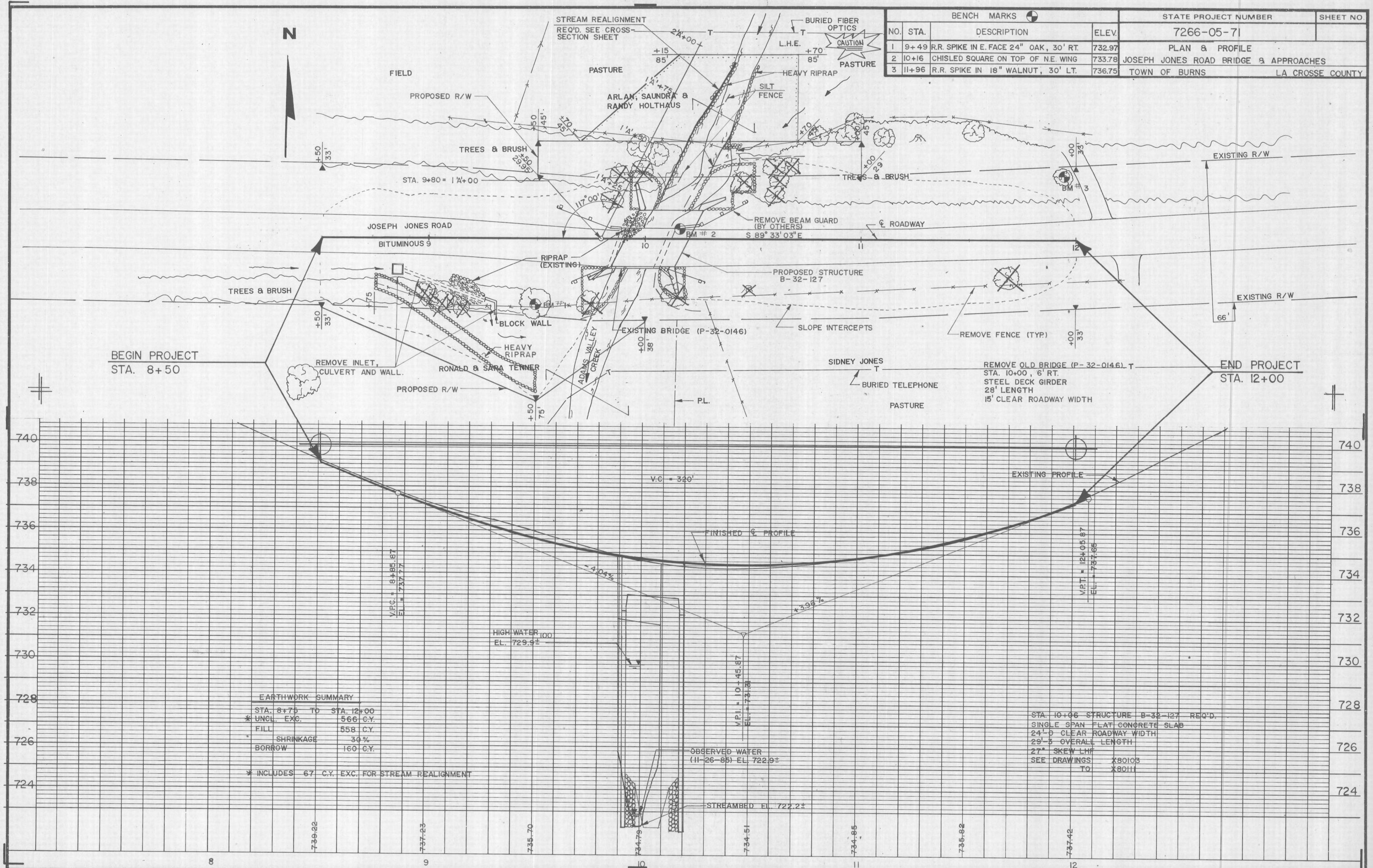
U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION 5 WISCONSIN DIVISION

Approved: _____
Date: _____
Division Administrator



TOTAL NET LENGTH OF CENTERLINE = 0.047 MILES

BENCH MARKS			STATE PROJECT NUMBER	SHEET NO.
NO.	STA.	DESCRIPTION	ELEV.	
1	9+49	R.R. SPIKE IN E. FACE 24" OAK, 30' RT.	732.97	
2	10+16	CHISLED SQUARE ON TOP OF N.E. WING	733.78	
3	11+96	R.R. SPIKE IN 18" WALNUT, 30' LT.	736.75	
			7266-05-71	
			PLAN & PROFILE	
			JOSEPH JONES ROAD BRIDGE & APPROACHES	
			TOWN OF BURNS	LA CROSSE COUNTY



BEGIN PROJECT
STA. 8+50

END PROJECT
STA. 12+00

EARTHWORK SUMMARY

STA. 8+75 TO STA. 12+00	
* UNCL. EXC.	566 C.Y.
FILL	558 C.Y.
SHRINKAGE	39%
BORROW	160 C.Y.
* INCLUDES 67 C.Y. EXC. FOR STREAM REALIGNMENT	

STA. 10+06 STRUCTURE B-32-127 REQ'D.
SINGLE SPAN FLAT CONCRETE SLAB
24'-0" CLEAR ROADWAY WIDTH
29'-3" OVERALL LENGTH
27" SKEW LHM
SEE DRAWINGS X80103
TO X80111

BENCH MARKS			STATE PROJECT NUMBER	SHEET NO.
NO.	STA.	DESCRIPTION	ELEV.	
1	9+49	R.R. SPIKE IN E. FACE 24" OAK, 30' RT.	732.97	
2	10+16	CHISELED SQUARE ON TOP OF N.E. WING	733.78	
3	11+96	R.R. SPIKE IN 18" WALNUT, 30' LT.	736.75	
			7266-05-71	

DESIGN DATA

LIVELOAD:
 DESIGN RATING: HS-20
 INVENTORY RATING: HS 24
 OPERATIONAL RATING: HS 39
 STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

ALLOWABLE DESIGN STRESSES:
 CONCRETE MASONRY - SLAB $f_c' = 4000$ P.S.I.
 - ALL OTHER $f_c' = 3500$ P.S.I.
 HIGH STRENGTH AND COATED HIGH STRENGTH BAR STEEL REINFORCEMENT, GRADE 60 $f_y = 60000$ P.S.I.

FOUNDATION DATA:
 ABUTMENTS SHALL BE SUPPORTED ON HP10x42 STEEL PILING DRIVEN TO A MINIMUM BEARING VALUE OF 55 TONS PER PILE. ESTIMATED 75'-0" PILE LENGTHS WEST ABUT., 90'-0" PILE LENGTHS EAST ABUT.

HYDRAULIC DATA:
100 YEAR FREQUENCY
 DRAINAGE AREA 4.9 SQ. MI.
 Q_{100} 900 C.F.S.
 VELOCITY 6.6 F.P.S.
 WATERWAY AREA 137 SQ. FT.
 HIGH WATER 100 ELEVATION 729.9 ±
 ROADWAY OVERFLOW DESIGN FREQUENCY N/A

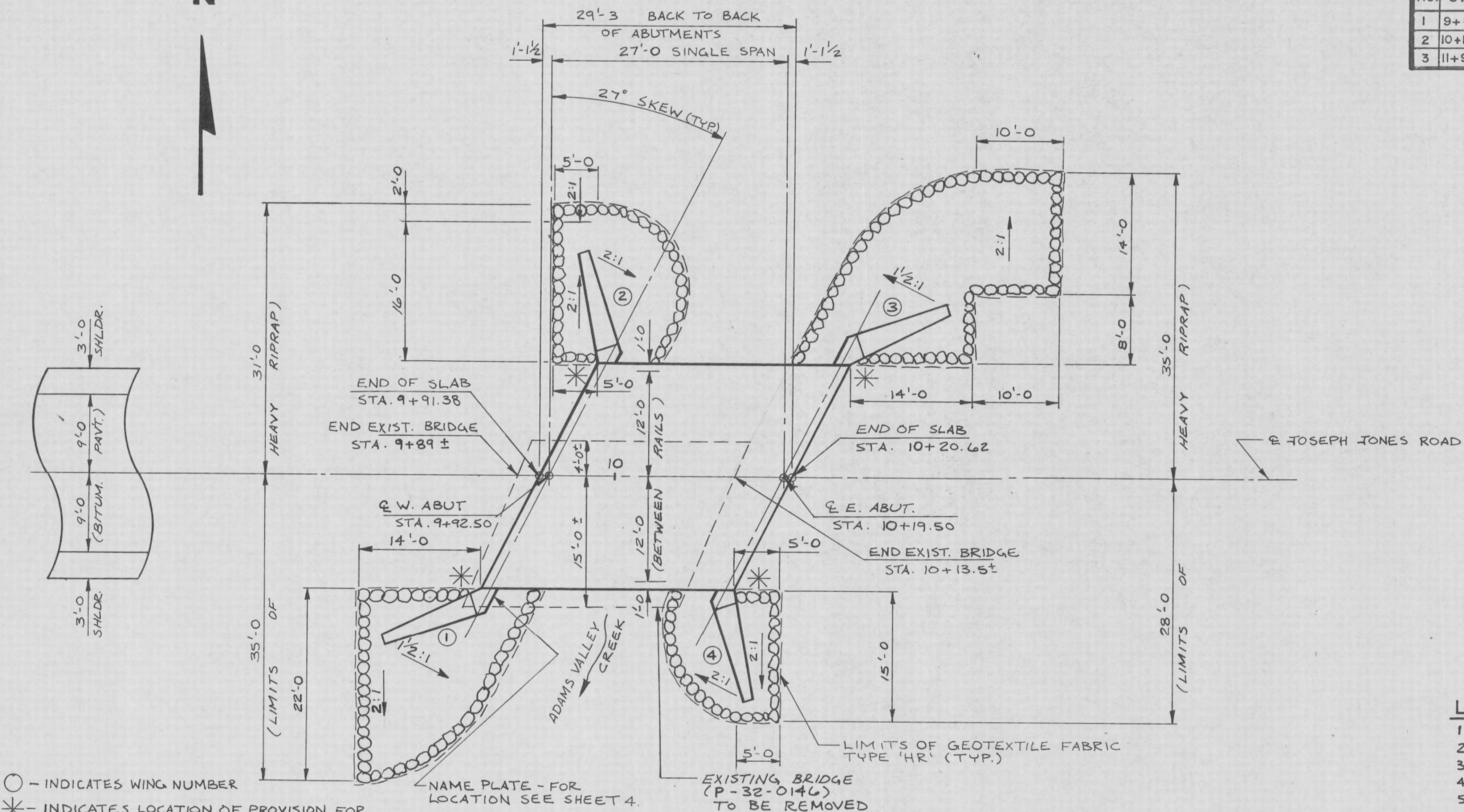
TRAFFIC DATA:
 A.D.T (1986) 40
 A.D.T (2006) 80
 DESIGN SPEED 30 M.P.H.

LIST OF DRAWINGS

- 1. GENERAL PLAN X80103
- 2. CROSS SECTION AND QUANTITIES X80104
- 3. SUBSURFACE EXPLORATION X80105
- 4. WEST ABUTMENT X80106
- 5. WEST ABUTMENT DETAILS X80107
- 6. EAST ABUTMENT X80108
- 7. EAST ABUTMENT DETAILS X80109
- 8. SUPERSTRUCTURE X80110
- 9. TUBULAR STEEL RAILING, TYPE "F" X80111

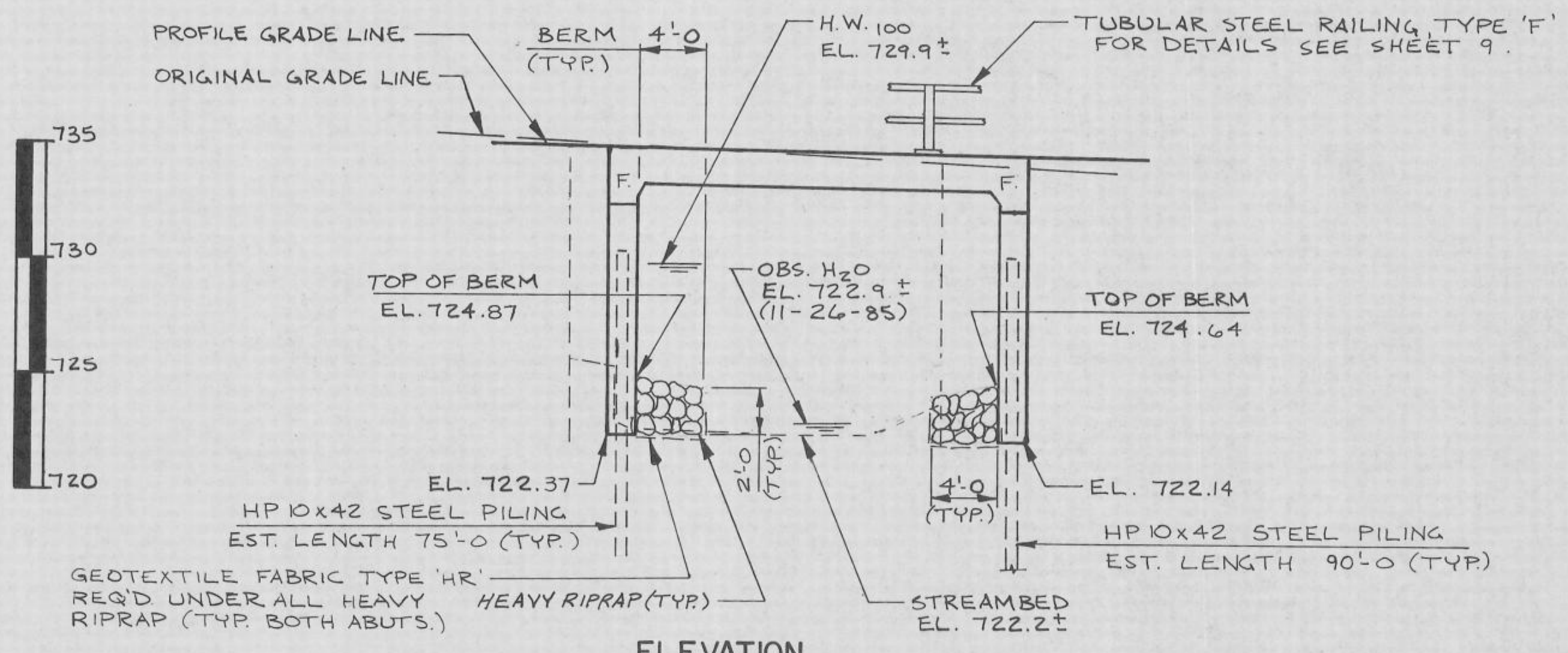


N



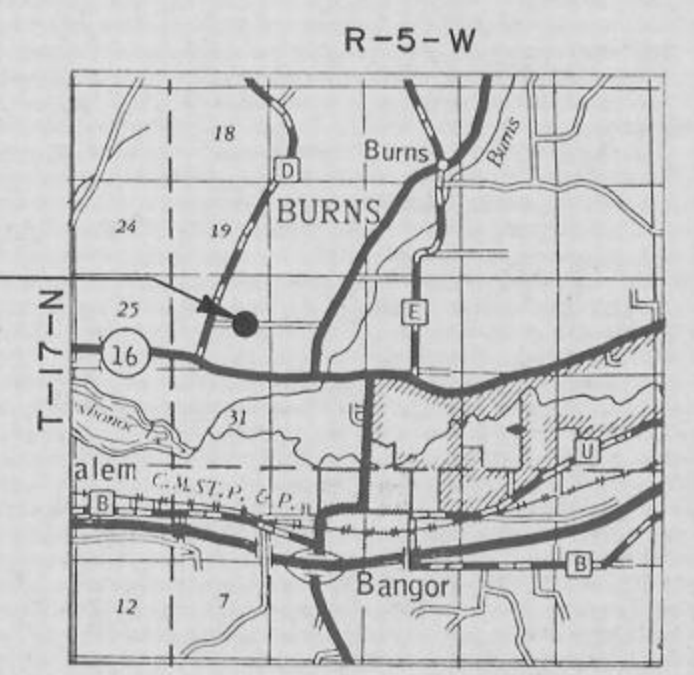
○ - INDICATES WING NUMBER
 * - INDICATES LOCATION OF PROVISION FOR FUTURE BEAM GUARD ATTACHMENT

PLAN
 (SINGLE SPAN - CONCRETE SLAB)



ELEVATION
 (NORMAL TO ADAMS VALLEY CREEK)

PROJECT LOCATION
 B-32-127



LAYOUT

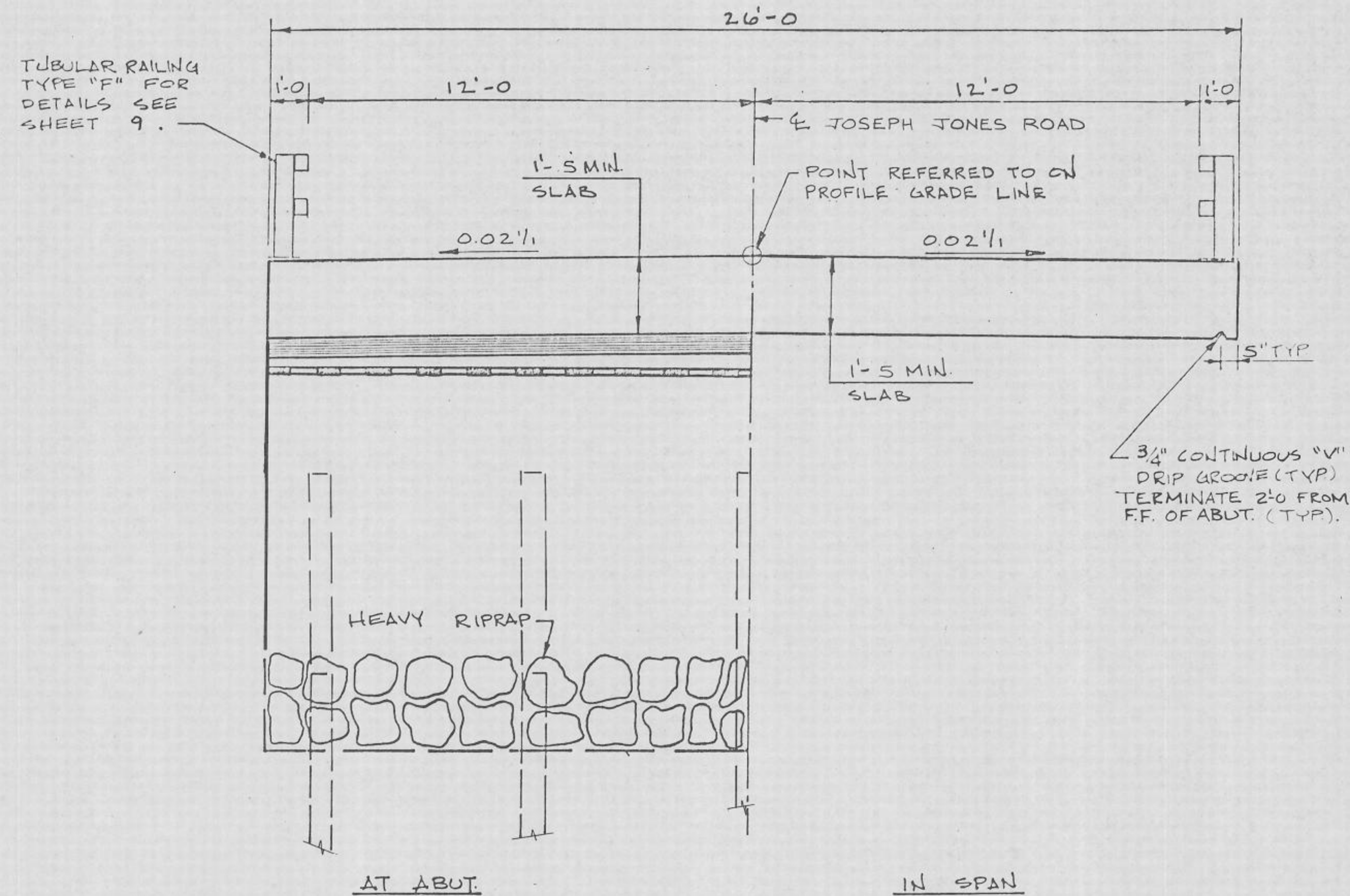
No.	Date	Revision	By
PLANS PREPARED BY			
MSA MID-STATE ASSOCIATES, INC. 1230 S Blvd., Baraboo, WI 53913			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127		JOSEPH JONES ROAD OVER ADAMS VALLEY CREEK	
County	LA CROSSE	Town/City/Village	BURNS
Design Spec.	AASHTO 1984	Load	HS-20
Designed By	MSR	Drawn By	RLR
Checked	RJR	Checked	MSR
Approved	State Bridge Engineer	Date	
GENERAL PLAN			SHEET 1 OF 9
			X80103

TOTAL ESTIMATED QUANTITIES

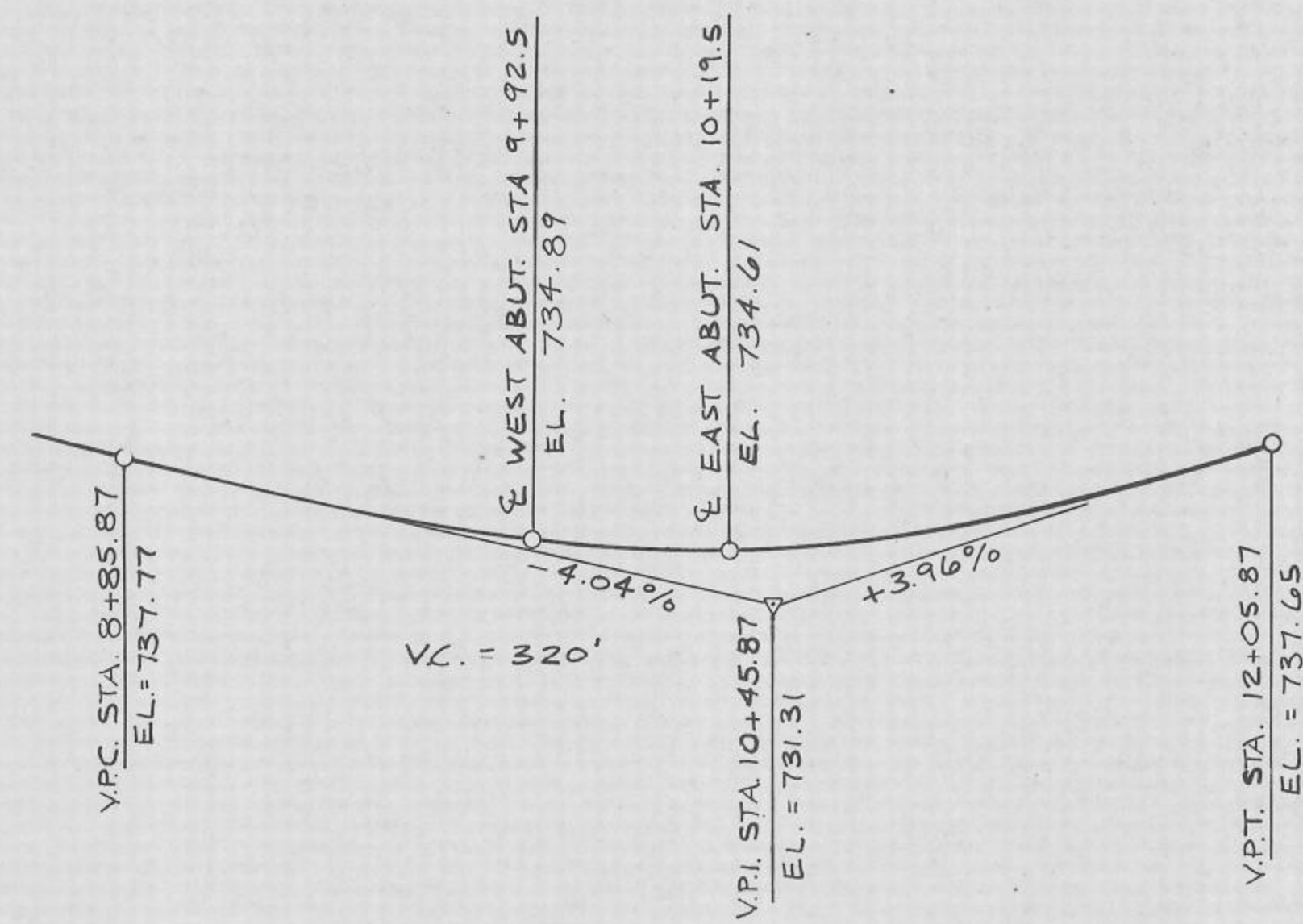
BID ITEMS	UNIT	WEST ABUT.	EAST ABUT.	SUPER	TOTAL
REMOVING OLD BRIDGE, STATION 10+00, 6' RT.	L.S.	---	---	---	1
EXCAVATION FOR STRUCTURES, BRIDGES B-32-127	L.S.	---	---	---	1
CONCRETE MASONRY, BRIDGES	C.Y.	43.9	43.7	44.4	132
PROTECTIVE SURFACE TREATMENT	GAL.	---	---	4	4
HIGH STRENGTH BAR STEEL REINFORCEMENT, BRIDGES	LB.	3350	3350	4870	11570
COATED HIGH STRENGTH BAR STEEL REINFORCEMENT	LB.	---	---	1170	1170
STEEL PILING DELIVERED & DRIVEN, HP 10 INCH, 42 POUND	LF.	525	630	---	1155
TUBULAR RAILING, TYPE 'F', STRUCTURE B-32-127	L.S.	---	---	---	1
HEAVY RIPRAP	C.Y.	60	65	---	125
GEOTEXTILE FABRIC, TYPE 'HR'	S.Y.	120	130	---	250
NON-BID ITEMS					
FILLER	SIZE	---	---	---	1/2" 3/4"
POLYVINYL CHLORIDE WATERSTOP	LF.	34	34	---	68

GENERAL NOTES

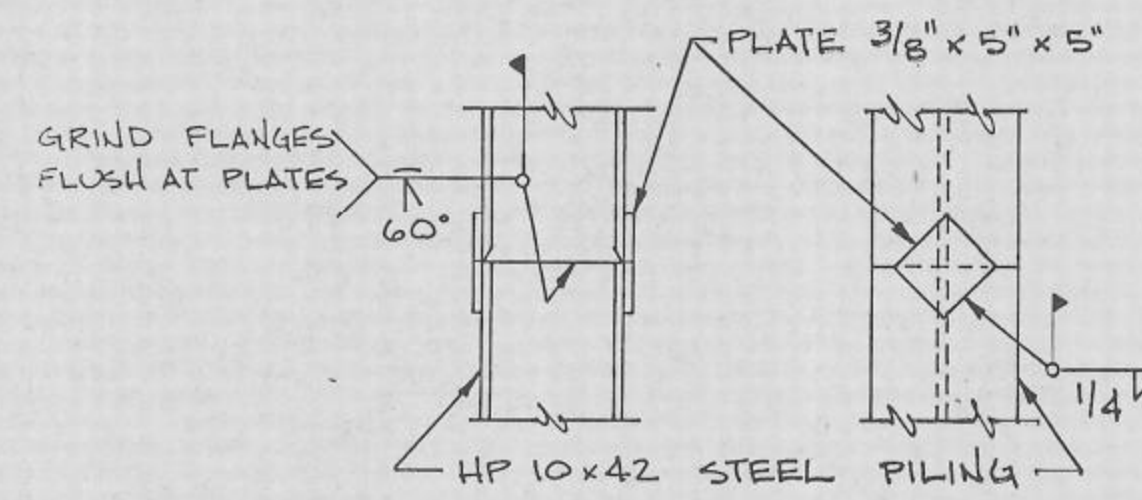
- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE. THE FIRST DIGIT OF A THREE DIGIT BAR MARK SIGNIFIES THE BAR SIZE.
- FILLER SHALL CONFORM TO AASHTO DESIGNATION M153, TYPE I, II OR III OR AASHTO DESIGNATION M213.
- SLAB FALSEWORK SHALL BE SUPPORTED ON PILES UNLESS ALTERNATE METHOD IS APPROVED BY THE ENGINEER.
- THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP TO THE LIMITS SHOWN ON SHEET 1 AND ON THE ABUTMENT SHEETS OR AS DIRECTED BY THE ENGINEER.
- THE EXISTING GROUNDLINE SHALL BE THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES" FOR THE ABUTMENTS.
- BACKFILL 2'-0" ABOVE THE BOTTOM OF ABUTMENT ELEVATIONS SHALL NOT BE PLACED UNTIL THE SUPERSTRUCTURE IS IN PLACE.
- THIS STRUCTURE WILL REPLACE EXISTING BRIDGE P-32-0146, A 28FT. LONG SINGLE SPAN STEEL DECK GIRDER.



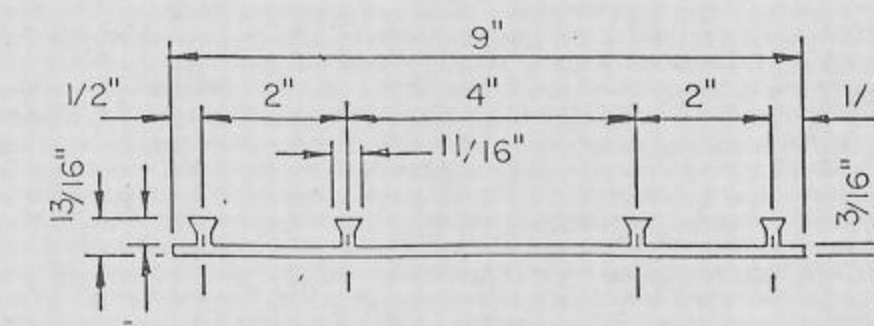
CROSS SECTION THRU BRIDGE
(LOOKING EAST)



PROFILE GRADE LINE - JOSEPH JONES ROAD



PILE SPLICE DETAILS



P.C.W.
POLYVINYL CHLORIDE WATERSTOP

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI. "81"	Drawn By	PLS
		Checked	MSR
CROSS SECTION & QUANTITIES			SHEET 2 OF 9 X80104

ABBREVIATIONS
 F — Fine M — Medium C — Coarse
 Ws — Weathered So — Sound

MATERIAL SYMBOLS

Topsoil	Silt	Sandstone
Sand	Peat	Limestone
Gravel	Clay	Igneous Rock

LEGEND OF PROBING

95/6=95 Blows for 6" Penetration
 Probing taken with a 350# wt. Falling 18" on a 2" O. D. Point.

Probing No. Sta. Elevation
 7 Average Blows Per Foot
 Refusal 95, 6

LEGEND OF BORING

Unconfined Strength → 7.7
 Blows Per Ft. Using 140# Wt. Falling 30"
 Wash Sample
 Shelby Tube — S. T.
 Ground Water Elevation
 No Ground Water Observed Above This Elevation

Boring No. Sta. Elev.
 Sandy Gravel
 Boulders or Cobbles
 Sand
 Silty Clay
 So
 Limestone

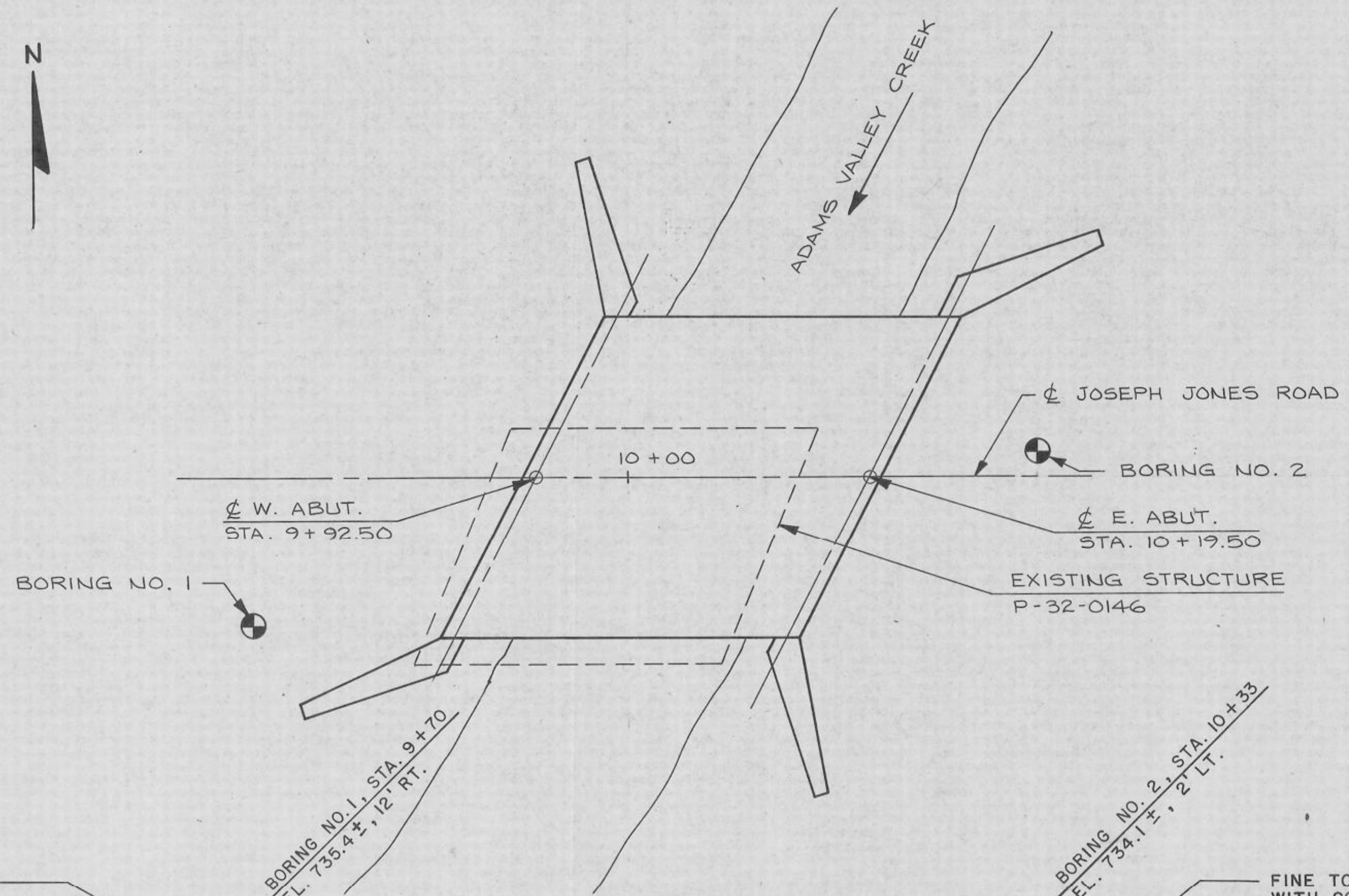
Unless otherwise specified, the blows per foot at the locations indicated are based on driving a 2" O. D. x 1.4" I. D. split spoon sampler with a 140# hammer having a free fall of 30". The blow count is taken in undisturbed soil immediately below a cased or open hole eliminating side friction on the drive pipe.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

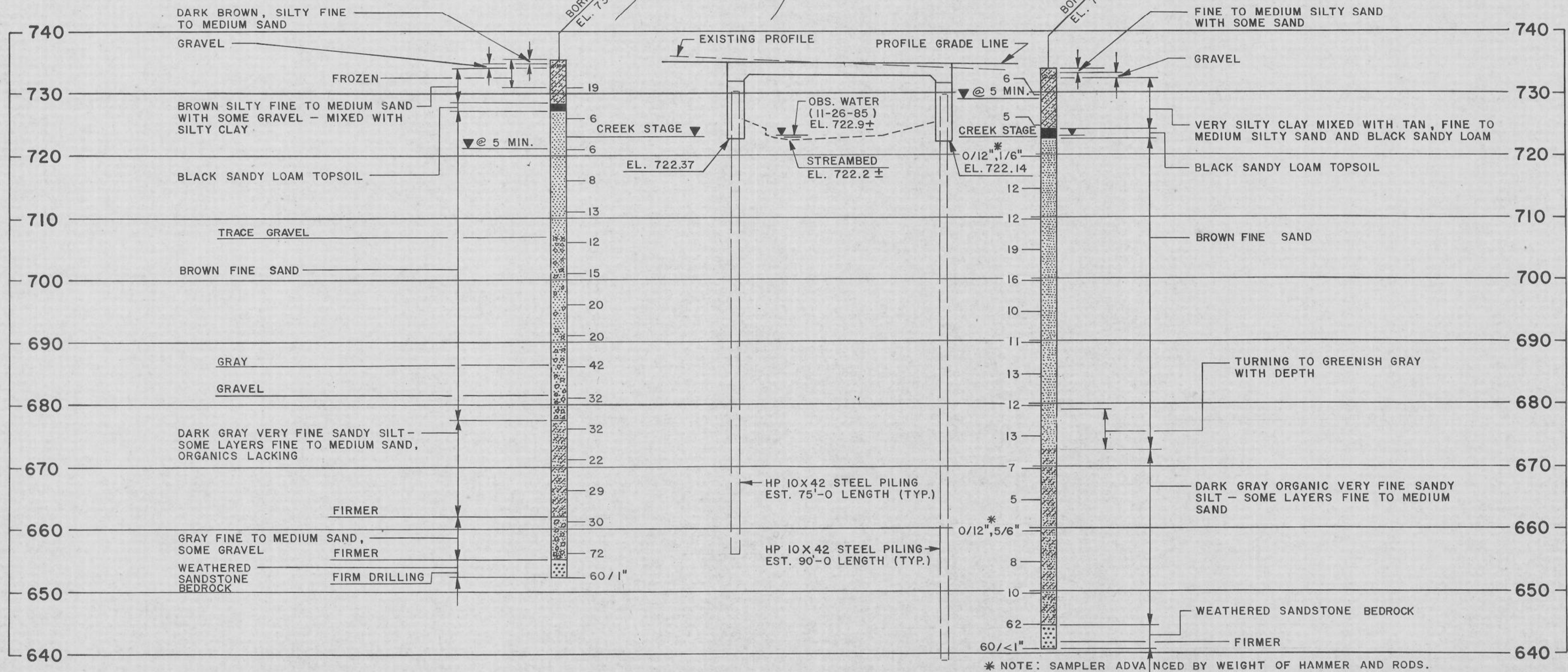
To obtain relative data concerning the character of material in and upon which the foundation might be built, borings and/or soundings were made at points approximately as indicated on this drawing. The data presented herein represents the findings of the subsurface explorations made. However, because the depths investigated are limited and the area of the borings and/or soundings is very small in relation to the entire area, the Division of Highways does not warrant conditions below the depths investigated or that the classification of material encountered in these investigations is necessarily typical of the entire site.

No.	Date.	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI "81"	Drawn By RHM	Plans Checked MSK
SUBSURFACE EXPLORATION			SHEET 3 OF 9
			X80105

SOIL BORINGS PERFORMED
 BY: SOILS & ENGINEERING SERVICE
 MADISON, WISCONSIN
 ON: BORING NO. 1, MARCH 11, 1986
 BORING NO. 2, MARCH 10, 1986



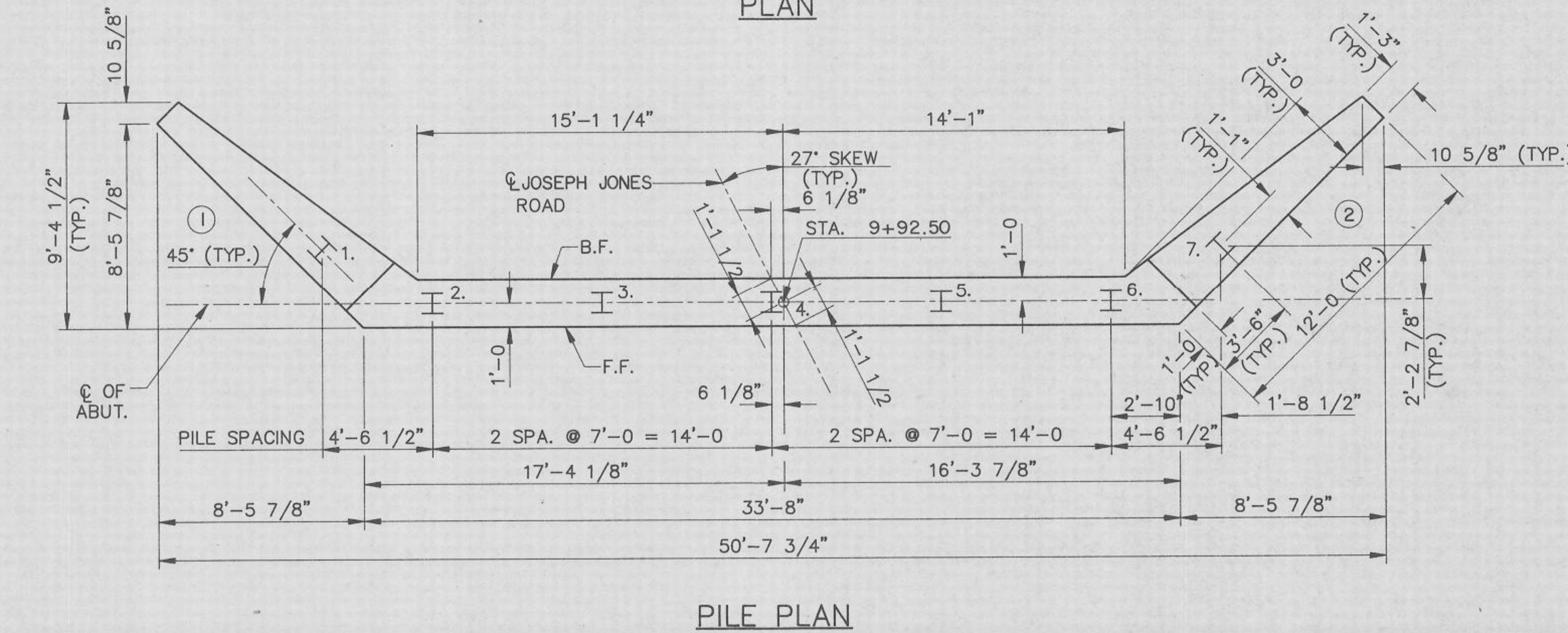
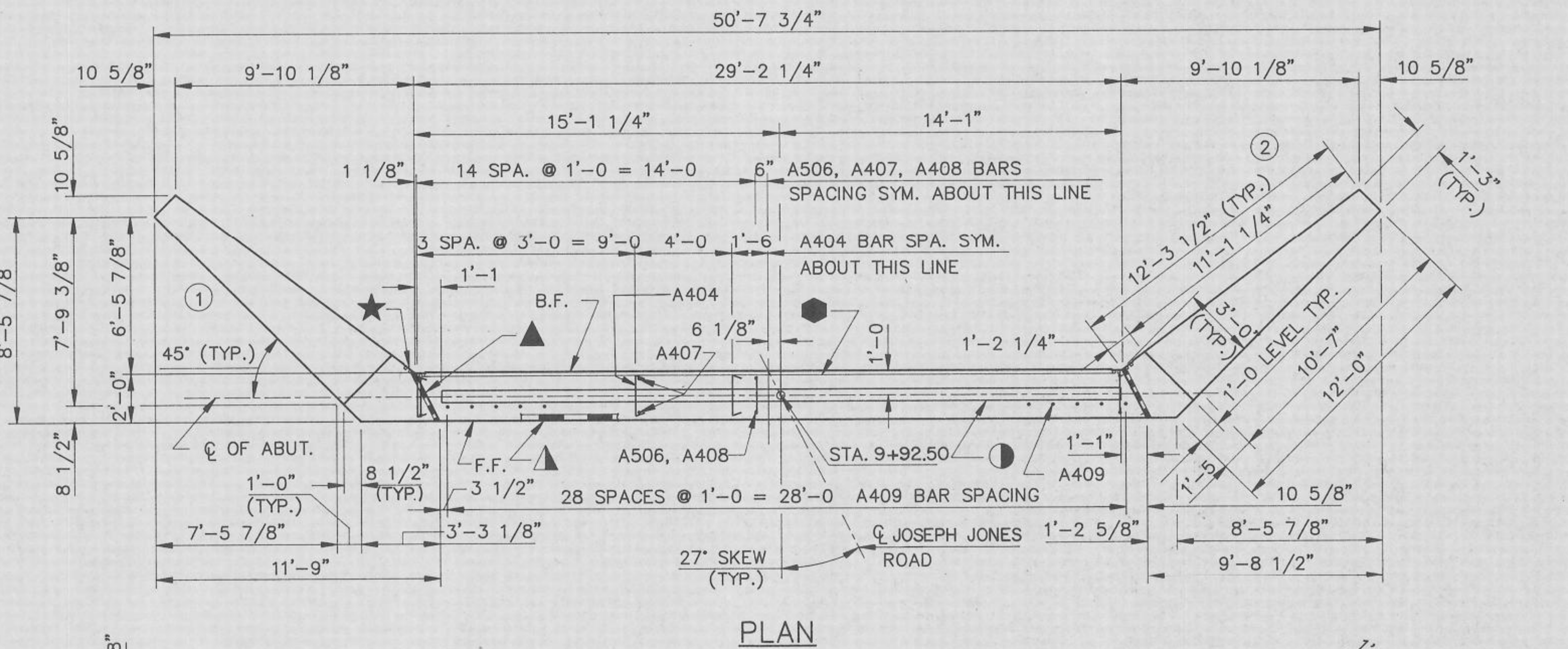
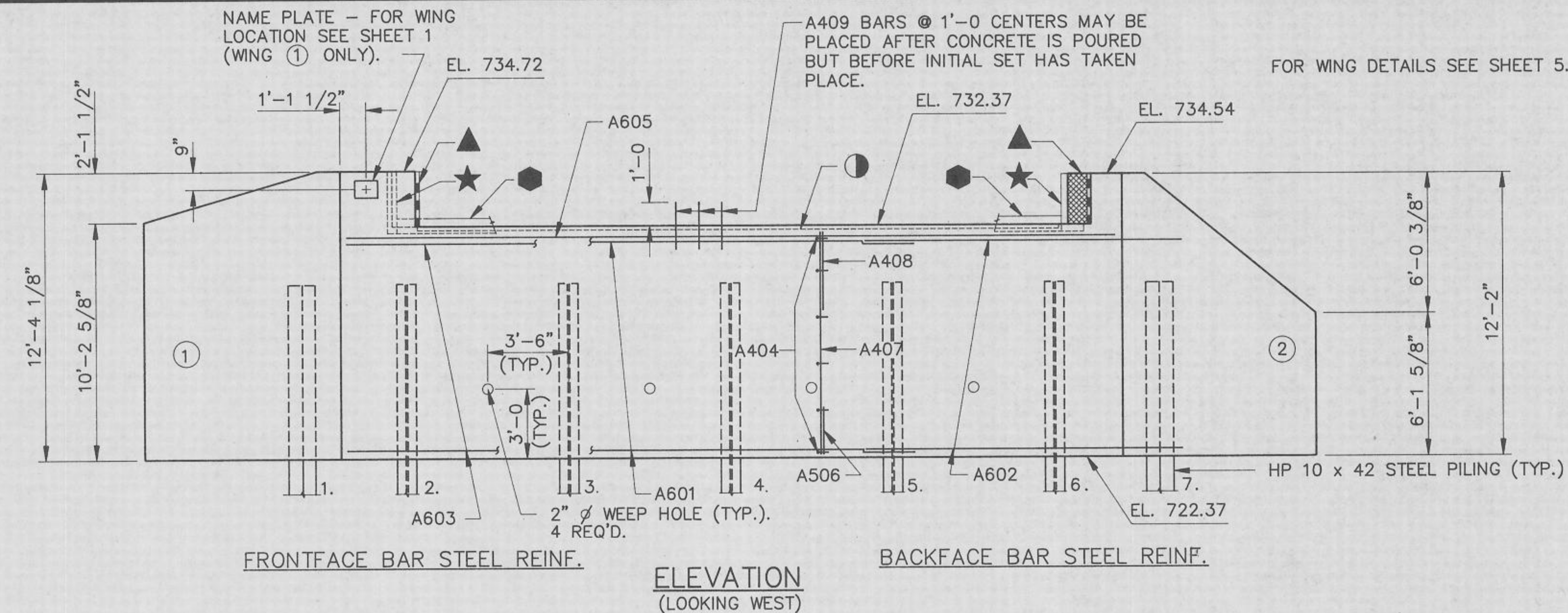
PLANS PREPARED BY:
 MID-STATE ASSOCIATES, INC.
 BARABOO, WISCONSIN



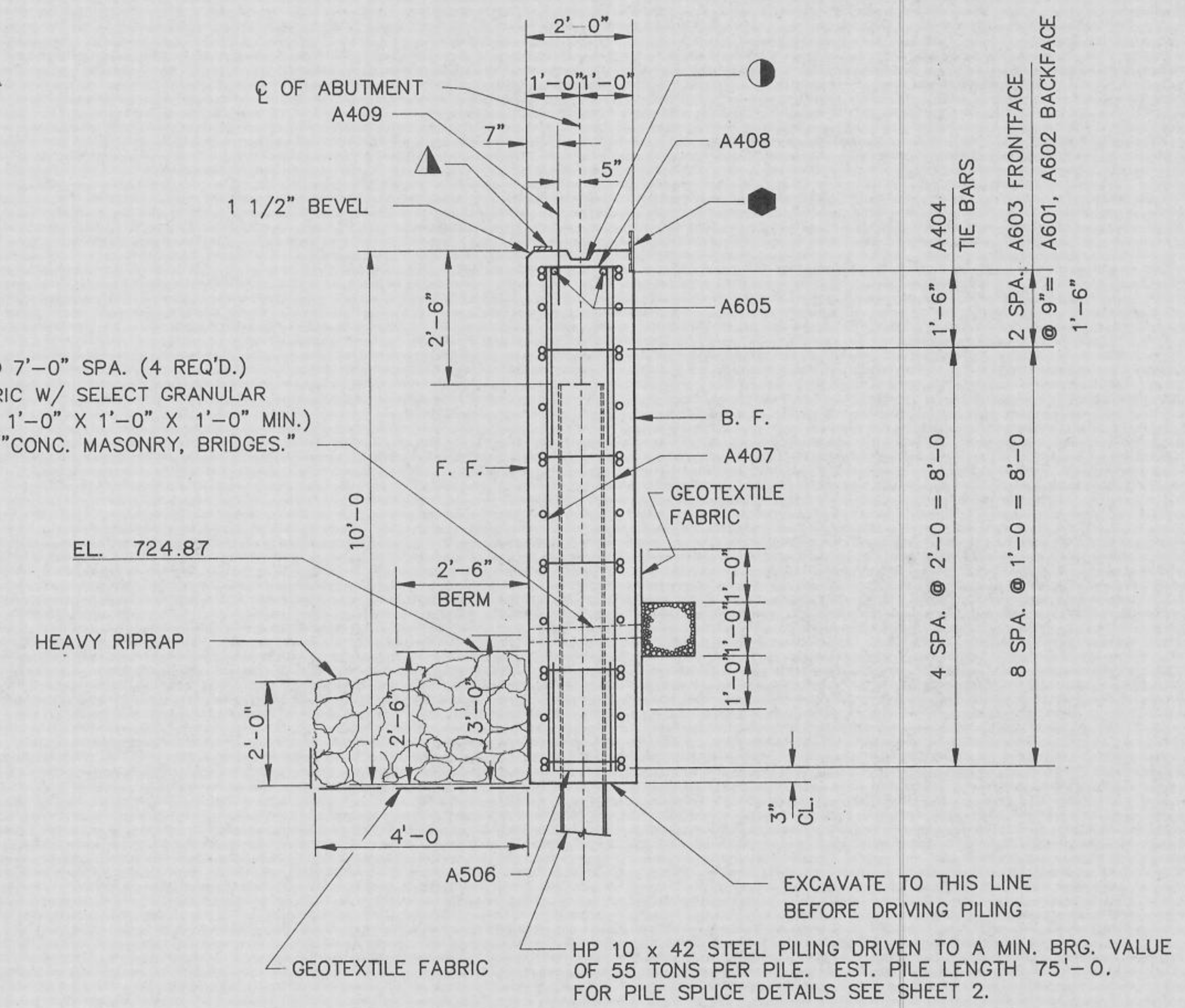
LEGEND

- — KEYED CONSTRUCTION JOINT FORMED BY SURFACED & BEVELED 2 X 6.
- ★ — VERTICAL P.C.W. EXTEND FROM 5" BELOW BRIDGE SEAT TO TOP OF WING. BUTT-SPLICE AT ALL INTERSECTIONS WITH HORIZ. P.C.W. BY USING A HEATED SPLICING IRON. HOLD FLUSH WITH FACE OF CONCRETE. SEAL VERTICAL ENDS OF P.C.W. WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER.
- — HORIZONTAL P.C.W., EXTEND BETWEEN VERT. P.C.W. AT WINGS. HOLD FLUSH WITH FACE OF CONCRETE.
- ▲ — 1/2" FILLER EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONCRETE).
- ▲ — 4" X 3/4" FILLER, EXTEND FULL LENGTH OF ABUTMENT BETWEEN EDGES OF SLAB.

FOR P.C.W. DETAILS SEE SHEET 2



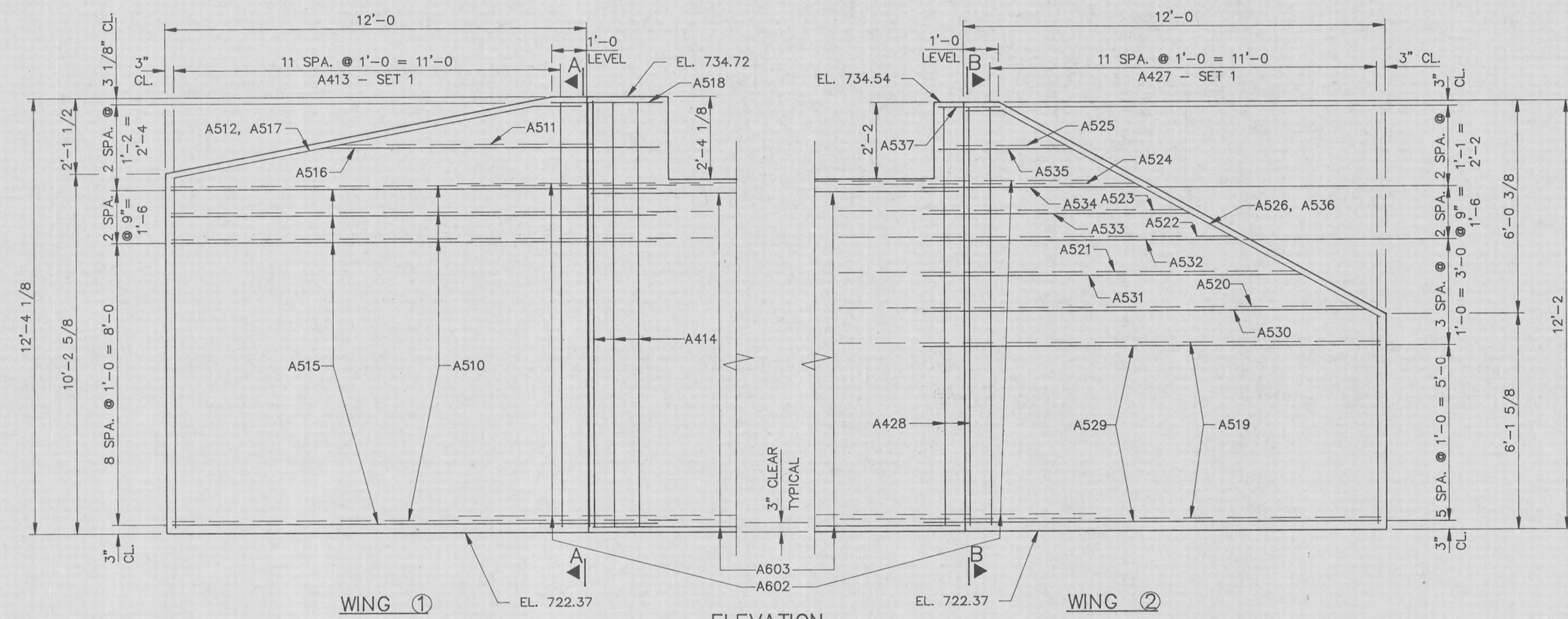
2" DIA. WEEP HOLE @ 7'-0" SPA. (4 REQ'D.) USE GEOTEXTILE FABRIC W/ SELECT GRANULAR BACKFILL. (ON B. F. 1'-0" X 1'-0" X 1'-0" MIN.) COST INCIDENTAL TO "CONC. MASONRY, BRIDGES."



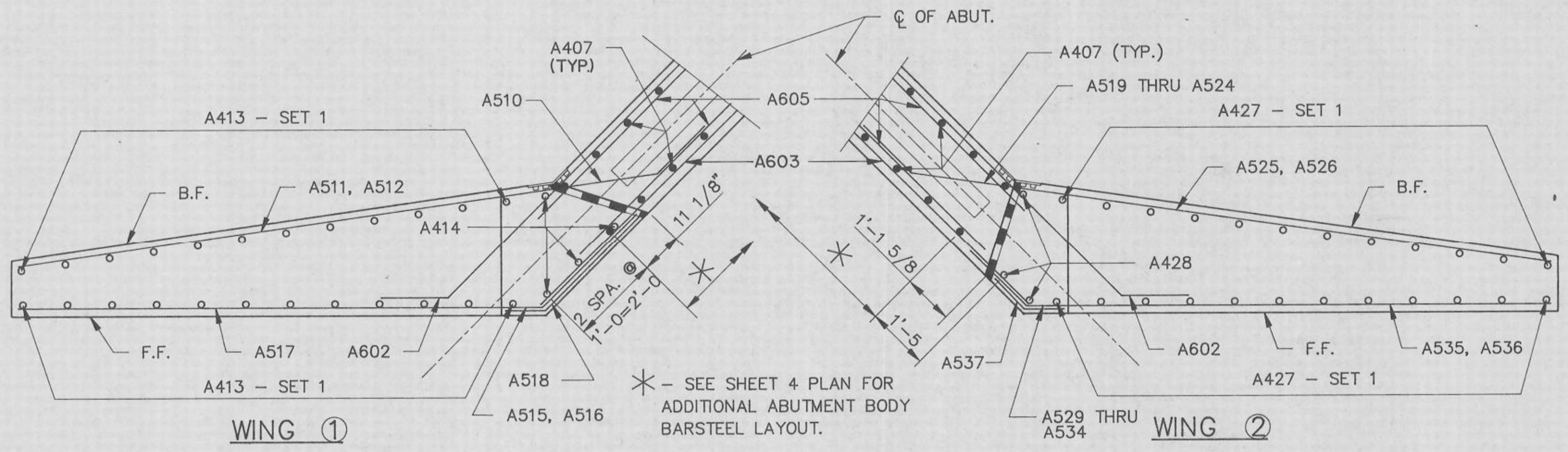
NOTE: CONTRACTOR SHALL FIELD DRILL EIGHT (8) 1" Ø HOLES IN THE WEB OF PILES #2 AND #6 FOR PLACEMENT OF BACKFACE WING REINFORCEMENT. WING ① A510, WING ② A519 THRU A521.

No.	Date	Revision	By
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS			
STRUCTURE B-32-127			
Const. Spec.	WI "81"	Drawn By	Plans Checked
		RLR	MSR
WEST ABUTMENT			SHEET 4 OF 9
X80106			

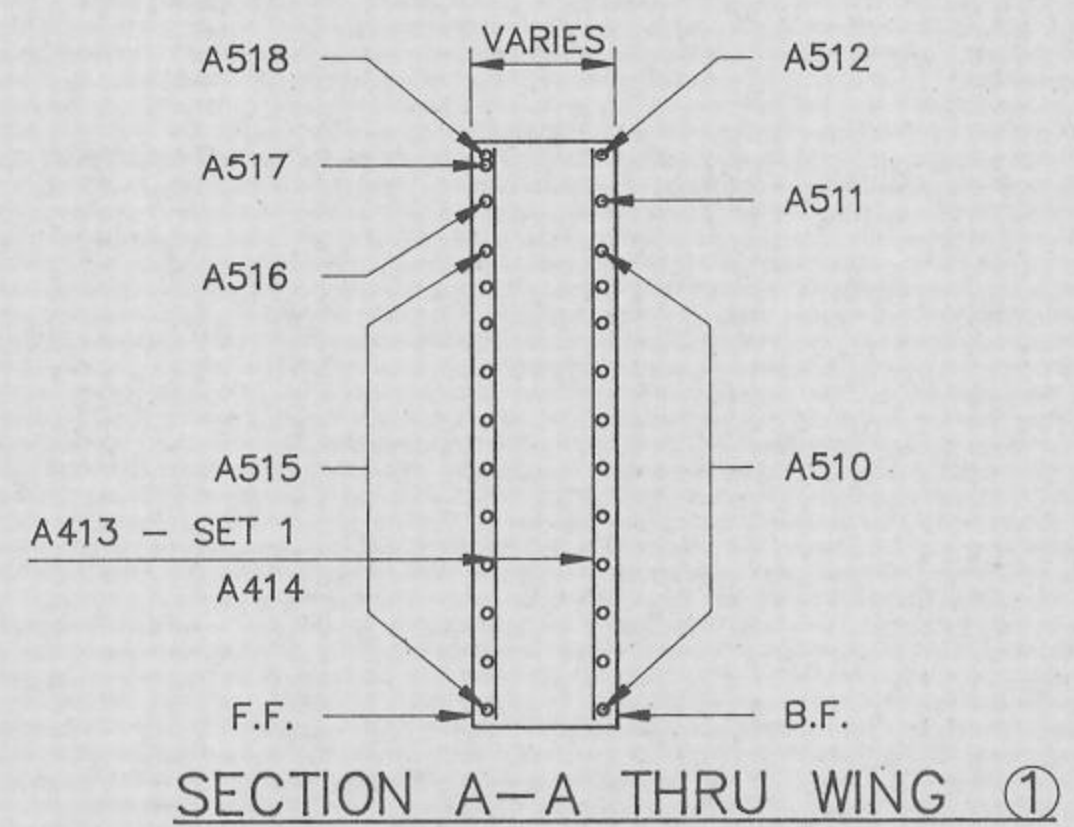
NOTE: FOR NAME PLATE LOCATION
WING ① SEE SHEETS 1 & 4.



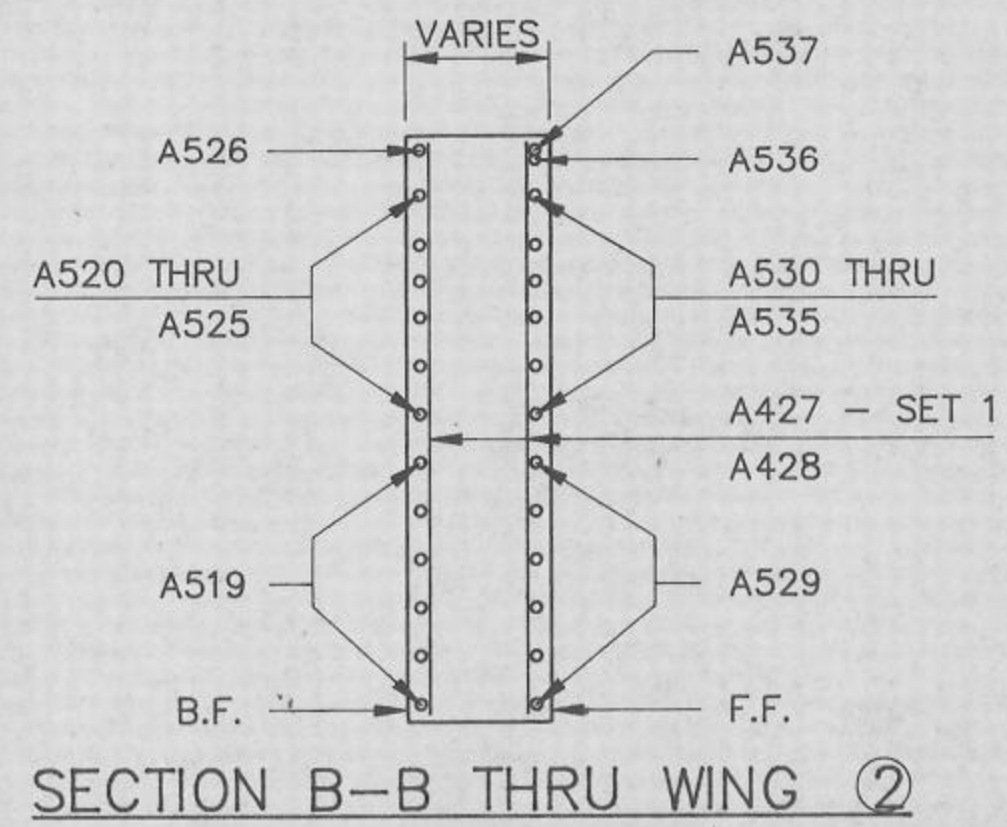
ELEVATION
(LOOKING AT F.F. OF WINGS)



PLAN



SECTION A-A THRU WING ①



SECTION B-B THRU WING ②

BILL OF BARS

MARK	NO. REQ'D	LENGTH	BENT	CUT. DIAGR.	LOCATION
A601	11	17'-0			BODY - B.F. - HORIZ. CENTER
A602	22	14'-3	X		" & WING - " - "
A603	11	33'-6			" - F.F. - "
A404	60	2'-3	X		" - TIES - "
A605	2	30'-0			" - TOP - "
A506	30	5'-4	X		" - BOTTOM - VERT.
A407	60	9'-8			" F.F. & B.F. - "
A408	30	8'-1	X		" - TIES TOP - "
A409	29	2'-0			" - DOWELS - "
A510	11	16'-3	X		WING ① - B.F. - HORIZ.
A511	1	7'-10			" " - " - "
A512	1	12'-0	X		" " - " - TOP
A413	12	22'-0		X	" " - F.F. & " - VERT.
A414	4	12'-0			" " - " - "
A515	11	14'-6	X		" " - " - HORIZ.
A516	1				